

TERRA VISTA COMMUNITY PLAN

RANCHO CUCAMONGA, CALIFORNIA

**LEWIS DEVELOPMENT COMPANY
AND
WESTERN PROPERTIES**

**GRUEN ASSOCIATES
PLANNING CONSULTANTS**

TERRA VISTA

COMMUNITY PLAN

Rancho Cucamonga, California

Lewis Development Company and
Western Properties

Third Printing - February, 1996

Prepared by

Gruen Associates
Madole & Associates, Inc.
Emmet L. Wemple & Associates
Land Concern Limited

TERRA VISTA COMMUNITY PLAN

**Passed, Approved, and Adopted by the City Council of the City of Rancho Cucamonga
February 16, 1983
Ordinance No. 190**

**Approved and Adopted by the Planning Commission of the City of Rancho Cucamonga
January 26, 1983
Resolution No. 83-13**

**Third Printing - February, 1996, Incorporates Passed, Approved and Adopted
Terra Vista Community Plan Amendments through TVCPA 9501, October 4, 1995.**

RESOLUTION NO. 83-13

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, RECOMMENDING APPROVAL OF PLANNED COMMUNITY ZONE NO. 81-01 AND CERTIFICATION OF THE FINAL ENVIRONMENTAL IMPACT REPORT TO THE CITY COUNCIL FOR THE TERRA VISTA PLANNED COMMUNITY

WHEREAS, an application and supporting documents have been filed for the establishment of a Planned Community Zone for approximately 1321 acres generally located between Base Line and Foothill Boulevard on the north and south, and Rochester and Haven on the east and west; and

WHEREAS, the Planning Commission has held several duly advertised public hearings pursuant to Section 65854 of the California Government Code; and

WHEREAS, the Planning Commission has reviewed and considered all elements of the proposed Planned Community and the associated Environmental Impact Report.

NOW, THEREFORE, the Planning Commission of the City of Rancho Cucamonga, California does resolve as follows:

SECTION 1: The Planning Commission recommends certification of adequacy for the final Environmental Impact Report to the City Council based on the following findings:

1. The final Environmental Impact Report has been prepared in accordance with the California Environmental Quality Act, the State, and local EIR guidelines.
2. The Planning Commission has reviewed and considered the information contained within the EIR prior to recommending approval of the project.

SECTION 2: The Planning Commission recommends adoption of this statement of overriding considerations to the City Council:

To the extent that the Planned Community allows the occurrence of significant effects identified in the final EIR without full mitigation the City Council has identified specific economic, ecological, and social reasons to support its action which make infeasible the project alternatives described in the final EIR or additional mitigation measures. The Planning Commission finds that facts supporting this finding are contained in the final EIR and the Planned Community text. Mitigation measures have been made a condition of approval of the Planned Community and are intended to mitigate or avoid the significant environmental effects identified in the

final EIR. The Planned Community itself is a mitigation measure which is intended to mitigate or avoid the significant environmental effects of development which could otherwise occur without a planned comprehensive approach such as the Planned Community standards, guidelines and regulations.

SECTION 3: The Planning Commission recommends adoption of Planned Community No. 81-01 to the City Council based on the following findings and recommended conditions;

FINDINGS:

1. Changes and alterations have been incorporated into the project which mitigate significant environmental effects to an acceptable level.
2. The Planned Community provides for the development of a comprehensively planned urban community within the zone that is superior to development otherwise allowed under alternate regulations.
3. The Planned Community provides for development within the zone in a manner consistent with the General Plan and with related development and growth management policies of the City.
4. The Planned Community provides for construction, improvements, or extension of transportation facilities, public utilities, and public services required by development within the zone.

CONDITIONS

1. Prior to final approval of the first tract map in Terra Vista, a detailed parks and open space implementation plan shall be prepared by the applicant and approved by the City Council.
2. Prior to consideration and approval of any development in Terra Vista, mitigation measures outlined in the final Environmental Impact Report shall be reviewed and considered.

3. All flood control and drainage structures needed for each individual development shall be constructed by the developer. Adequate plans showing that each phase can be safely and properly drained shall be submitted to and approved by the City Engineer prior to issuance of building permits for that phase.
4. All traffic and circulation improvements shall be designed and installed by the developer at the direction of the City Engineer and as needed for each development phase.
5. Prior to final approval of any residential development, adequate capacity shall exist or will be provided at the time of development for public services such as schools, sewer treatment capacity, water availability, and police and fire protection and utilities.
6. Prior to final approval of the first tract map, the creation of a maintenance district or other acceptable alternative shall be established for Terra Vista.
7. The developer shall encourage and facilitate bus service and transit routes throughout Terra Vista as development occurs. Programs such as ride sharing, provisions for park and ride facilities, bicycle lanes, vanpool programs, shall be considered.
8. As development phases occur, appropriate physical improvements shall be made by the developer for pedestrian and bicycle routes and transit facilities such as bus pullouts and waiting areas.
9. As each development phase occurs, berms, walls, building attenuation shall be provided to adequately mitigate any potential noise impacts.
10. The following standards and sections of the Planned Community text shall be revised as follows:
 - A. Commercial setbacks on Special Boulevards:
 1. Parking Area - Parking areas should be setback at an average depth of 43 feet from the face of curb and in no case shall be located closer than 28 feet to the face of curb.

2. Building Setback - Buildings shall be setback at an average of 43 feet from the face of curb and shall be no closer than 38 feet from the face of curb.

B. Residential Setbacks on Special Boulevards:

1. Building Setbacks - Two story or greater buildings shall be setback at an average depth of 43 feet from the face of the curb and no less than 38 feet from the face of curb. One story buildings shall be setback at an average 38 feet from the face of the curb and no less than 33' from the face of the curb.
2. Walls - The use of walls shall be discouraged wherever possible through the use of side on cul-de-sacs, berming, landscaping or building setbacks and orientation. Where walls are needed for single family detached units, they shall be setback at an average of 20 feet from the face of the curb and no less than 18' from the face of the curb. Where walls are needed for clustered multiple family units, they shall be setback a minimum of 28 feet from the face of curb.

- C. The format of the residential development standard section shall be revised to eliminate repetition and shall use matrixes for uses and standards.

- D. The text beginning in Section VI, page 2 through 4, under the subtitle "Applicable Law and Park Plan" shall be eliminated in its entirety.

- E. On page VI-4 of the text under "Provisions for Meeting Park Requirements", the statement on public open areas should be amended to read..."not to total less than 42.6 acres or 77% whichever is greater of the total amount required.

EXHIBIT "B"

CITY COUNCIL ACTION OF

FEBRUARY 7, 1983 HEARING

The City Council, in addition to accepting the conditions recommended by Planning Commission Resolution No. 83-13, altered and added the following items:

1. Condition 10-E of Commission Resolution was amended by the following action:
 - A. Beginning on page VI-4 of the Terra Vista Text, under the section entitled "Provisions for Meeting Park Requirements," the first point under the first paragraph shall be amended to require not less than 55.8 acres of public parks, greenways and trails. The second point under the first paragraph shall be entirely eliminated. The second paragraph is amended to eliminate the phrase, "and provision of credited private open space, as discussed below."
 - B. The section entitled, "Credit for Private Open Space," beginning on page VI-4 and continuing to VI-7, shall be eliminated entirely.
 - C. Any other areas of the text with reference to parks shall be amended to reflect the intent of the above changes.
2. The 13.2 acres of public park area, which is currently not shown on the land use plan, shall be shown on the north side of Base Line, adjacent to the east side of Deer Creek Channel.
3. The neighborhood center located on the corner of Spruce and Terra Vista Parkway shall be eliminated, and a neighborhood center shall be designated on the northeast corner of Haven and Base Line.
4. An expanded green area, not less than 10,000 square feet in area, shall be provided on the southeast corner of Haven and Base Line. Further, this area shall not be credited towards park fees.

Ordinance No. 287

**Amendment No. 2
Ordinance No. 301**

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Victoria Planned Community Amendment 85-01, to modify the community plan text to allow a recreational vehicle storage lot with mini-warehouse in the medium-high (MH) or high (H) land use category, and to allow its operation on a commercial, for rent basis, open to the general public.

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Community Plan Amendment 86-01, to modify the Community Plan text to change the land use designation from junior high school site to low medium residential (4-8 du/ac) for 20 acres of land located on the west side of Rochester Avenue, north of Church Street.

Passed, approved, and adopted: March 5, 1986, by the City Council of the City of Rancho Cucamonga.

Passed, approved, and adopted: November 5, 1986, by the City Council of the City of Rancho Cucamonga.

**Amendment No. 1
Ordinance No. 270**

**Amendment No. 3
Resolution No. 87-457**

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Development Plan Amendment 85-03, requesting a change to the Land Use Plan and text for the southeast quadrant of Terra Vista, including a hospital and mixed commercial, office and residential uses, located on the north side of Foothill Boulevard, west of Rochester Avenue, east side of Milliken Avenue.

A Resolution of the City Council of the City of Rancho Cucamonga, California, approving Terra Vista Planned Community Amendment No. 87-02, amending the Community Plan text by establishing a business park overlay zone for area designated as Office Park District south of Town Center Drive, west of Spruce Avenue.

Passed, approved, and adopted: August 21, 1985, by the City Council of the City of Rancho Cucamonga.

Passed, approved, and adopted: August 19, 1987, by the City Council of the City of Rancho Cucamonga.

**Amendment No. 5
Ordinance No. 334**

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Planned Community Amendment 87-04 to modify the Land Use Designations from Office Park (OP), Commercial (C) and Community Commercial (CC) to Office Park (OP) and Community Commercial (CC) involving a 71-acre parcel within the Terra Vista Planned Community located on the north side of Foothill Boulevard between Haven Avenue and Spruce Avenue - APN 1077-421-5, 6 and 13.

Passed, approved, and adopted: January 20, 1988, by the City Council of the City of Rancho Cucamonga.

Ordinance No. 342

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Community Plan Amendment 88-01, to modify the Community Plan text to require a Conditional Use Permit for Shopping Centers and certain uses in the Office and Commercial Standards section of the Terra Vista Community Plan.

Passed, approved, and adopted: March 16, 1988, by the City Council of the City of Rancho Cucamonga.

Ordinance No. 346

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Community Plan Amendment 88-02 to modify the Low-Medium and Medium Residential Development Standards.

Passed, approved, and adopted: May 18, 1988, by the City Council of the City of Rancho Cucamonga.

Ordinance No. 407

An Ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Community Plan Amendment 89-01 to change various school, trail, and park site designations; to allow the establishment of auto service stations subject to the Conditional Use Permit process along selected major arterials; to delete a portion of Orchard Avenue; and to update the Landscape Design Guidelines and Land Use maps to reflect previous project approvals and "as-built" conditions.

Passed, approved, and adopted: December 6, 1989, by the City Council of the City of Rancho Cucamonga.

Ordinance No. 433

**Amendment No. 6
Ordinance No. 466**

An ordinance of the City Council of the City of Rancho Cucamonga, California, approving Terra Vista Community Plan Amendment 90-01, amending the density provisions of the Terra Vista Community Plan to require changes in land use designations to be approved by City Council.

An ordinance of the City Council of the City of Rancho Cucamonga, California, approving Terra Vista Planned Community Amendment 91-02, amending various development standards and design guidelines for multi-family residential districts.

Adopted July 11, 1990.

Adopted November 20, 1991.

Ordinance No. 457

**Amendment No. 7
Ordinance No. 527**

An ordinance of the City Council of the City of Rancho Cucamonga, California, approving Terra Vista Community Plan Amendment 91-01, establishing certain streetscape and site design standards consistent with the Foothill Boulevard Specific Plan for that portion of Foothill Boulevard within the Terra Vista planned community.

An ordinance of the City Council of the City of Rancho Cucamonga, California, approving Terra Vista Community Plan Amendment 94-01, amending the Land Use Map from Office Professional to Community Commercial for 25 acres of land bounded by Foothill Boulevard on the south, Spruce Avenue on the west, Church Street on the north, and Elm Avenue on the east, and amending various text and graphics of the Community Plan.
APN: 1077-421-58 and 63.

Adopted October 2, 1991.

Adopted July 20, 1994.

Ordinance No. 533

An ordinance of the City Council of the City of Rancho Cucamonga, California, adopting Terra Vista Community Plan Amendment 94-02, amending the land uses within the Office and Commercial areas.

Adopted August 3, 1994.

**Amendment No. 9
Ordinance No. 550**

An ordinance of the City Council of the City of Rancho Cucamonga, California, approving Terra Vista Community Plan Amendment 95-01 to change the land use map from "mixed use, office, commercial, residential," to community commercial for approximately 47.3 acres of land and to medium high residential (14-24 dwellings per acre) for approximately 19.2 acres of land bounded by Foothill Boulevard on the south, Rochester Avenue on the east, the future Poplar Drive and future Church Street on the north, and the future Orchard Avenue on the west, and to make changes to portions of the text and graphic exhibits of the Community Plan to implement design features of the land use designations.

Adopted October 4, 1995

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I Introduction

I Introduction

The planned community of Terra Vista, encompassing 1,321 acres below the San Gabriel Mountains west of San Bernardino (Figure I-1), will be the heart of the City of Rancho Cucamonga. The community is designed not only to provide high-quality environments for its own residents, but also to provide a mix of commercial, recreational, and cultural activities that will make Terra Vista an important, multi-faceted asset to the entire City of Rancho Cucamonga.

How will this dual purpose be reflected in the community's physical development? Terra Vista will be a complete community with a unique pedestrian orientation, both in residential areas and in commercial centers. It will be easily accessible from adjoining areas, integrating with--and completing in many instances--the citywide network of roads, trails, and transit facilities. Terra Vista will join people of varying ages, backgrounds, and interests; their interaction will be fostered through human-scaled design and a full complement of community facilities. In short, Terra Vista will be a balanced community with a mix of functions so rich as to make it almost self-sufficient; and it will also be a major focus of commerce, recreation, and employment for Rancho Cucamonga and the region.

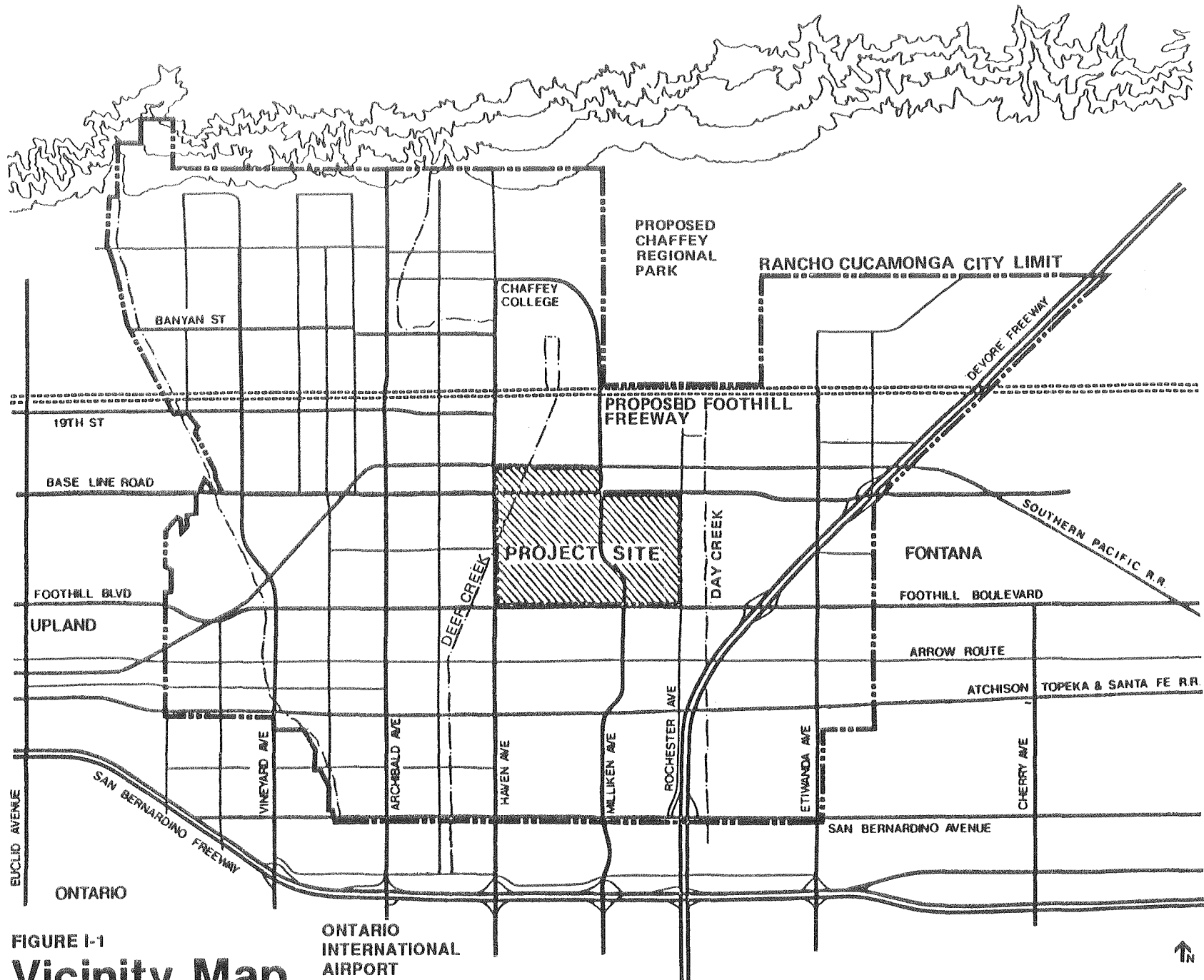


FIGURE I-1
Vicinity Map

Overview of Planning Concept

Even though the mix of activities in Terra Vista will be very broad, the planning concept is relatively straightforward. It is based on a thorough analysis of the natural environment, man-made improvements, projections of regional growth, and City development objectives.

The community will take its basic shape from the following three elements:

- Neighborhood organization
- Park and greenway system
- Multi-faceted circulation system

Neighborhood Organization

Four neighborhoods will be created in Terra Vista (Figure I-2), each intended to have or share an elementary school, distinguishing landscaping, commercial areas, and green space. While the balance of residential uses will vary with each neighborhood, all neighborhoods will have a mix of densities in order to accommodate residents of many income groups and lifestyles.

Park and Greenway System

A major landscaped spine -- the Terra Vista Greenway -- will bisect the community from northeast to southwest (Figure I-3). Greenway fingers will extend to and through each of the community's many residential enclaves, school sites, and important community facilities. The western terminus of the greenway spine is the Terra Vista Town Center. Because of the greenway system's

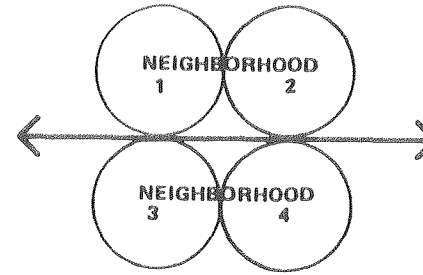


FIGURE I-2
Neighborhood Concept Diagram

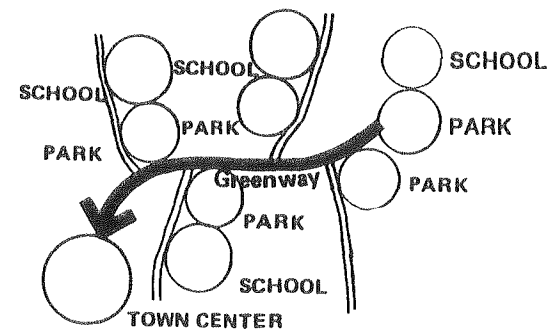


FIGURE I-3
Greenway Concept Diagram

design, which will include walking trails and bicycle routes, residents can participate in most community activities without having to use a car.

Multi-faceted Circulation System

While regional circulation will be served by the arterials that bound the community, automobile and bus circulation for Terra Vista residents will be focused on a landscaped loop parkway that connects residential areas with the Town Center and other commercial clusters (Figure 1-4). Most of the primary activity centers in Terra Vista, including many of the community's numerous parks, are situated along this loop parkway. Landscaping along the loop parkway will create an enclosed arbor effect in certain places and will expose broad vistas at others. The loop parkway will provide a unique driving experience that will be appreciated by community residents and visitors alike.

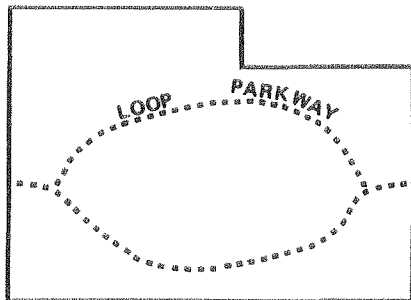


FIGURE I-4
Circulation Concept Diagram

Relationship of General Plan and Community Plan

Development within the City of Rancho Cucamonga is governed by the General Plan adopted in April 1981. In conformance with this General Plan, the Terra Vista Community Plan is the document that will guide the development of the Terra Vista site. Among the major General Plan goals that are addressed by the Terra Vista Community Plan are:

- Land use planning consistent with infrastructural capacities and environmental resources
- Efficient circulation system with provision for alternatives to the automobile
- Good living environments for all economic segments of the community
- Recreational facilities that promote relaxation and social interaction
- Design features that reinforce the identity of the City and enhance its appearance
- Provision for community services in furtherance of education, cultural entertainment, public safety and health, and civic pride
- Reduced dependence on nonrenewable energy sources

The Terra Vista Community Plan was approached within the context of these goals, which pertain to the entire City of Rancho Cucamonga, as well as in pursuit of several more specific objectives that have been developed for Terra Vista.

This Community Plan and its accompanying Environmental Impact Report have been reviewed by the City of Rancho Cucamonga Planning Commission and City Council in a series of public hearings. Adoption of these documents by the City Council has zoned the property "PC" (for "Planned Community") and made its future development subject to the Terra Vista Planned Community guidelines.

Use of Graphic Illustrations in this Document

This Community Plan contains many conceptual diagrams, maps, plans, and artists' renderings in order to enhance the text and to explain more clearly the type of development that will occur in Terra Vista. Except as otherwise noted in the text, the conceptual diagrams, maps, and plans are intended to govern the ultimate development pattern at Terra Vista and are considered to be part of the document. The artists' renderings of particular portions of the Terra Vista site are intended to convey the image and quality of development that is planned for Terra Vista. In interpreting the artists' renderings, the following should be kept in mind:

- Since much of development at Terra Vista will not take place for ten years or more, the designs, shapes, and finishes of buildings will undoubtedly differ from those depicted in illustrations. However, the developments can

be expected to conform to high quality standards as indicated by the illustrations.

- Landscaping in the illustrations is depicted at full maturity; in reality, it may take a number of years for this to occur following the development of individual sites at Terra Vista. The locations, species, and sizes of the trees and plant materials may differ from those depicted in the illustrations, but the overall landscaping concept should conform generally to concepts depicted graphically.

In some cases, illustrations of other communities have been provided in order to show the general approach to a design problem that may be considered for Terra Vista. The actual development of such features at Terra Vista will most likely differ in materials, construction, and other factors but can be expected to have a similar quality and effect.

Organization of Community Plan Text

The Community Plan is divided into five major chapters in addition to this Introduction:

- Chapter II - Background and Existing Conditions
- Chapter III - The Plan
- Chapter IV - Design Guidelines
- Chapter V - Community Development Standards
- Chapter VI - Implementation of the Plan

"Background and Existing Conditions" describes existing site conditions and plans for the surrounding land and roadways affecting the development concept for Terra Vista.

"The Plan" describes the overall planning concept as well as the general pattern of development planned for Terra Vista. The chapter concludes with a statistical summary of the overall development plan.

"Design Guidelines" provides the link between the plan and actual development by specifying criteria that will be used to assure high-quality design.

"Community Development Standards" contains the specifications to which ultimate development must adhere, based on the more general planning concept and design guidelines.

"Implementation of the Plan" describes the procedural steps that lead from the Community Plan to actual developments.

II Background and Existing Conditions

II Background and Existing Conditions

The Terra Vista site is in the geographic center of Rancho Cucamonga, bounded by Haven Avenue on the west, Foothill Boulevard on the south, and Rochester Avenue on the east. The northern boundary between Rochester Avenue and the future extension of Milliken Avenue is Base Line Road; the Southern Pacific Railroad is the northern boundary of the site between the Milliken Avenue extension and Haven Avenue.

The entire planned community encompasses 1,321 acres. Approximately 27 acres are contained within existing highway rights-of-way. The remaining 1,294 acres are owned by Lewis Development Co. and affiliated entities.

Natural Features

The relatively flat site slopes gently from north to south, with a two- to three-percent overall slope (Figure II-1). The Deer Creek Flood Control Channel, which traverses the northwestern part of the property, has been improved by the U.S. Army Corps of Engineers as part of the Cucamonga Creek Improvement Project. The flood control improvements are now "flood-ready". The final inlets for local drainage were completed in 1983.

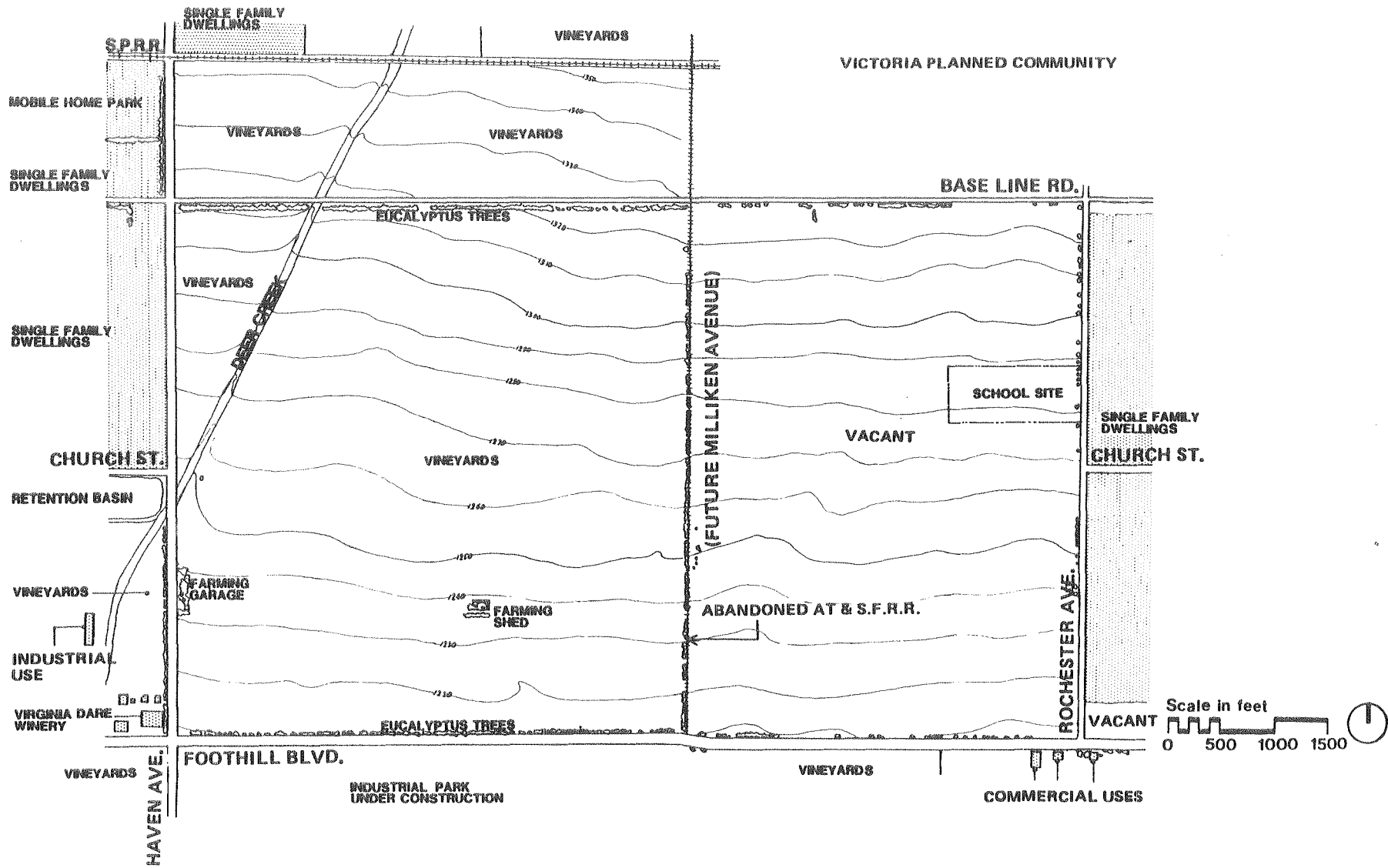


FIGURE II-1

Existing Conditions (AT ADOPTION, 1983)

Existing Structures

There are not structures on the site, other than four farming sheds on Haven Avenue south of Church Street and a farming shed north of Foothill Boulevard and west of Milliken Avenue.

Vegetation

The entire site was once a producing vineyard. Over time, changing biological conditions and the expanding development in the area have made continued agricultural use economically unrealistic. The western portion of the site, west of the Milliken Avenue extension, is still producing grapes as an interim use until development occurs. East of Milliken Avenue the site is covered with scrub vegetation and native grasses.

Along some of the site boundaries are rows of eucalyptus trees (figure II-1). These eucalyptus rows are continuous and fairly dense along both Foothill Boulevard and Base Line Road between Haven Avenue and the Milliken Avenue extension. East of Milliken Avenue on both Foothill Boulevard and Base Line Road, the eucalyptus trees are more scattered, and the eucalyptus planting along Rochester Avenue is sparse. There is also a continuous eucalyptus row running north to south along the future Milliken Avenue alignment, next to an abandoned Santa Fe Railroad track which has been removed. The only other eucalyptus trees onsite are small stands located at Deer Creek where it crosses Haven Avenue, on Base Line Road between Rochester and Milliken Avenues, and around the farm sheds. Along Haven Avenue, there are trees on the side of the street opposite the site between Foothill Boulevard and Deer Creek.

Surrounding Uses

On the west side of Haven Avenue, single-family homes adjoin the site from Church Street northward, with a mobile home park just south of the Southern Pacific Railroad right-of-way. South of Church Street are a retention basin and vineyards associated with the Virginia Dare Winery buildings at the northwest corner of Haven Avenue and Foothill Boulevard.

To the east, single-family developments adjoin the property along the full length of Rochester Avenue, except for a small undeveloped area at Foothill Boulevard.

All of the property south of the site is within the City's industrial area and is planned for light industrial use. Rancho Cucamonga Business Park, directly south of the site between Haven Avenue and Milliken Avenue, is under development, with roads and a number of industrial buildings in place. Ground has also been broken for the San Bernardino County West Valley Law and Justice Center, near the southeast corner of Haven Avenue and Foothill Boulevard, adjoining which the City of Rancho Cucamonga plans its civic center complex. East of Milliken Avenue, much of the land is in interim agricultural use except for a few small commercial uses on the south side of Foothill Boulevard at Rochester Avenue. To the north, the only existing developments are single-family homes on the east side of Haven Avenue and a lumber yard near Rochester Avenue and Base Line Road. The remainder of property to the north is essentially vacant; most of it is within the Victoria Planned Community, slated to be developed in residential use. Farther north are the San Gabriel mountains, which provide a dramatic visual backdrop.

Access

The site is surrounded by major regional circulation routes. Haven Avenue is a primary north-south traffic artery leading to Chaffey College and the Alta Loma residential areas. Foothill Boulevard is a major east-west artery. Both streets are four-lane, relatively high speed routes. Base Line Road is a two-lane road that provides a primary traffic link to Etiwanda. Rochester Avenue, the site's eastern boundary, is improved to half-width and provides two lanes of travel.

None of the streets surrounding the project is developed to its ultimate configuration. The General Plan calls for all of these streets to be eventually widened to four or six lanes.

The site also has convenient freeway access, as shown in Figure I-1. The San Bernardino Freeway (I-10), two and one-half miles south of the site, provides access at its Haven Avenue and Milliken Avenue offramps (the latter is not presently a through connection). The Devore Freeway (I-15) passes in a diagonal direction three-quarters of a mile east of the site, with offramps at Foothill Boulevard and Base Line Road.

The proposed Foothill Freeway (Route 30), approximately three-quarters of a mile north of the site, will also provide access via planned offramps at Milliken and Haven Avenues.

Community Services

Educational Facilities

The project falls within three elementary school districts (Figure II-2). The area north of Base Line Road is within the Alta Loma School District. South of Base Line Road, the area west of the Milliken Avenue extension is within the Central School District and the area east of Milliken Avenue is within the Etiwanda School District.

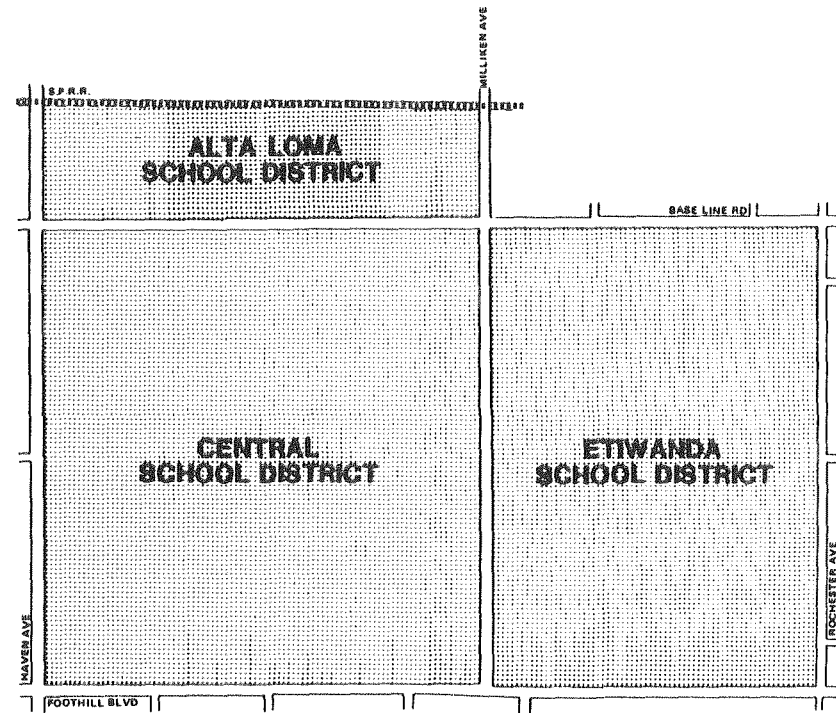


FIGURE II-2
School Districts

Secondary education is provided by the Chaffey Joint Union High School District, which operates Alta Loma High School west of the site and Etiwanda High School to the east. The General Plan calls for several schools within the project: an elementary school in the Etiwanda School District, and two elementary schools and one junior high school in the Central School District.

Other Community Services

Terra Vista will be served by the Foothill Fire Protection District, which currently has a facility on Base Line Road one quarter mile east of the site. Police protection will be provided by the San Bernardino County Sheriff's Department under contract with the City of Rancho Cucamonga. The County also operates a library on Base Line Road near Hellman, supplementing additional library services associated with public schools. Lion's Park Community Center, adjacent to the library on Base Line Road, is operated by the City of Rancho Cucamonga. The Rancho Cucamonga City Hall and the West Valley Law and Justice Center are built south of the Terra Vista site.

III The Plan

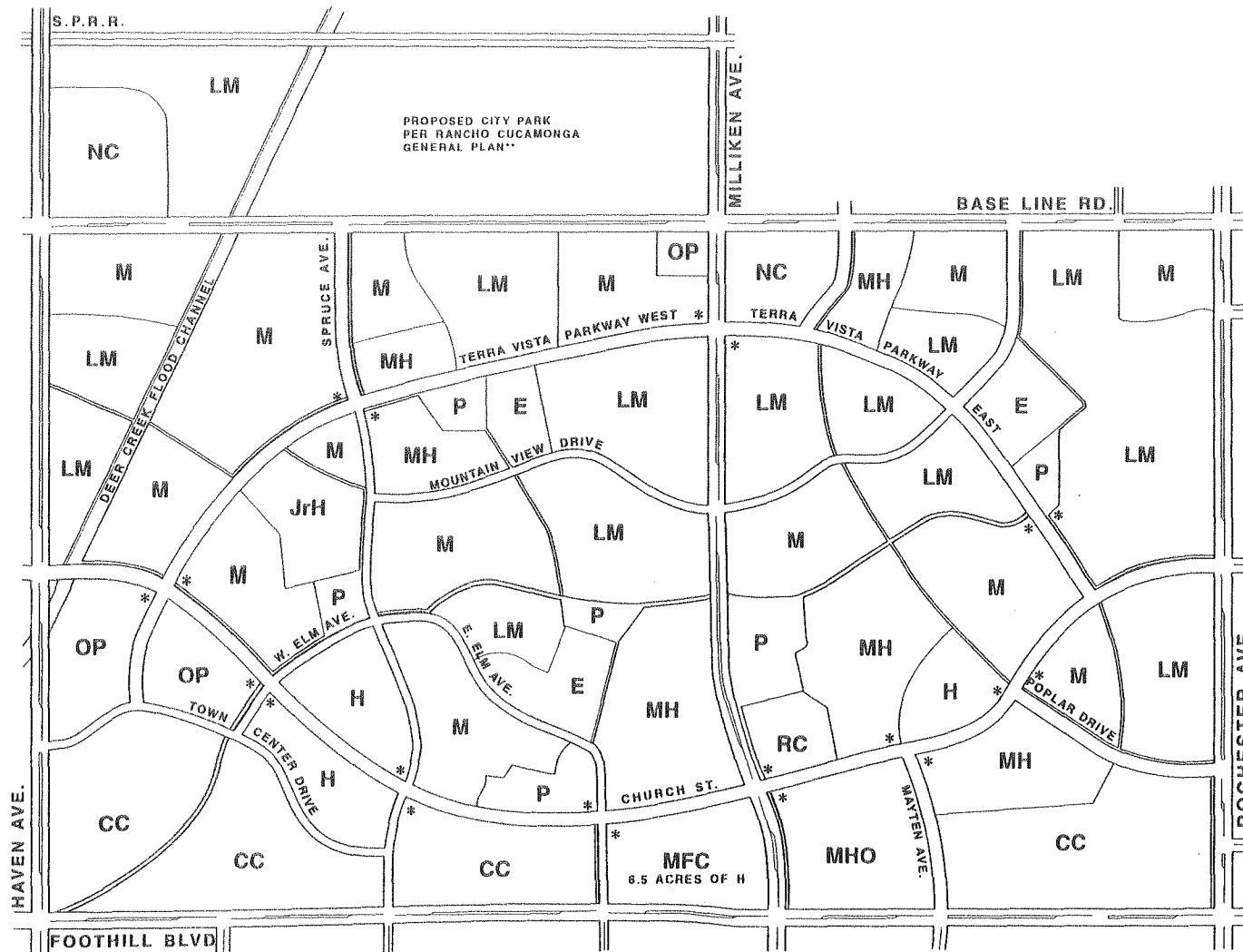
III The Plan

The major organizing elements of the Terra Vista Community Plan introduced briefly in Chapter I are elaborated on in this chapter, which demonstrates how numerous elements on different scales have blended together to form the planned community. Among the major elements are:

- Neighborhood organization and identity
- The park and greenway system
- Circulation--private vehicles, transit, bicycle, pedestrian
- Land use

Also figuring significantly in the plan are considerations integral to all the above elements, including infrastructural systems, safety, resource conservation, and other issues.

The interrelationship of these components has resulted in a planned community whose layout cannot be credited to any one overriding element. Instead, these multiple components have contributed to a flexible, comfortable melding of scales, treatments, and visual experiences. In total, the development of Terra Vista will represent a major stride in realizing the goals of the City of Rancho Cucamonga. The plan for Terra Vista is presented in Figure III-1. A statistical summary of land use within the Terra Vista community at full buildout concludes this chapter.



RESIDENTIAL	
LM	LOW MEDIUM DENSITY (4-8 DU/AC)
M	MEDIUM DENSITY (8-14 DU/AC)
MH	MEDIUM HIGH DENSITY (14-24 DU/AC)
H	HIGH DENSITY (24-30 DU/AC)
COMMERCIAL	
CC	COMMUNITY COMMERCIAL
NC	NEIGHBORHOOD COMMERCIAL
RC	RECREATIONAL COMMERCIAL
OP	OFFICE PARK
MIXED USE	
MFC	FINANCIAL, RESTAURANTS, RESIDENTIAL HOSPITAL & RELATED FACILITIES, OFFICE
MHO	MIXED OFFICE
PUBLIC & QUASI-PUBLIC	
JrH	JUNIOR HIGH SCHOOL (PROPOSED)
E	ELEMENTARY SCHOOL (PROPOSED)
P	PARKS
*	BUS STOP

**PARTIAL DEDICATION OF UNIMPROVED LAND



FIGURE III-1

Terra Vista Plan

Density Ranges of Approved Projects may vary slightly from the Plan; See "As Built Land Use Progress Plan" - Figure VI-3 on page VI-11.

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III-2

Because so many of the factors that have contributed to the Terra Vista Community Plan are interrelated, some of the items discussed in this chapter have design implications that warrant additional explanation. These are covered in the following chapter on design guidelines (Chapter IV).

Neighborhood Organization and Identity

The Terra Vista community is conceived as a series of planned, interrelated neighborhoods linked together by common recreational and institutional functions (Figures III-2 and III-3). The greenway system is both the focus of and the division between the individual neighborhoods.

Neighborhood Organization

The four neighborhoods of Terra Vista have certain characteristics in common, while other characteristics are unique to each neighborhood. Among the characteristics shared by all four neighborhoods are:

- A mix of housing type and densities
- Educational facilities and parks
- Commercial facilities
- Linkage with all community facilities via the greenway and trail system

Mix of Housing Types and Densities. One of the goals of the Rancho Cucamonga General Plan is to provide accommodation for the full range of housing types serving all income groups. In

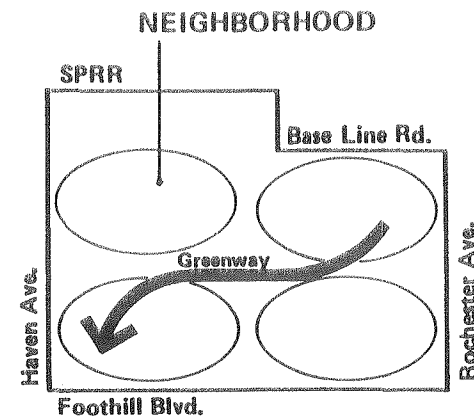


FIGURE III-2
Four Neighborhoods Defined by Greenway and Major Arterials

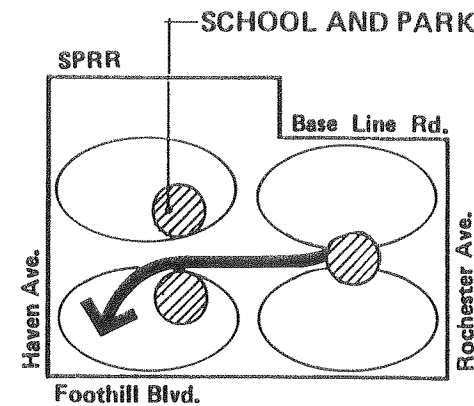


FIGURE III-3
Elementary School and Park at Each Neighborhood Core

Terra Vista, this goal is reflected in the neighborhood units, where provision has been made for housing densities ranging from low-medium to high-density. Physically, this translates into building types ranging from single-family housing through high-density multifamily housing, with the full range of opportunities for cluster, duplex, townhome, and other housing configurations in between. With this variety, people from many income groups and of differing lifestyles can enjoy living in--and can afford housing in--Terra Vista.

Educational Facilities. The four school districts serving the Terra Vista community were noted in Chapter II (see Figure II-2). This overlapping of multiple districts on the planned community combines with the neighborhood concept to allow each neighborhood to have its own elementary school or to share an elementary school. In the Central School District, each neighborhood has its own elementary school and two neighborhoods share a junior high school. In the Etiwanda School District, two neighborhoods share an elementary school (Figure III-4). The two neighborhoods in the Etiwanda School District will attend a junior high school outside of the planned community. The high school that will serve the Terra Vista site is also located outside the planned community. The greenway and trail system is planned to maximize opportunities for students to walk or ride bicycles to school (Figure III-4).

Each of the elementary schools and the junior high school within Terra Vista are combined with adjacent parks to provide opportunities for joint use of the two facilities. The parks are all accessible via the greenway and trails, which are designed in all cases to provide interconnection of community areas without disrupting potential joint use activities. This is achieved by orienting trails to the sides of parks opposite the schools.

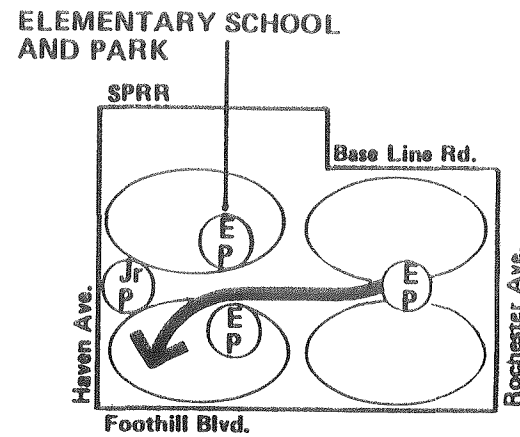


FIGURE III-4
Junior High School and One Elementary School Shared by Neighborhoods

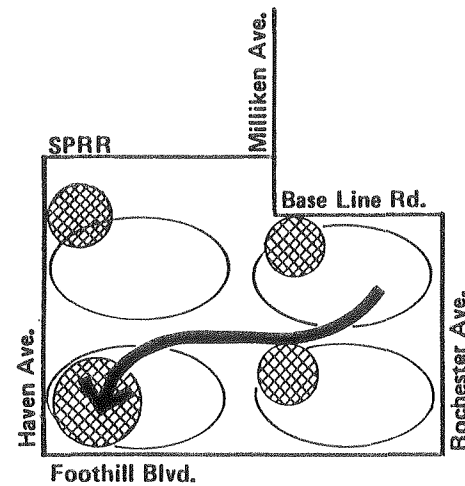


FIGURE III-5
Commercial Facilities in Each Neighborhood

Commercial Facilities. Each of the Terra Vista neighborhoods has convenient access to commercial facilities within the planned community via the greenway and trail system. For example, the mixed-use commercial centers along Foothill Boulevard are served by four north-south greenway or trail connections. Beyond these community-oriented commercial facilities, each of the neighborhoods also features a neighborhood-oriented commercial node (Figure III-5), as follows:

- The northwest neighborhood is served by a planned neighborhood commercial center at the northeast corner of Base Line Road and Haven Avenue.
- The northeast neighborhood is served by planned neighborhood commercial development at the southeast corner of Milliken Avenue and Base Line Road.
- The southwest neighborhood is served by the community commercial center at the northeast corner of Haven Avenue and Foothill Boulevard.
- The southeast neighborhood features some specialized facilities which are a part of the recreational commercial center, as well as convenience commercial facilities within the mixed use parcels, planned to be oriented toward residents of the high-density housing.

Although the centers will be identified with their respective neighborhoods by virtue of their locations, their designs, and their landscape treatments, the locations of the centers at neighborhood edges encourage their use by residents of other neighborhoods within Terra Vista, as well as by people living elsewhere in the City of Rancho Cucamonga.

Linkage With Community Facilities. As indicated in Figure III-2, the neighborhoods are defined by -- and linked by -- the community greenway and trail system. The neighborhoods are conceived and arranged to focus on the greenway and trail system that ties together all of Terra Vista. This openness and sense of communitywide focus on public open space amenities are assured through design guidelines and development standards that are covered in detail in Chapters IV and V, respectively.

Within the context of this communitywide linkage and access, each of the four neighborhoods of Terra Vista may also have its own specific community-oriented facilities, such as civic, religious, and community service institutions. Since locational needs and site proportions are unique to each of these facilities and institutions, specific locations have not been designated in the Community Plan. Instead, as such facilities are developed, they will be reviewed individually for compatibility, site planning, design and other factors to assure their comfortable fit and maximum contribution to the community.

Neighborhood Identity

While the preceding discussion focused on the elements that are central to the neighborhood concept and features that are shared by all four neighborhoods within the planned community, the factors that give each neighborhood its distinct identity within the community are also essential to the concept. These factors include:

- The emphasis of residential density
- The balance of non-residential land uses

Emphasis of Residential Density. While each neighborhood will feature a full range of housing types and densities, the neighborhoods of Terra Vista have been conceived to conform with developments around them outside of the planned community. This is achieved in two fashions:

- In general, higher overall densities have been oriented toward the two southern neighborhoods. Lower overall densities can be found in the two northern neighborhoods in order to be compatible with the generally lower densities in the existing and planned communities to the northwest and northeast of the Terra Vista site. The higher overall densities in the two southern neighborhoods are compatible with the commercial/industrial development that constitutes the southern boundary of Terra Vista along Foothill Boulevard (Figure III-6). Furthermore, greater efficiencies are achieved in having the most residential units closest to employment and commercial centers. It is expected that many Terra Vista residents will be employed both in the on-site commercial and office parks and in the industrial park immediately south of Terra Vista.
- Within each of the neighborhoods, regardless of its overall density, the lower-density developments are oriented to the outside of the neighborhood. The higher-density parcels tend to be in the interior of the neighborhood, often adjacent to the greenway and parks. This arrangement further insulates surrounding developments from high-density residential clusters within Terra Vista and also places important open space amenities near the greatest number of community residents.

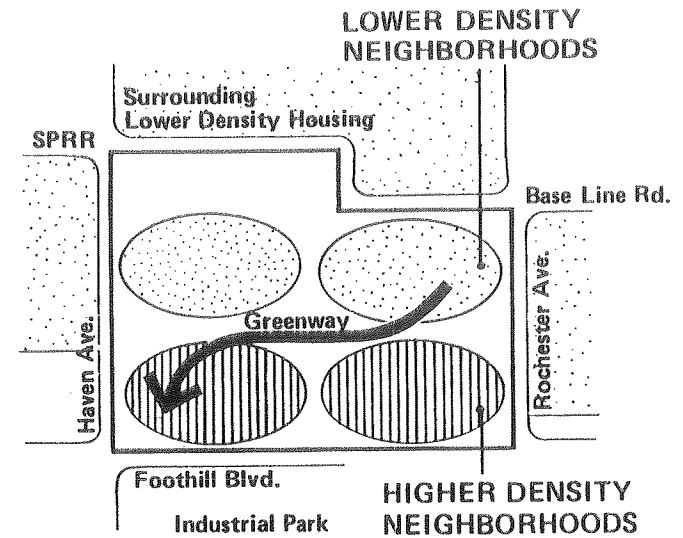


FIGURE III-6

Higher-Density Neighborhoods Near Foothill Boulevard and Lower-Density Neighborhoods Near Base Line Road

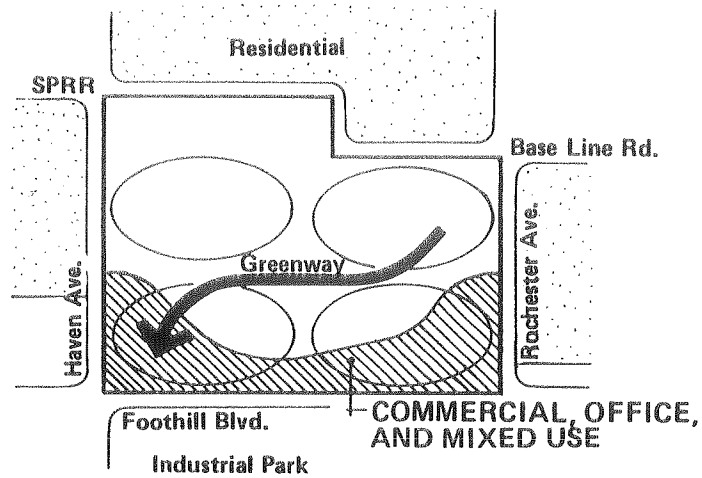


FIGURE III- 7
Residential Orientation for Northern Neighborhoods and
Commercial/Mixed-Use Orientation for Southern Neighborhoods

Balance of Land Uses. To assure the further compatibility of development at Terra Vista with neighboring communities, the northern two neighborhoods of Terra Vista are more residentially-oriented overall than the two southern neighborhoods. In the two northern neighborhoods, other than the neighborhood commercial/professional centers and community uses in various locations, all development is planned to be residential and open space. In contrast, in the southern two neighborhoods, a much higher proportion of land is devoted to commercial and mixed uses, in conjunction with commercial development along Foothill Boulevard and the industrial park to the south (Figure III-7).

The four neighborhoods of Terra Vista are shown with their full complements of land uses in Figure III-8.

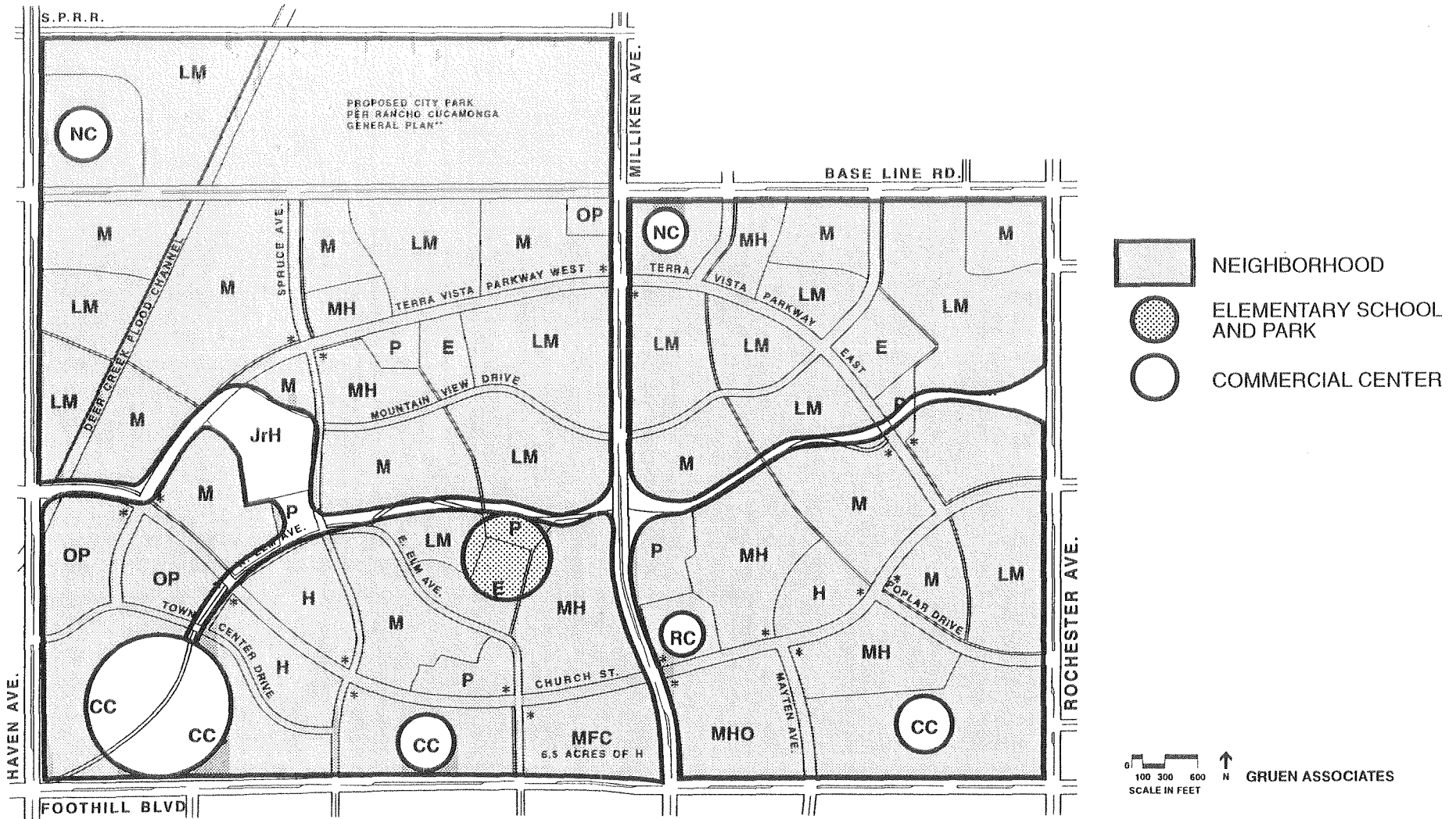


FIGURE III-8
Neighborhoods

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 III-8

The Park and Greenway System

The backbone of the Terra Vista community is the greenway, a landscaped, linear park running from northeast to southwest through the entire community (Figure III-9). Within this landscaped spine are paths for both pedestrians and bicycles, along with areas for active and passive recreation. Projecting from this major open space are secondary greenways--or trails--that meander through the four neighborhoods of Terra Vista, penetrating residential developments, commercial clusters, and parks (Figure III-10). Located along the greenway and trails are the parks and schools of Terra Vista (Figure III-11).

The primary function of the greenway system is to provide a focus for leisure-time activities and community services, a verdant image for the community, and a pleasant means of walking or cycling from one part of the community to another. This section explores a number of aspects of the park and greenway system that make it central to the planning of Terra Vista, including:

- The greenway and trails
- Public parks and open space

The Greenway and Trails

The greenway corridors through the Terra Vista community (Figure III-12) have been carefully located to meet the following criteria:

- "Anchors" to promote full use of greenway and trails

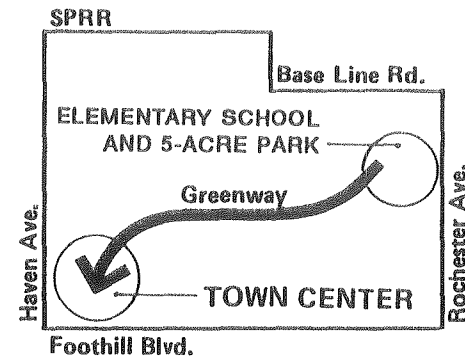


FIGURE III-9

Greenway Spine for East-West Linkage Through Community with Integral Pedestrian and Bicycle Circulation

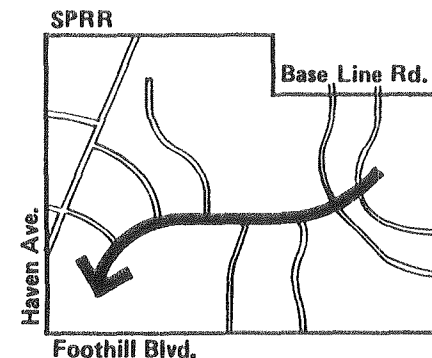


FIGURE III-10

Community Tied Together by Secondary Greenway Trail Network

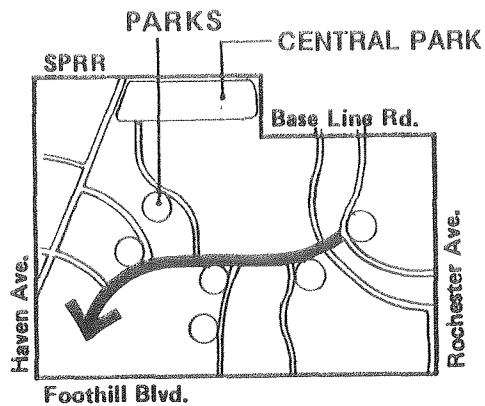


FIGURE III-11
Parks along Greenway and Trail System within Walking Distance
of All Residents

- Trail connections to neighborhoods and community subareas
- Trail access to all school sites

Each of these criteria is explained further below.

Greenway System Anchors. The greenway system is anchored at the east side of Terra Vista community by a park and elementary school site. Connections across Rochester Avenue allow residents in the adjoining development to the east to have easy access to the Terra Vista greenway amenities. At the southwest corner of the community, the greenway terminates at the community commercial development in the Town Center, at the most important crossroads of the community. The Town Center will be an attraction for the entire City of Rancho Cucamonga and will offer transit access, extended hours of activity and nightlife, and extensive shopping opportunities.

By providing direct access to destinations at either end of the greenway spine, the greenway offers the most direct access possible to either of these destinations, and many more in between, from nearly everywhere within Terra Vista. Terra Vista residents will be encouraged to avail themselves of the pleasant walking and bicycling potential of the greenway and can leave their cars at home.

Trail Connections. The trails that project from the major greenway spine connect residential areas with community uses, commercial developments, employment centers, and schools. They also link with destinations beyond the project boundaries and with surrounding employment centers. Bus stops, which will be coordinated with OmniTrans, are intended at locations where the trail system intersects with key collector streets and arterials.

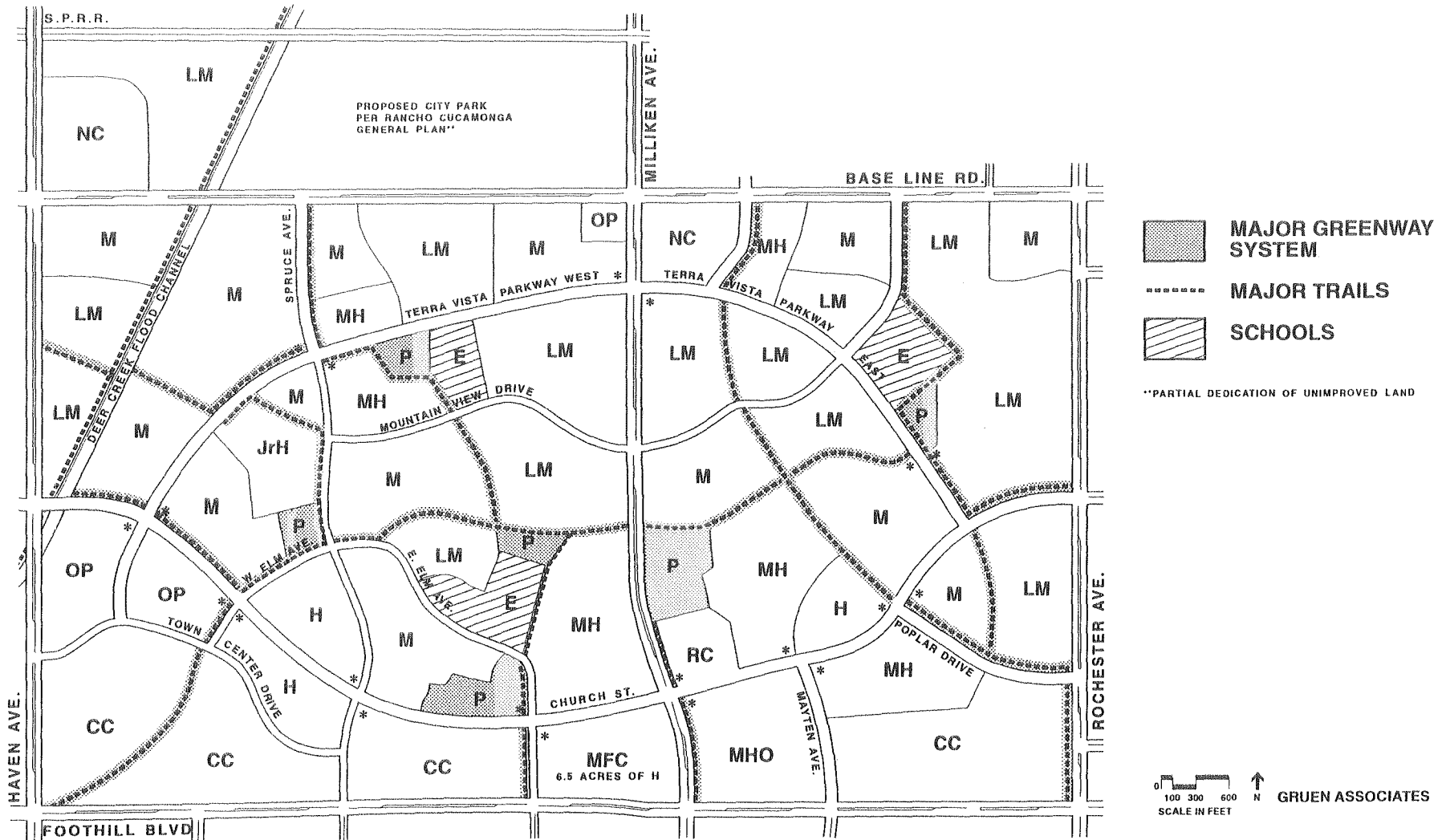


FIGURE III-12
Park and Greenway System

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Safety has been a major consideration in the design of the greenway and trail system, so as to encourage its use by adults and children alike. Conflicts between vehicle and pedestrian routes have been kept to a minimum. The primary greenway spine, for example, experiences only four at-grade road crossings along its nearly one and one-half mile length. Special safety features have been incorporated into remaining grade crossings, including special provisions to alert cyclists and motorists of upcoming crossings. The chapter on design guidelines (Chapter IV) includes more specific information on these features.

Access to School Sites. All the school sites at Terra Vista are served directly by either the primary greenway spine or trails, making these facilities especially accessible to younger community residents.

Public Parks

Public parks are an important component of the Terra Vista planned community. As indicated by the land use plan (Figure III-1), green areas, encompassing the primary greenway spine, trails projecting from this spine, a series of local parks, parklike nodes, and a portion of the proposed central City park constitute the public park system.

Public open space serves the following functions:

- Because it extends through the community, public open space ties various neighborhoods and activities together.
- It provides visual relief and contrast to urban development.

- Its accessibility to all community residents -- and, in fact, to all people regardless of where they live -- will make public open spaces the meeting places and activity centers for recreation and leisure time, providing a positive image for the City of Rancho Cucamonga.
- Because public open space will be owned by the City of Rancho Cucamonga, arrangements for the effective joint use of open space can become a matter of public policy. Furthermore, the locations of parks at Terra Vista have been selected with many factors in mind. Among these factors, some of which were noted briefly in the preceding discussion, are population densities, other recreational features and community uses, visibility and community image, and joint use.

Park functional and locational criteria are discussed below.

Linkage of Neighborhoods and Activities. As discussed in the previous section on neighborhoods of Terra Vista, public open space in the form of the greenway spine links the four community neighborhoods together. Residential developments can be oriented toward, rather than away from, the greenway system because of its open quality and landscaping. It also provides access to commercial developments, to educational and other public institutions, to employment centers south of Terra Vista, and to neighboring communities -- all of which gives public open space the role of the "community crossroads."

Access and Visual Relief. Public open space has been distributed throughout the planned community in approximate balance with the anticipated distribution of population. This means that everyone will have easy access to open spaces. In addition to being easily accessible, the open space within the community will provide attractive greenery and vistas for the enjoyment of people passing through the community, as well as those living and working in it.

Recreational and Leisure Time Activities. Public parks and nodes along the greenway system will provide opportunities for numerous recreational pursuits, both active and passive.

Park Locations to Serve All Residents. Local open space at Terra Vista has been allocated to the various neighborhoods and subareas of the community in proportion to the number of residents nearby. Based on these projected population factors, the majority of local parks and locally-oriented open space can be found in the southern two neighborhoods, since these neighborhoods tend to have higher residential densities than the two northern neighborhoods. Also in the northern half of the community, the City has proposed a central park at the northwest corner of Base Line Road and Milliken Avenue, which would serve the entire City of Rancho Cucamonga and be convenient for residents in the northern half of Terra Vista. The Terra Vista park plan includes dedication of a portion of this proposed central park. As a result, the northwest neighborhood contains considerably more total park land, in relation to its population, than other areas of Terra Vista.

Similarly, the highest population densities at Terra Vista are found at the interior of the planned community; lower population densities are at the site periphery to

conform with neighboring developments. In order to make the public parks convenient and accessible to the most community residents, local parks have been oriented for the most part to the interior of the site, in most cases adjacent to higher-density developments. Another factor in the distribution of open space is the fact that, since the major commercial/mixed use centers are located in the southern half of the community, the access routes to these attractions (i.e., the greenway spine and parks along it) should be more generous in correspondence with the number of people who will be using the facilities for access to shopping and employment centers.

Relationship to Other Recreational Features and Community Uses. Parks can be used more effectively if they are tied together into a recreational circulation network. For this reason, all the parks and nodes at Terra Vista are situated along the greenway and trail system, as noted earlier. Not only will these parks be conveniently accessible to nearby residents, but also to residents throughout the planned community and from other parts of the City.

The location of two parks in the vicinity of the recreational commercial parcel has obvious advantages in users' ability to pursue organized field sports, jogging, cycling, and other recreational activities in conjunction with their visits to recreational commercial establishments, such as health clubs. Public and community facilities may also wish to locate on the greenway system.

Visibility. As other sections of the Community Plan will show, Terra Vista has been planned to be an exceptionally attractive community and a source

of pride for the people of Rancho Cucamonga. A landscaped, parklike quality has been a major factor in fulfilling this important objective. For this reason, special consideration has gone into landscaping of the Terra Vista loop parkway roads, along which thousands of people will be exposed daily to the greenery and open space of Terra Vista. The location of several public parks and nodes along the loop parkway greatly enhances the image of the community. The parks also allow permanent views into the community and toward the mountains in many cases. Regularly spaced tree plantings along the loop parkway open up at parks to create variety and interest along the parkway for drivers.

Joint Use. Many of the public parks at Terra Vista are designed to serve dual functions, either in conjunction with school recreational programs or as part of the community stormwater management system.

- Joint use of parks and school facilities is encouraged. This provides economies for both the school and the park, since a single recreational facility can serve both the school athletic program and the public at large. Because park use is greatest during non-school hours, there is virtually no loss of park utility for the community as a result of the joint use concept.
- Some parks in Terra Vista are conceptually designed to serve occasionally a temporary role in the community's stormwater drainage system in extremely rare occurrences of heavy rainfall.

In such occurrences--and never during times that the parks would be used for recreation -- some water from the storm drainage system could be briefly detained in the lower contours of the parks. Extensive research and analysis have shown that the time the parks would be unavailable for full recreational use would be immeasurably small and that, due to excellent soil drainage characteristics in Rancho Cucamonga, the park would once again be available to serve recreational functions in just slightly more time than that required at any single-purpose park after a heavy rain.

The design of these joint use parks can be nearly indistinguishable from that of a single-purpose park. The primary differences would be slightly more contouring (which is an asset when well landscaped) and the presence of an inconspicuous, bubble-up type of inlet/outlet mechanism at the lowest point in the park. In Chapter IV, design alternatives for a joint use park are presented to demonstrate its attractiveness within the community and its versatility in meeting the recreational needs of the community and the City of Rancho Cucamonga.

Circulation

The circulation plan for Terra Vista serves dual objectives. The first objective -- to provide crucial linkages in the regional circulation system primarily for through traffic -- is met by a continuation of three arterials through the Terra Vista site: Spruce Avenue, Milliken Avenue, and Church Street (Figure III-13). At the same time, existing arterials that make up most of the Terra Vista boundary will be upgraded as the community develops. These major arterials include Foothill Boulevard, Haven and Rochester Avenues, and Base Line Road.

The second objective -- to organize access to the interior of the community -- is met by a specially landscaped, divided loop parkway (Figure III-14). The southern half of this loop coincides with the extension of Church Street through the site and provides access to two of the community's neighborhoods and to the numerous office and commercial centers between Church Street and Foothill Boulevard. The northern half of the loop road, Terra Vista Parkway, provides access to the remaining two neighborhoods. All around the loop, motorists will be exposed to the entire range of land uses in Terra Vista as well as to views of the mountains, parks and recreational amenities.

The overall circulation plan, noting the hierarchy of roadway types, is provided in Figure III-15. The paragraphs that follow focus on the individual elements that make up the circulation plan. More detailed discussions relating to the design features of the Terra Vista circulation system follow in Chapter IV.

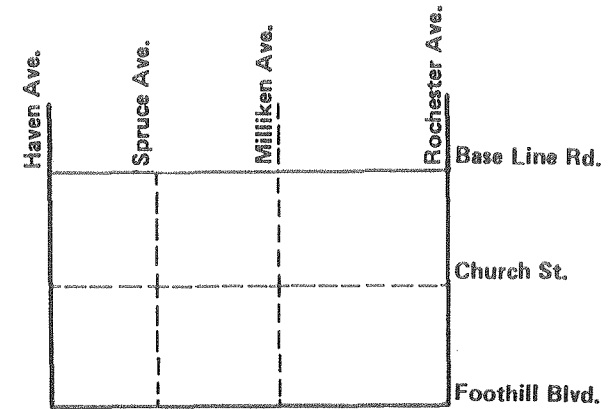


FIGURE III-13

Continuation of Arterials Through Terra Vista to Complete Regional Circulation Network

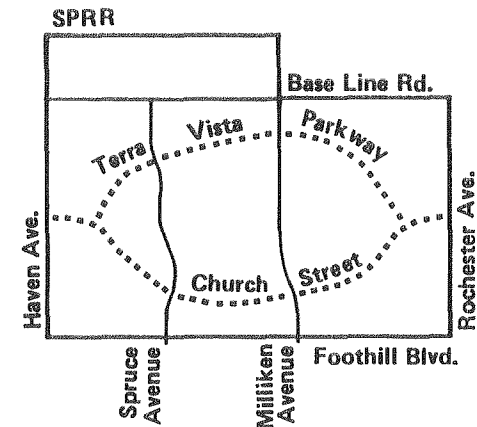


FIGURE III-14

Loop Parkway for Major Internal Circulation and Community Theme

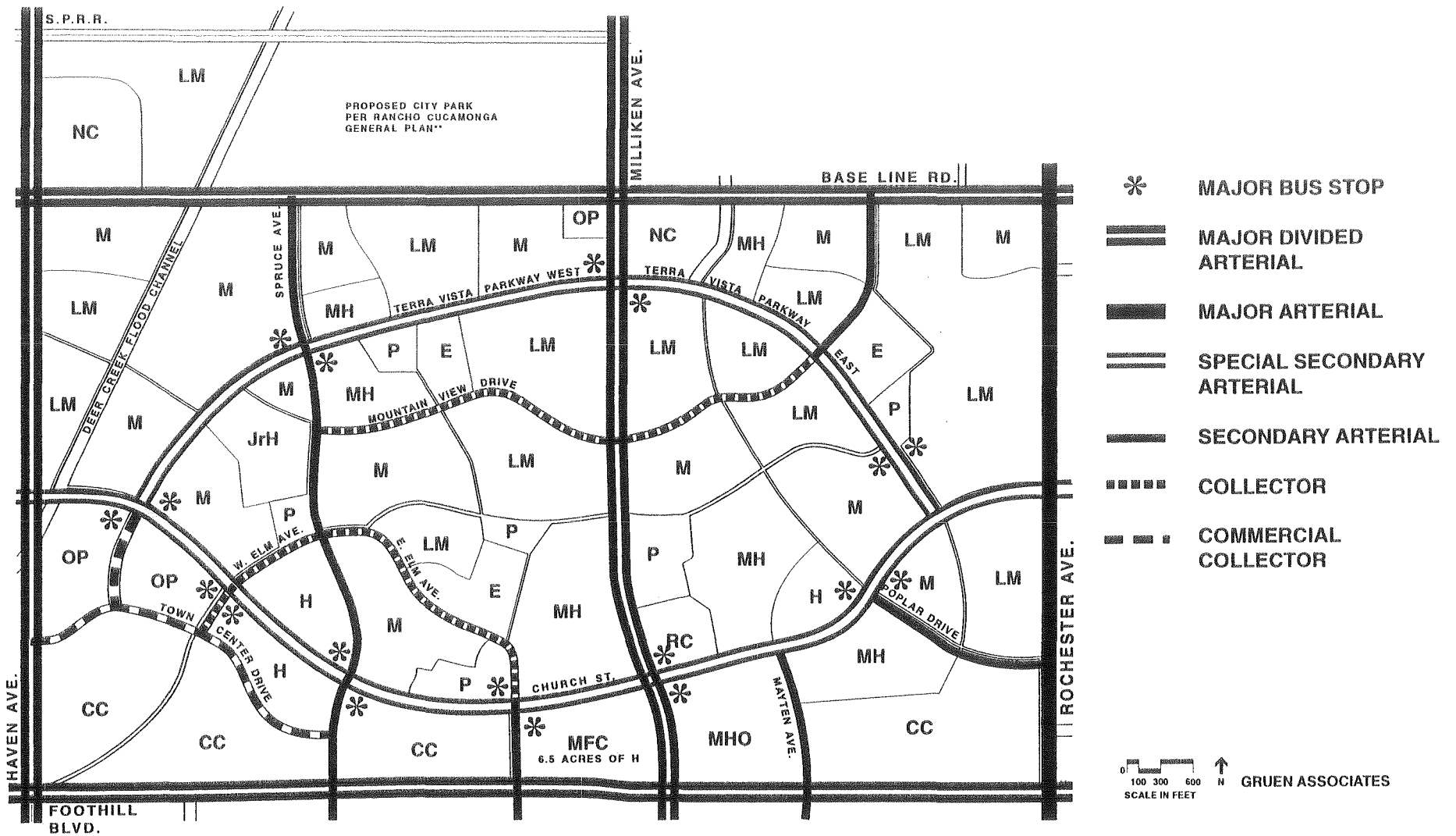


FIGURE III-15
Circulation Plan

REVISED Amendment Nos. 1, 2, 5, 6, 7 & 9
 III-16

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Roadway Types

The roadway system consists of seven classifications of streets: major divided arterial, major arterial, special secondary arterial, secondary arterial, residential collector, commercial collector, and local street. Proposed secondary arterials through the site relieve traffic volumes on the adjacent major arterials while providing a community-level looping circulation pattern. Residential collector streets provide circulation through the neighborhoods, connecting the core community uses with the residential uses.

Existing and Master Planned Routes

The existing pattern of travel in the area is a grid system of north-south and east-west arterials at approximate one-mile intervals. The paragraphs that follow profile the major arterials in the vicinity of Terra Vista today and the future roles they are to serve in the citywide context, in accordance with the Rancho Cucamonga General Plan.

Foothill Boulevard. Foothill Boulevard is the primary east-west route through the City. As U.S. Route 66, it was in the past a major interstate thoroughfare; today, as a supplement to the San Bernardino Freeway (I-10), it remains an important carrier of regional traffic.

When Rancho Cucamonga is fully developed, Foothill Boulevard will continue to function as the primary east-west arterial through the City and will serve as the hub of the City's major commercial and office development, enjoying direct access from the Devore Freeway three-quarters of a mile east of the site. Foothill Boulevard will also become the boundary between the 5,000-acre industrial complex to the

south (see Figure II-1) and the residential areas to the north.

Haven Avenue. Haven Avenue is today one of the City's major north-south routes, leading to Chaffey College and the eastern part of Alta Loma on the north. To the south, it leads to industrial developments, Ontario International Airport, and the commercial/residential/office complex proposed for the former Ontario Motor Speedway. It has a freeway interchange at the San Bernardino Freeway (I-10) two and one-half miles to the south, and will connect with the future Foothill Freeway (Route 30) three-quarters of a mile to the north. Haven Avenue is the central north-south route in the City and the geographical center of Rancho Cucamonga is in the vicinity of Haven Avenue and Foothill Boulevard.

Rochester Avenue. Rochester Avenue, which presently dead-ends north and south of Terra Vista, is planned to extend in order to provide a continuous route between the northern and southern City limits. However, unlike Haven and Milliken Avenues, Rochester Avenue will not have an interchange with the San Bernardino or Foothill Freeways and therefore will not carry as much traffic as the community's other north-south routes.

Base Line Road. Base Line Road is today only minimally improved from Haven Avenue eastward. When the City is fully developed, Base Line Road will be an important arterial, but it is not expected to be as heavily traveled as Foothill Boulevard.

Milliken Avenue. The General Plan calls for new north-south arterials to continue the existing grid pattern. Milliken Avenue is to become the major

gateway to the industrial area and will connect the I-10 Freeway on the south to the proposed Foothill Freeway and Chaffey College on the north.

Church Street. One of two new intermediate arterials recommended by the General Plan is an east-west connection from Haven Avenue to Rochester Avenue that will be continuous with the existing Church Street to the east and west. This will provide an alternative means of access to downtown and the eastern part of the City. It will also reduce ultimate traffic levels on Foothill Boulevard and Base Line Road.

Spruce Avenue. The second planned half-mile arterial is Cleveland Avenue, to be renamed Spruce Avenue, midway between Haven Avenue and Milliken Avenue. This route will provide alternative access to the industrial area and to the Town Center.

The Loop Parkway

The loop parkway is the key circulation element of the Terra Vista Planned Community. As explained above, the loop is made up of Church Street on the south and Terra Vista Parkway on the north. The loop parkway concept has many significant benefits for Terra Vista and neighboring areas, including:

Internal Access and External Traffic. The loop parkway allows Terra Vista residents to reach almost any part of the Planned Community without using the perimeter arterials around the project. Retail areas, places of employment, and community uses can all be reached inside the project boundaries via the loop. The result is that many trips will occur entirely within Terra Vista, with no traffic effect on surrounding roads or neighborhoods.

Through Traffic. Church Street extends through Terra Vista as called for by City circulation plans (see Figures III-14 through III-16). However, by swinging Church Street southward, traffic is kept away from the residential areas and the greenway at the heart of the project. The curvilinear alignment itself also encourages drivers to slow down, which should benefit not only Terra Vista but also the residential areas east and west of it. And finally, the presence of Terra Vista Parkway to the north will split traffic within and through the community into two halves, lessening the traffic that would occur on any single road.

Commercial/Mixed Use Area Access. Church Street in its loop alignment provides a very convenient second route for driving to the office and commercial areas along Foothill Boulevard. The road itself also serves as a buffer. Terra Vista residents can be expected to use Church Street, rather than Foothill Boulevard, to reach these areas for shopping, services, employment, and entertainment. Employees and other Rancho Cucamonga residents may also prefer to use the Church Street route. The result will be less traffic on Foothill Boulevard.

Transit. The loop parkway is intended to be the primary internal transit corridor for the Planned Community. Virtually all of the higher density residential areas, significant office and commercial areas, and most community uses have been located on or next to the loop parkway. This concentration of people and activity centers along the route is important to make transit service economically feasible.

Visual Identity. The Terra Vista loop parkway will signal to drivers that they are entering a special

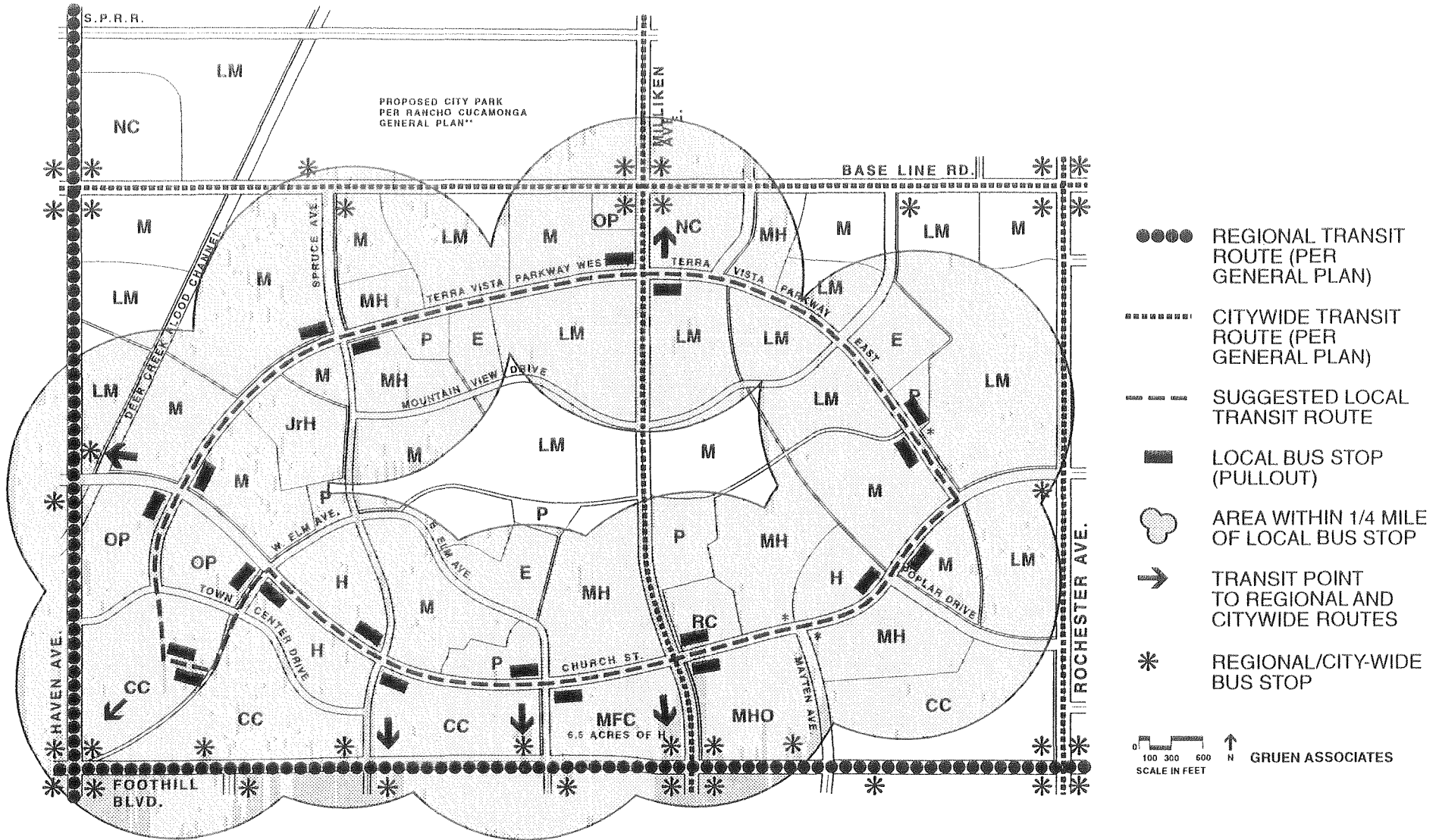


FIGURE III-16
Suggested Transit Plan

community. It will provide a unique visual experience for visitors and residents alike with distinctive landscaping treatments and a variety of land uses and building types. Special features of the parkway are noted below:

- The loop curves gently but continuously, which is itself unusual for major streets in this area and also encourages drivers to slow down and appreciate their surroundings.
- The landscaping of the loop parkway will be distinctive. The overall landscaping concept, including a raised median planted with trees, will be continuous all the way around the loop. This will reinforce a driver's perception that he is indeed on a loop that circles through the entire community. Deciduous trees will provide fall color with the change of seasons, and ornamental flowering trees in a raised median will highlight open spaces and views. Street trees will create an enclosed arbor effect in some areas, opening up in others, for a continually changing streetscape.
- A traveler around the loop will encounter the entire range of land uses in Terra Vista, including all kinds of residential neighborhoods and a variety of office parks and commercial developments. This variety alone will make the street interesting and unusual. Drivers will especially notice the many parks which come into view every few moments.

For all of these reasons, the loop parkway will be a unique road lending identity to all of Terra Vista. Both functionally and visually, it will tie the entire community together.

Transit

In addition to providing convenient, efficient, and safe routes for private vehicles, the Terra Vista Community Plan places a high emphasis on planning for public transportation. Two levels of transit service are anticipated, as shown in Figure III-16.

- The first level is a suggested internal route within Terra Vista that connects the key travel destinations within the project. As explained above, the loop parkway is intended to be Terra Vista's primary internal transit route; people and activity centers have been concentrated along the loop to the extent possible.
- The second level of transit service is the city-wide/regional network planned to operate on the major arterials bounding Terra Vista, as shown in the City's General Plan.

The regional and citywide bus routes are likely to include stops at major intersections around and through Terra Vista. The Community Plan provides for convenient connections between the internal and external routes.

Figure III-16 indicates the suggested route of local transit service around the loop parkway. Bus stops along this route would coincide with major traffic generators, such as high-density housing, schools, and commercial uses. Over 90 percent of Terra Vista is within a quarter-mile walking distance of the suggested transit stops shown on this plan. It is anticipated that, when transit is feasible, a local transit authority would provide service along this or a similar route.

While the specific routing, timing, and level of transit service cannot be determined by this Community Plan, every effort has been made to make transit a logistically and economically feasible element of the community's future.

Greenway and Trail System

In addition to the major circulation system elements described above, the greenway and trail system will serve as the backbone for the Terra Vista pedestrian circulation system. The greenway and trail system will provide convenient connection among key activity generators in the community and will also provide convenient interchange with planned transit routes. A more complete discussion of the greenway and trail system design, as well as its role in the planned community circulation system, can be found in the previous section of this chapter.

Land Uses

The various land uses at Terra Vista have been carefully arranged to foster easy access, frequent and generous open spaces, and a coherent, unified community, as well as individual neighborhood identities. The subsections that follow describe each of the land uses at Terra Vista and the planning principles that have contributed to their arrangement. Land uses are discussed in this order:

- Residential
- Commercial
- Mixed Use
- Public and Quasi-Public

Residential Land Uses

Residential land uses in Terra Vista complement the open space, circulation, commercial, mixed use, and community facilities uses. Residential use locations were determined in conjunction with other uses within the entire community as well as at the neighborhood level. In some cases, existing land uses in neighboring developments influenced land uses assigned to specific areas within Terra Vista. For example, one planning criterion was the orientation of lower-density parcels away from major arterials (which surround most of Terra Vista). However, another criterion was the location of lower-density parcels across from similar development densities in adjoining communities. The Terra Vista plan reconciles these criteria into a viable, innovative plan, placing responsiveness to environmental and existing community factors at

higher priority than rigid adherence to internal community planning criteria.

A variety of housing types and densities is planned for Terra Vista. Figure III-17 indicates the locations of the various densities. Each residential land use is indicated by a letter corresponding to the residential categories indicated in the Rancho Cucamonga General Plan. The density ranges for residential land uses and probable housing types in each density range are given in Table III-1.

Several probable housing types are identified within each category. These housing types may change due to variations in the market or new technologies and, therefore, are provided only to give an indication of the major types of housing that would conform to each density range at this time. (Note: The land use designations of this plan do not restrict the use of varying construction technologies. Mobile or manufactured homes, for instance, which are typically single family dwellings, may be developed in any appropriate land use category.)

Among the many considerations that went into the arrangement of various residential densities were:

- Compatibility with adjacent developments
- Energy efficiency and community interaction
- Relationship to public open space amenities and alternative transportation facilities
- Variety of densities within all neighborhoods

TABLE III-1

RESIDENTIAL DENSITY RANGES AND HOUSING TYPES

Category	Density Range Units per Acre	Probable Housing Types
LM Low Medium	4-8	Single-Family Detached and Attached
M Medium	8-14	Single-Family Detached and Attached, Town- homes, Garden Apartments, and Condominiums
MH Medium High	14-24	Townhomes, Garden, Apart- ments, and Condominiums
H High	24-30	Apartments and Condominiums

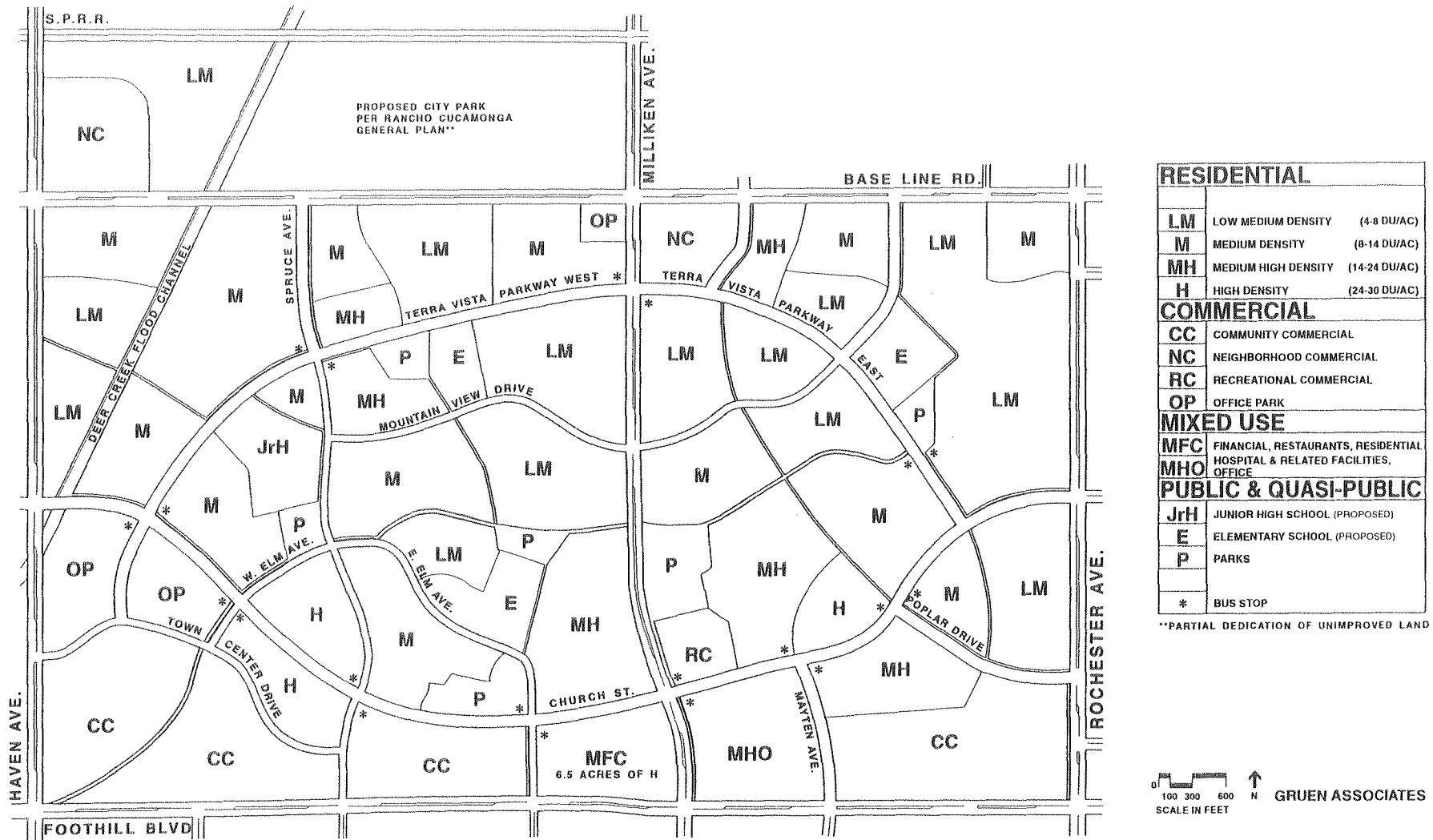


FIGURE III-17

Land Use Plan

Density Ranges of Approved Projects may vary slightly from the Plan; See "As Built Land Use Progress Plan" - Figure VI-3 on page VI-11.

Compatibility with Adjacent Developments. Lower residential densities (LM and M) have been provided at places along the periphery of the Terra Vista community adjoining existing low-density development (Figure III-18). For example, in the northeast neighborhood along Base Line Road, LM and M densities are provided in order to be compatible with similar planned densities in the Victoria planned community to the north. LM and M densities are provided opposite the existing single-family homes along Rochester and Haven Avenues (Figure III-17).

Energy Efficiency and Community Interaction.

Higher density residential developments (MH and H) have been oriented primarily to the interior of the community (Figure III-19) to be:

- Near the park and greenway system
- Near the neighborhood core areas
- On the loop parkway
- Near key intersections where transit stops would be appropriate
- Within and near mixed use parcels to promote extended hours of activity

Examples of how these location criteria have been applied can be found in each of the neighborhoods (Figure III-17):

- In the northeast neighborhood, the MH parcel is situated near the neighborhood commercial center, near a park, on the trail system, on the loop parkway, and near a recommended transit stop.

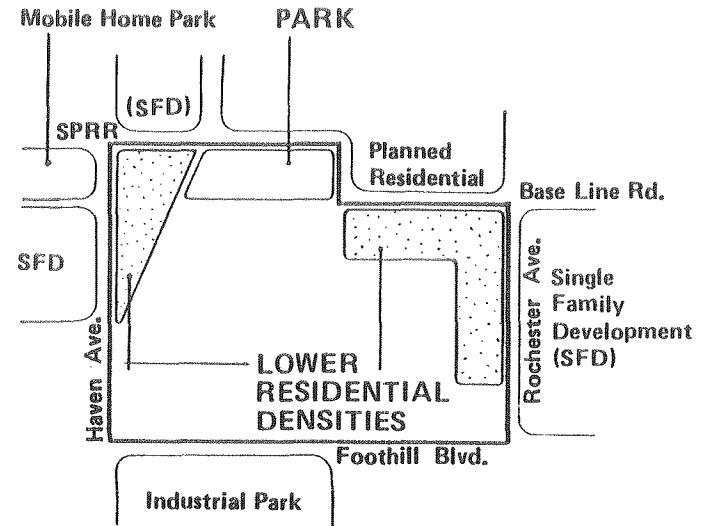


FIGURE III-18
Lower Residential Densities at Site Periphery Adjoining Existing Low-Density Housing

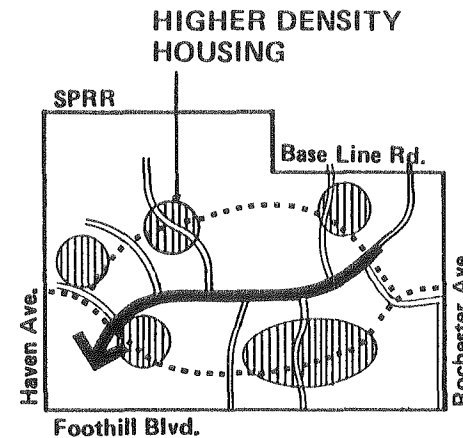


FIGURE III-19
Higher-Density Housing Locations to Promote Energy Efficiency and Community Interaction

- In the northwest neighborhood, the two MH parcels are both on the greenway/trail system as well as on the loop parkway, and close to a key intersection appropriate for transit stops.
- In the southwest neighborhood, there is one MH parcel on the greenway system, adjacent to or opposite three parks and at a key intersection. There are two H parcels, both at key intersections, and an H parcel within a major mixed-use development along the greenway and at a key intersection.
- In the southeast neighborhood, there are three higher-density residential parcels. An MH parcel is situated next to a park and at a key intersection. Another MH parcel and an H parcel are located near a key intersection and on the loop parkway.

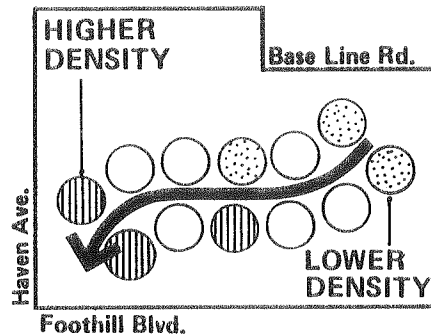


FIGURE III-20
Mix of Housing Densities along Greenway

Relationship to Public Open Space Amenities and Alternative Transportation Facilities. Since the greenway system is designed to encourage walking and cycling to various activity centers within the Terra Vista Community-- and since it is the visual focus of all the neighborhoods of Terra Vista -- a wide range of housing densities has been oriented toward the primary greenway spine. This variety will add to the range of images encountered by people using the greenway system for daily trips to employment centers and commercial facilities.

As discussed earlier, the average population density is lower in the northern neighborhoods and higher in the southern neighborhoods. As a result, overall residential densities along the greenway system are higher at the southwestern end of the greenway and lower at the northeastern end (Figure III-20).

Variety of Densities Within All Neighborhoods. Each neighborhood in Terra Vista has a mix of housing types, varying from single-family detached to multifamily housing, allowing people from many income groups and of differing lifestyles to live in Terra Vista. While the neighborhoods differ in character, no neighborhood is entirely higher density or entirely lower density.

Commercial Land Uses

Commercial land uses in the Terra Vista planned community are grouped in centers according to functional requirements to create convenient, efficient, and visually pleasing environments. Most commercial centers are situated between Foothill Boulevard and Church Street (Figure III-21); however, neighborhood and other small commercial centers can be found elsewhere in the community to serve the more localized and specialized needs both of community residents and of residents in adjoining communities.

The various commercial land uses in Terra Vista include:

- Community Commercial
- Neighborhood Commercial
- Recreational Commercial
- Office Park

Community Commercial (CC). The Community Commercial parcel at the corner of Foothill Boulevard and Haven Avenue can accommodate department store development, numerous tenant stores arranged around either an open or enclosed pedestrian mall, and community-oriented service establishments such as drug

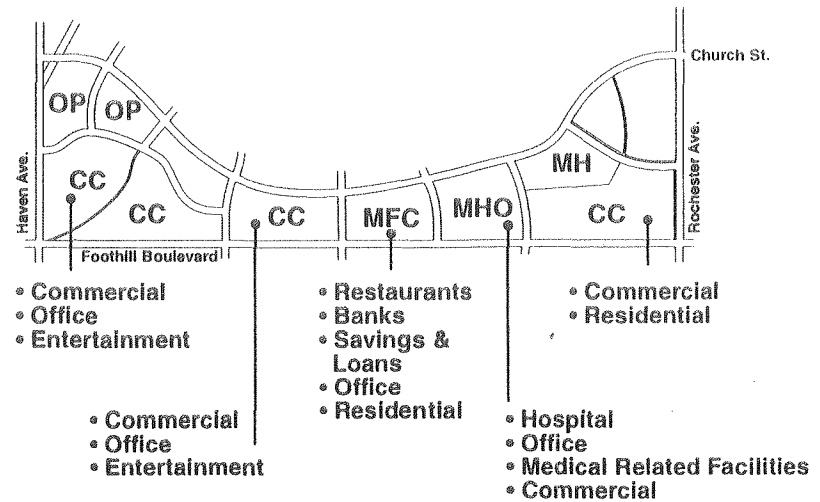


FIGURE III-21
Centers Concept Along Foothill Boulevard

stores, supermarkets, financial institutions, and other functions. In addition, the Community Commercial center can have entertainment facilities and/or restaurants. The intent is that the Community Commercial parcel will cater to a mix of uses that will make the development lively well into the night, to function as an active "people place" in service to the residents of Terra Vista and the City of Rancho Cucamonga. A more detailed description of the Community Commercial parcel in terms of design guidelines is provided in Chapter IV.

The Community Commercial parcel on Foothill Boulevard between Spruce and Elm Avenues will complement the uses found in the Community Commercial parcel to the west. Many of the same uses now found in the parcel to the west, such as restaurants, supermarkets, community-oriented service establishments and offices, as well as other community-wide tenants requiring large floor areas such as electronic stores can be accommodated on this parcel.

Neighborhood Commercial (NC). Two neighborhood commercial centers are specified in the Terra Vista Community Plan, one at the southeast corner of Milliken Avenue and Base Line Road, and another at the northeast corner of Base Line Road and Haven Avenue. The purpose of these centers is to provide sites for markets, drug stores, laundromats, dry cleaners, and other neighborhood shops and services. Their locations make both centers easily accessible by automobile from all parts of Terra Vista via the loop parkway, while offering exposure to residents in adjoining communities. The centers are near to extensions of the greenway system for easy access on foot and by bicycle.

In addition to the two Neighborhood Commercial centers, some local shopping uses are permitted within the Recreational Commercial site near the Milliken Avenue greenway underpass, and at the southeast corner of Church Street and Mayten Avenue (MOC parcel). Convenience commercial uses may occur elsewhere as discussed in Chapter V.

Recreational Commercial (RC). The recreational commercial center site is near the geographic heart of Terra Vista, adjacent to Milliken Avenue and Church Street. Facilities that will be developed on the site will cater to a combination of indoor and outdoor, recreation-oriented activities. These may include racquetball courts, a golf driving range, batting cages, a miniature golf course, tennis courts, health clubs, pro shops, swimming clubs, outdoor-oriented restaurants, lounges, health food stores, athletic equipment sales and service, and other functions. The location near the primary greenway spine (and Milliken Avenue undercrossing) makes the facilities especially

conducive to Terra Vista residents' jogging or cycling to the recreational commercial center, rather than driving. In the same vein, the presence of public parks and parkways adjacent to the center allows joint use of the public open space. In this way, team sports and spontaneous recreational activities will be fostered -- available to the public, regardless of residence in Terra Vista. Also, as noted above, the site can also accommodate some local commercial facilities.

Office Park (OP). Office Park sites are present north of Town Center Drive. The concept is that these Office Park parcels can attract professional and corporate tenants to Rancho Cucamonga by offering high-quality buildings in a landscaped, pedestrian-oriented environment with numerous amenities at close hand. Ideally, many of the people populating these office developments would also live in Terra Vista, allowing them to walk or cycle to work. A smaller Office Park site, intended for professionals serving the local area, is planned at the southwest corner of Milliken Avenue and Base Line Road.

Business Park Overlay Zone. Subject to approval of a CUP, the Business Park Overlay Zone may be used in any area otherwise zoned for office use. The purpose of the Business Park Overlay is to provide for multi-tenant projects fostering small business development. Small business users typically require a combination of office and limited storage area, with the office use expanding, over time, to fill most of the building area; for this reason, parking requirements for Business Parks shall be the same as for office parks, unless otherwise approved by the Planning Commission.

Mixed Use Center Concept

While the preceding discussions dealt with the primary thrust of development at the various commercial sites in Terra Vista, this section explores in more detail the concept of "mixed use centers." The concept capitalizes on the ability of a mixed-use center to provide an integrated environment, to respond to evolving market conditions and human needs, to offer a variety of physical development types, and to have a strong pedestrian orientation.

Integrated Environments. Mixed use centers have the ability to provide stimulating, integrated environments that include commercial, office, entertainment and leisure time, and residential developments--all clustered together into unified, highly identifiable developments.

Response to Evolving Market Conditions. Mixed use centers can respond over time to changing market conditions. Since the mixed use developments at Terra Vista will not be the first parcels in the community to be developed, the mixed use designation allows specific development at each center to vary-- within certain parameters -- in response to evolving market demands.

Types of Physical Development. The type of physical development that can occur within the mixed use parcel will cover a variety of building types catering to different specific uses. For example, one parcel may feature stores, offices, and housing all stacked together in one multi-use building. Another parcel may feature a split-level arrangement, through the use of site contouring, that puts commercial functions at the ground level facing Foothill Boulevard and residential units above the

commercial -- at a higher ground level -- oriented toward the parklike Terra Vista environment to the north. Still another parcel might contain a combination of both building types.

Pedestrian Orientation. Common to all commercial and mixed use parcels along Foothill Boulevard is the pedestrian orientation of development. A prominent pedestrian walkway along Foothill Boulevard, per the Foothill Boulevard Specific Plan, is planned to link all developments with each other. Trails link the centers with the Terra Vista greenway system, and -- via the greenway system -- with adjoining communities within the City of Rancho Cucamonga. This is described more fully in Section IV.

Specific Mixed Use Developments

While all the commercial parcels along Foothill Boulevard are "mixed use" developments, the three parcels that make up the boulevard frontage from just west of Milliken Avenue eastward to Mayten Avenue feature an even broader spectrum of function than the parcels to the west. The Terra Vista Plan (Figure III-17) designates these parcels as MFC (mixed use financial facilities, restaurants, residential), MOC (mixed use office, commercial, medical facilities, residential), and MHO (mixed use hospital, office, commercial, residential). These designations represent a unique focus for each center, as the following profiles describe:

MFC Parcel. The MFC parcel, immediately west of Milliken Avenue, will feature a broad mixture of financial institutions (banks, savings and loans, and brokerages), restaurants (to function as the "restaurant row" of Rancho Cucamonga), and residential development. The physical arrangement of the site will provide parking at the center, hidden from direct view of motorists on

Foothill Boulevard when all buildings are completed. The central parking will be jointly used by businesses and luncheon restaurants during the day and by dinner restaurant patrons in the evening; this joint use allows there to be less overall parking than would be required without a mixed use arrangement. The residential portion of the parcel will be oriented to the northern portion of the site to take advantage of park views. A loop pedestrian pathway will link all developments in the MFC parcel together and with adjoining parcels.

MHO Parcel. The MHO parcel, located east of Milliken Avenue, is planned as a medical park to accommodate hospitals, other specialized health care facilities, medical offices, and other office/commercial uses. Through coordinated site planning and a central pedestrian network, these facilities can function together in an integrated campus environment. As explained below, the campus can be extended eastward to accommodate expansion. The higher density housing surrounding this site makes it an extremely convenient location for both health care consumers and providers.

Summary. As the previous discussions have noted, mixed use centers offer:

- A variety of activities and opportunities.
- A lower need for use of the automobile, since numerous tasks can be accomplished without the need for intermediate car trips.
- More stimulating living, working, shopping, and leisure time environments with extended hours of activity and a variety of people intermixing.
- A human scale, with a tendency toward the clustering of various sizes of buildings.
- A special image and identity that reflects well upon both Terra Vista and the City of Rancho Cucamonga.

More detailed descriptions of the various mixed use and commercial centers along Foothill Boulevard are provided in Chapter IV.

Public and Quasi-Public Facilities

Public and quasi-public facilities in Terra Vista include the proposed junior high and elementary schools, the hospital, parks, and community uses.

Schools. Coordination with the Central School District has resulted in allocating to that school district two elementary school sites, each adjacent to a park, and one intermediate school site. One elementary school site is allocated to the Etiwanda District.

Hospital. Hospital development is planned at the northwest corner of the MHO parcel. Its location was selected to allow easy access from both Foothill Boulevard and Milliken Avenue and from Church Street in order to provide better medical service to all of Rancho Cucamonga and the West End at large. Additional health care facilities may be developed in the adjoining MOC parcel. Within the context of the mixed use centers concept, the developers of Terra Vista are confident that these hospital facilities can be a source of medical security to residents of Rancho Cucamonga as well as a good neighbor.

Parks. The parks of Terra Vista were described previously as part of this chapter under "The Parks and Greenway System." The General Plan proposes a city central park in the area north of Base Line Road, east of Deer Creek. This Community Plan proposes development of most of this site in other uses, as explained in Chapter VI. If the site is acquired by the City, however, its development as a park would be governed by the design guidelines given in Chapter IV.

Community Uses. Community facilities in Terra Vista may include civic facilities, fire stations, day care centers, libraries, religious institutions, and other functions of community support. It is expected that such uses may be incorporated into residential, commercial, and public areas throughout Terra Vista, contributing to the fabric of the community. It is likely that clusters of community facilities will be situated along the greenway system or along the loop parkway. These locations would lend greater identity to the facilities as key features of the Planned Community and ease accessibility by foot or by bicycle.

One of the benefits of clustering community facilities together or with other uses is the possibility of sharing parking areas. Since it is unlikely that all facilities will be used at once, the parking provided for one facility can serve the other, minimizing the total amount of parking area in the Terra Vista community.

Primary opportunities for parking consolidation among community facilities include churches sharing with commercial developments, local civic facilities sharing with schools, and similar situations.

Conservation of Resources

Conservation of resources is central to the Terra Vista Community Plan. Terra Vista is designed to make good use not only of energy but of other natural resources as well. Some of the highlights of conservation in Terra Vista are:

- A unique and extensive greenway and trail system that allows safe and pleasant pedestrian and bicycle travel and hence makes non-automobile travel a viable alternative.
- Land use patterns that combine or link related uses so that either travel is not necessary or one trip can serve several purposes.
- A land use plan that makes efficient use of roads, utilities, and other infrastructural elements, as well as the land.
- A land use distribution and both roadway and pedestrian circulation systems designed to be conducive to efficient transit service, including planned transit stops at key points in the community.
- Energy savings through site planning, architectural design, and landscaping, as described in Chapter IV.

Statistical Summary

Table III-2 summarizes in statistical form the intended land use and density allocations for the Terra Vista Planned Community.

The acreages contained are to the nearest tenth of an acre based on planimeter readings. The plan contains 8,120 dwelling units with a total planning area of 1,321 acres.

Table III-2
LAND USE SUMMARY

December 31, 1989
Revised January 4, 1996

Amendment #1, #2, #3, #5, #6, #7, and #9 Revised to Correspond to Land Use Map (Figure III-17, p. III-23)

LAND USE DESIGNATION	Gross Acres (AC)	Midrange Dwelling Units/ Gross Acre (DU/AC)	Number of Dwelling Units (DU)	Estimated Persons/ Household	Estimated Number of Persons
RESIDENTIAL					
Low Medium Density (4-8 DU/AC)	310.3	6	1,825 ¹	3.43	6,259
Medium Density (8-14 DU/AC)	280.3	9	2,700	2.58 ²	6,966
Medium High Density (14-24 DU/AC)	96.0	19	1,824	1.52	2,773
High Density (24-30 DU/AC)	<u>76.5</u>	27	<u>2,066</u>	1.53	<u>3,161</u>
Subtotal (Residential)	763.1		8,337		18,699
COMMERCIAL MIXED USE³					
Community Commercial	159.5				
Neighborhood Commercial	26.0				
Office Park	36.4				
Recreational Commercial	9.7				
Mixed Use ⁴ - Financial, Commercial, etc.	16.8				
Mixed Use ⁴ - Medical, Office, etc.	<u>21.0</u>				
Subtotal (Commercial/Mixed Use)	269.4				
QUASI-PUBLIC AND PUBLIC⁵					
Hospital	10.0				
Schools	46.0				
Central Park (Proposed by General Plan) *	99.2				
Parks and Trails ⁸	51.9				
Special Landscape (Loop Median)	4.9				
Flood Control/Recreation	<u>12.4</u>				
Subtotal (Quasi-Public and Public)	224.4				
MAJOR HIGHWAYS⁸					
	64.1				
TOTAL	1,321.0		8,334		18,851

FOOTNOTES FOR STATISTICAL SUMMARY
TABLE III-2

- ¹ It is assumed that a portion of the land shown in residential use will be developed in community uses through the process described elsewhere in this plan. For this reason, 6.4 acres of Low Medium Density and 3.0 acres of Medium High Density which appear on the Land Use Plan are excluded from the residential dwelling calculations on the previous page.
- ² Household size for Medium Density residential is an arithmetic average based on the estimated distribution of types of dwellings to be built. See Chapter VI of the Terra Vista text.
- ³ Descriptions of commercial land uses indicate the general type of development considered. Actual permitted land uses are enumerated in Chapter V of the Terra Vista text.
- ⁴ Commercial acreages in Mixed Use parcels exclude residential uses, estimated to total 6.5 acres of High Density.
- ⁵ Specific locations for community uses are not determined by this plan because the site size and location desired will vary depending on the user. However, it is estimated that 9.4 acres not included in the tabulation of public and quasi-public acreage will be developed in community uses. See Note 1 above.
- ⁶ Major highways acreage includes half-width rights-of-way for Foothill, Haven, Rochester, Base Line east of Milliken, and Milliken north of Base Line*, and full-width rights-of-way for Milliken south of Base Line and Base Line west of Milliken.
- ⁷ With density bonuses a total of 9,338 units are permitted in Terra Vista.
- ⁸ Park and trail acreage of 51.9 shown on the Land Use Summary and Land Use Plan (pages III-23 and IV-15) exceeds the 47.86 acre requirement (56.06 acres per page VI-3 less 8.2 acres private open space credit per the Park Implementation Plan) by approximately four acres. Trail widths and locations may change slightly as developments proceed through the planning process, and a portion of the park land shown on Milliken Avenue may be converted to another "community use" (a YMCA), but in no event will public park, greenway and trail acreage fall below 47.9.
- * now a part of City Park.

IV Design Guidelines

IV Design Guidelines

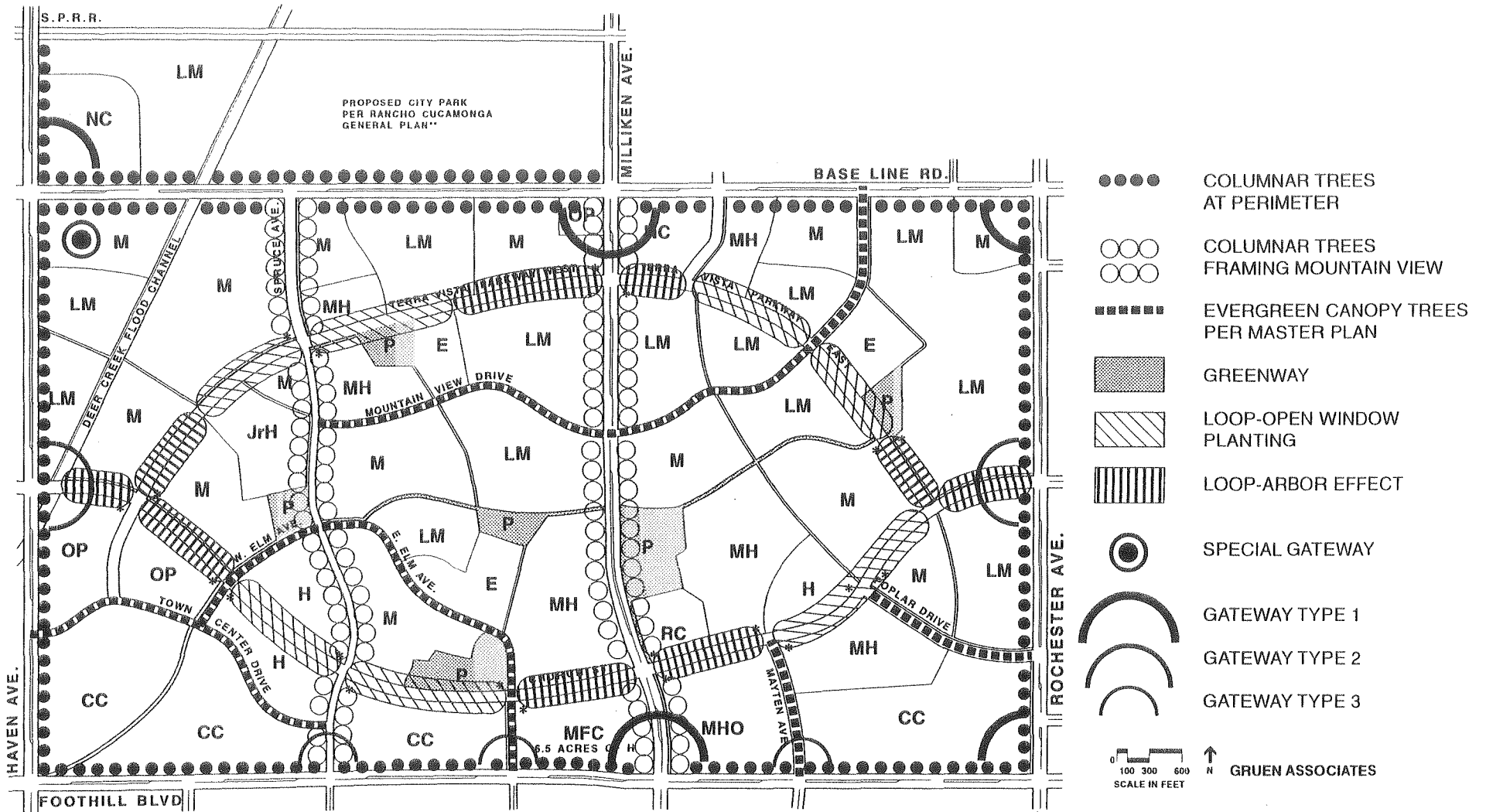
Design guidelines and criteria have been established for the Terra Vista community in order to assure that a wide range of individual developments can blend harmoniously. Guidelines, rather than a rigid set of requirements, have been incorporated into the Community Plan to encourage variety and innovation within the framework of an established community image.

The guidelines focus on design elements that are special to the Terra Vista Planned Community:

- Landscaping
- The Greenway and Parks System
- Circulation System
- Residential Development
- Foothill Boulevard Commercial and Mixed Use Development
- Energy and Resource Conservation

Landscaping Design Guidelines

The landscape treatment will establish the overall character and form the dominant image of the Terra Vista community. Therefore, the landscape guidelines and criteria are most important to the design of the community. Thoughtful landscaping will provide visual continuity and embellish the environment.



SPECIAL LANDSCAPING TREATMENT DESCRIBED IN FOOTHILL GUIDELINES

FIGURE IV-1

Landscape Plan

REVISED Amendment Nos. 1, 2, 5, 6 & 7

IV-2

The primary objectives of the landscape framework are to:

- Reinforce the community's identity
- Enhance the visual quality of the project area
- Provide a pleasant environment for living areas, recreational facilities, driving, walking, and other activities in the community
- Screen service yards, and parking
- Provide buffering
- Integrate Terra Vista into the City of Rancho Cucamonga

The Landscape Plan (Figure IV-1) presents an overview of the varied landscaping treatments found in Terra Vista. This conceptual plan can be used in conjunction with the more detailed cross-sections and plans presented on the following pages.

Landscape treatments for the following locations and conditions are discussed in this section:

- Entrance gateways
- Streets
- Buffer and edge conditions

Special discussions later in this chapter provide landscape guidelines for:

- The greenway system
- Foothill Boulevard commercial and mixed-use areas

Entrance Gateways

As indicated in Figure IV-1, there are numerous gateways to the Terra Vista community. Nine of these entrances will receive special treatment to identify the planned community. In some locations along Town Centre Drive, the Terra Vista community sign is used in combination with a sign identifying commercial developments. Four types of treatment will be used. Each of these gateway types would receive treatment appropriate to its importance, as follows:

Special Gateway. At the southeast corner of Haven Avenue and Base Line Road, the gateway will be considerably enlarged, to provide a parklike passive recreation node. (No park credit will be requested for this area.)

Type I Gateways. Type I gateways are found at the following five intersections:

- Along Foothill Boulevard at Milliken and Rochester Avenues
- Along Base Line Road at Haven, Milliken, and Rochester Avenues

Type I gateways will occupy triangular plots of land at the intersections (Figure IV-2). Gateway landscaping is planned to extend approximately 30 feet along each of the two intersecting streets. Within this area will be theme landscaping and a permanent monument sign with the Terra Vista name and symbol.

Type II Gateways. Type II gateways are found at the following two locations:

- Along Haven Avenue at Church Street
- Along Rochester Avenue at Church Street

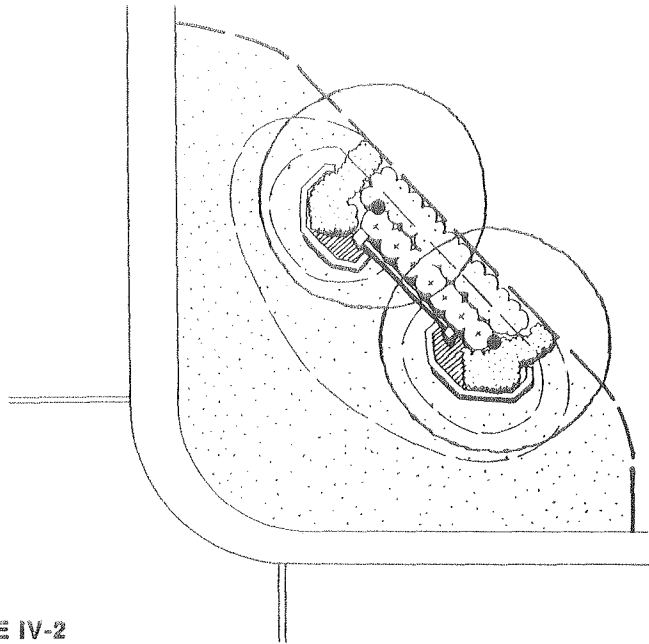


FIGURE IV-2
Concept for Type I Gateway

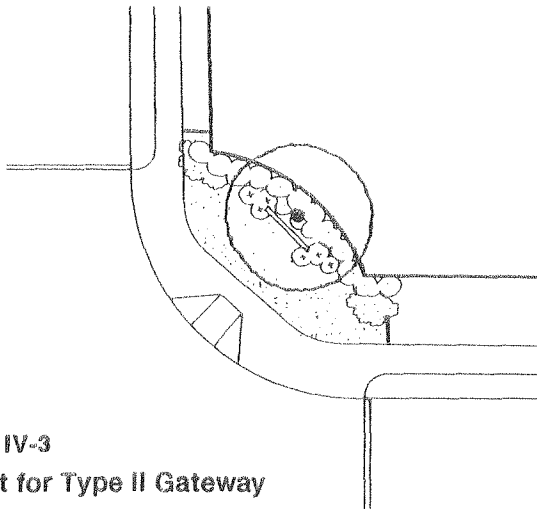


FIGURE IV-3
Concept for Type II Gateway

Type II gateways will repeat the theme landscaping found at the Type I gateways (Figure IV-3). It consists of a low sign with brick pilaster.

Type III Gateways. Type III Gateways are found at the following two locations:

- Along Foothill Boulevard at Spruce and Mayten Avenues.

Type III gateways are similar to Type II gateways except they are smaller and have no pilasters. These Type III monuments along Foothill may double as project identification monuments, if "Terra Vista" is included in the project name as in, e.g. "Terra Vista Corporate Center".

Streets

Major Arterials at Perimeter of Project Site. Typical edge conditions along perimeter arterials are shown in Figures IV-4 through IV-7. The perimeter arterials--Foothill Boulevard, Base Line Road, Haven Avenue, and Rochester Avenue--have been designed as special boulevards with similar columnar or conical trees planted to define the boundary of Terra Vista. Behind these columnar or conical trees, the planting materials will vary depending on the adjacent land use.

The boulevards generally border residential uses with the exception of commercial areas along Foothill Boulevard. The basic cross-section for Foothill Boulevard is shown in Figure IV-4. In commercial areas, parking will be set back from the curb a minimum of 28 feet with an average setback of 43 feet. Buildings will be set back from the curb a minimum of 38 feet and an average of 43 feet. The concept behind the combination of minimum and average setbacks is to promote variety along the boulevard, with landscaping varying accordingly from one center to another.

COMMERCIAL/OFFICE/MIXED USE

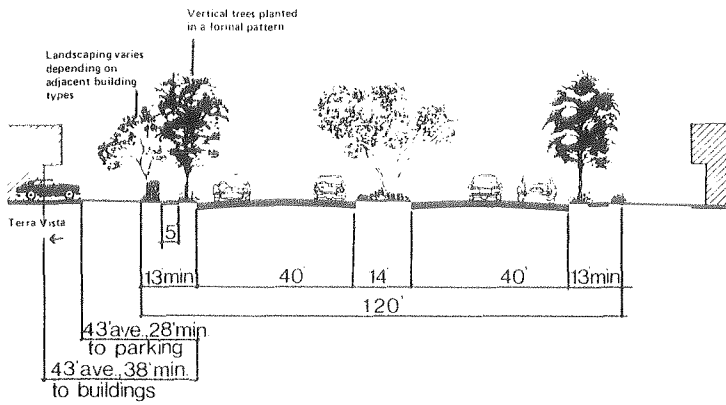


FIGURE IV-4
Landscaping and Edge Conditions on Special Boulevards Adjacent to Commercial

ADJACENT TO CONVENTIONAL SINGLE FAMILY

ADJACENT TO CLUSTERED MULTI-FAMILY

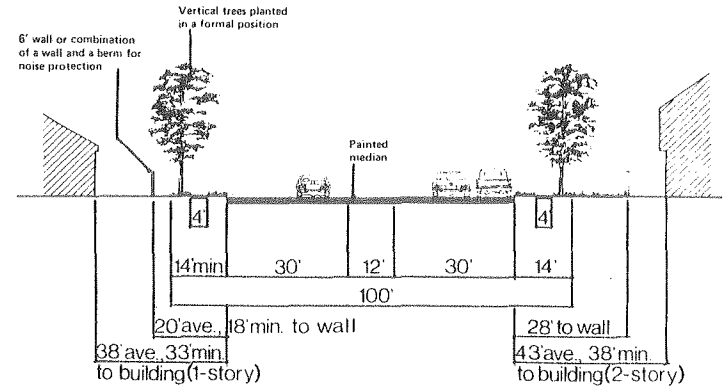


FIGURE IV-6
Landscaping and Edge Conditions on Rochester Avenue Adjacent to Residential

ADJACENT TO CONVENTIONAL SINGLE FAMILY

ADJACENT TO CLUSTERED MULTI-FAMILY

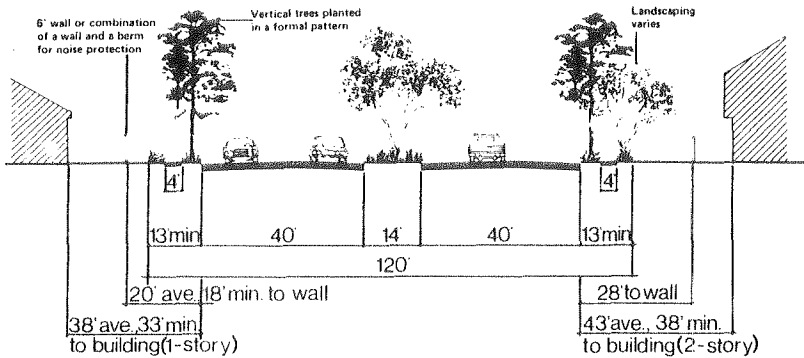


FIGURE IV-5
Landscaping and Edge Conditions on Haven Avenue and Base Line Road Adjacent to Residential

(See Foothill Boulevard section of this chapter for more details on landscaping adjacent to individual parcels.)

Along most major arterials adjacent to residential land uses (Haven, Milliken, and Rochester Avenues and Base Line Road), noise protection for residential parcels will be necessary. Noise protection can be handled in several ways, the most common way being a solid wall at the property line. In Terra Vista, the buffering concept for major arterials utilizes wall treatments in combination with landscaped setbacks to create a pleasant and varied environment for travelers on the main arterial (Figure IV-7).

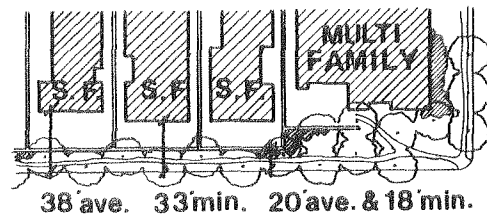
Where conventional single-family development occurs on major arterials, a six-foot solid wall is recommended

at the property line for noise protection, for privacy, and to allow for potential swimming pool construction. The average wall setback in these cases is 20 feet, with a minimum of 18 feet. Consistent wall materials will be used on major arterials. Setbacks for 1-story conventional single-family homes will be a minimum of 33 feet and an average of 38 feet from the curb; for 2-story homes, minimum and average setbacks will be 38 and 43 feet, respectively.

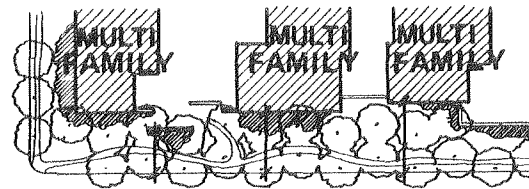
Where multifamily or clustered developments adjoin major arterials, the use of less continuous walls is preferred (Figure IV-7.) Minimum setbacks for such buildings are the same as those for conventional development. Independent walls will have a minimum setback of 28 feet from the curb, creating an open, landscaped street scene.

CONVENTIONAL SINGLE-FAMILY 1-STORY BUILDINGS

STRUCTURE TO CURB:
38' ave. & 33' min
WALL TO CURB:
20' ave. & 18' min.



38' ave. 33' min. 20' ave. & 18' min.



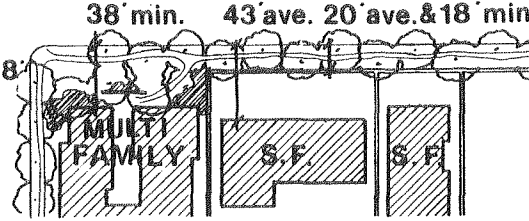
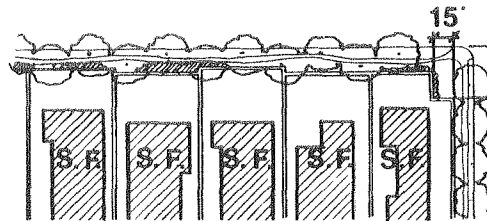
28' min. 38' min. 43' ave.

CLUSTERED MULTIFAMILY 2-STORY (TYPICAL)

STRUCTURE TO CURB:
43' ave. & 38' min.
WALL TO CURB: 28' min.

MULTIFAMILY OR SINGLE-FAMILY CORNER CONDITION

STRUCTURE OR WALL TO CURB: 38' AT CORNER



38' min. 43' ave. 20' ave. & 18' min.

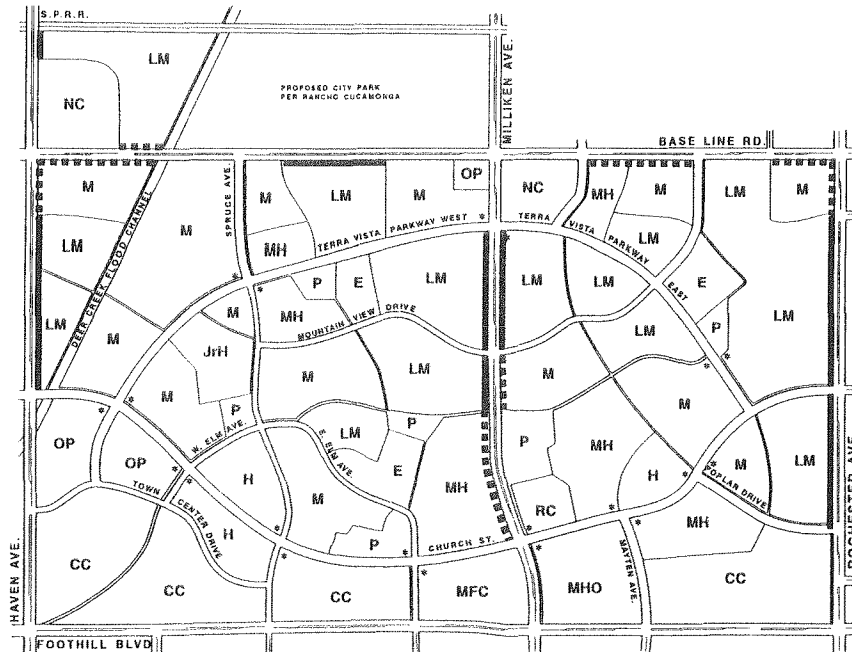
CONVENTIONAL SINGLE-FAMILY 2-STORY BUILDINGS

STRUCTURE TO CURB:
43' ave. & 38' min.
WALL TO CURB:
20' ave. & 18' min.

FIGURE IV-7

Edge Conditions to Major Arterials Adjacent to Residential

Potential locations for these single family and multi-family treatments are shown in Figure IV-8. As this figure indicates, it is anticipated that the multifamily treatment will predominate along major arterials. While the treatment in any given location may vary from that indicated in Figure IV-8, depending on the type of homes developed, it is the intent of the plan that a balanced mix of both treatments will occur on major arterials.



LEGEND:

- Solid Wall 20' ave., 18' min. from curb
- Wall Setback min. 28' from curb

FIGURE IV-8

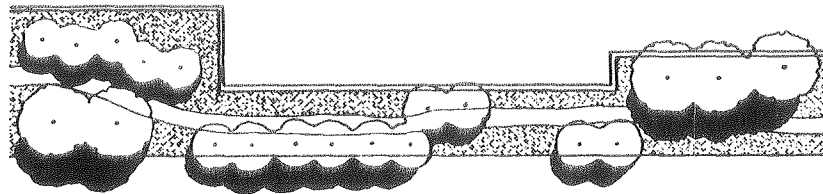
Likely Location of Edge Treatment along Major Arterial

One of the major objectives of the Terra Vista landscape guidelines has been to promote variety. This will be achieved by:

- Varying setbacks for both walls and buildings
- Gently meandering sidewalks with alternating plantings of tall, columnar or conical trees in conjunction with the sidewalk curvature
- Providing opportunities for ample landscaping and berming
- The staggering of walls at single-family residential developments to create shadow lines and to permit additional landscaping (Figure IV-9).
- Varying wall heights where permitted by slope conditions (Figure IV-9)

In addition, special consideration has been given to setbacks where the major arterials intersect collector or secondary roads. At these locations, entrance nodes will be created--and visibility of the intersection improved--by additional setback requirements. Within 15 feet of the right of way of the intersecting street, the minimum setback on the major arterial for both walls and buildings will be 38 feet from curb (see Figure IV-7).

Interior North-South Arterials. Milliken and Spruce Avenues (Figures IV-10 and IV-11) offer the opportunity to provide the motorist with views of the mountains. The landscaping concept capitalizes on this opportunity. On Milliken Avenue and Spruce, columnar/conical-shaped trees will be planted to frame the mountain views seen when traveling north.



Plan of Staggered Wall



Elevation of Staggered Wall

FIGURE IV-9
Possible Wall Treatments along Major Arterials

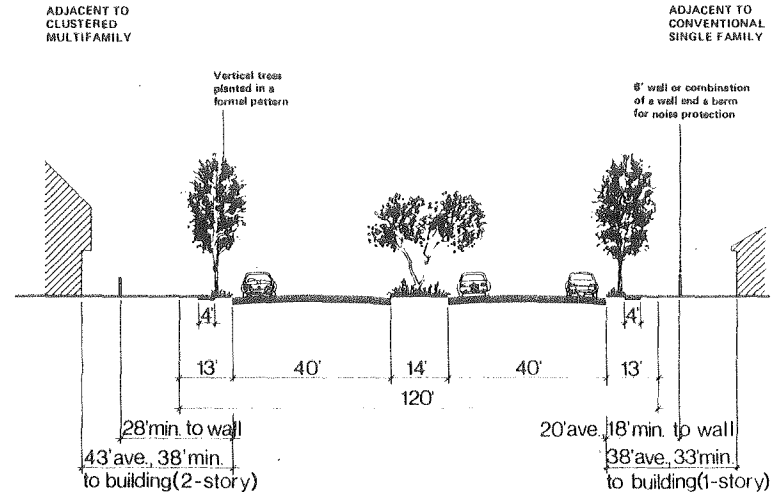


FIGURE IV-10
Landscaping and Edge Conditions on Milliken Avenue
Adjacent to Residential

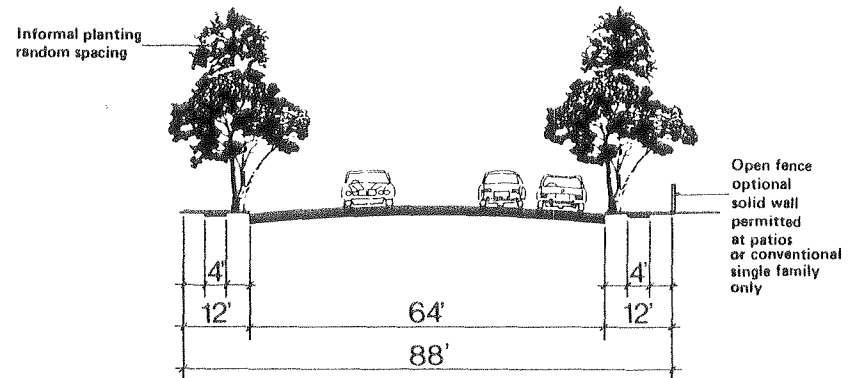


FIGURE IV-11
Landscaping and Edge Conditions on
Spruce Avenue

The columnar parkway trees will be repeated in the landscaped median. Setbacks for walls and buildings are similar to those for Haven Avenue and Base Line Road.

Along Spruce Avenue, the streetscape is treated differently due to the curving right-of-way. The evergreen edge undulates and has breaks to create small pockets of trees and green spaces. Along this route will be primarily medium- and higher-density housing whose lawns will merge with the parkway.

Church Street/Terra Vista Parkway Loop. The loop parkway (Figure IV-1) is the major circulation link connecting the neighborhoods of Terra Vista. To reinforce this link, the loop streetscape has a uniform landscaping treatment. The concept calls for alternating segments of enclosed canopies over the roadway and "windows" to park and open space areas, as shown in Figure IV-1.

A double-arched or arbor effect created by canopy-shade trees planted in the median and parkways is symbolic of the vineyard setting. This condition occurs at entrances to the loop parkway from major arterials, where the enclosed effect will create a sense of entry into Terra Vista. These canopies also provide shade, buffer adjacent residential uses, and provide a visual backdrop to the mountains (Figure IV-12).

The more open segments of the loop parkway consist of a cross-section of flowering trees in the median and a combination of deciduous and evergreen trees on the roadside to reflect the change of seasons. These segments are generally on the east-west portions of the loop, highlighting the presence of parks and allowing winter views of the mountains through the landscaping. Figure IV-13 illustrates this landscape treatment.

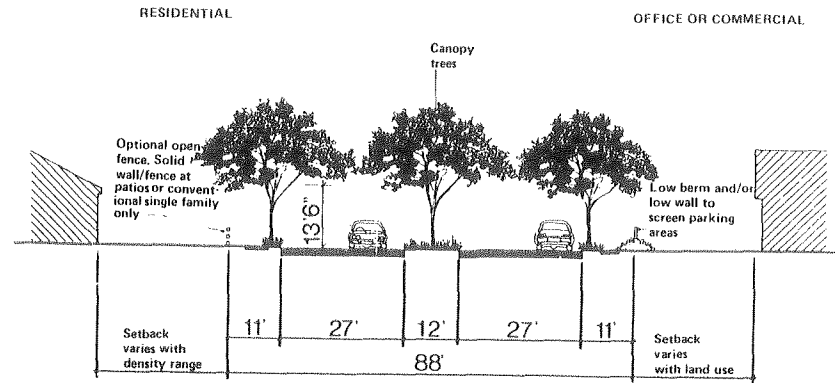


FIGURE IV-12
Landscaping and Edge Conditions on Loop Parkway-Arbor Effect

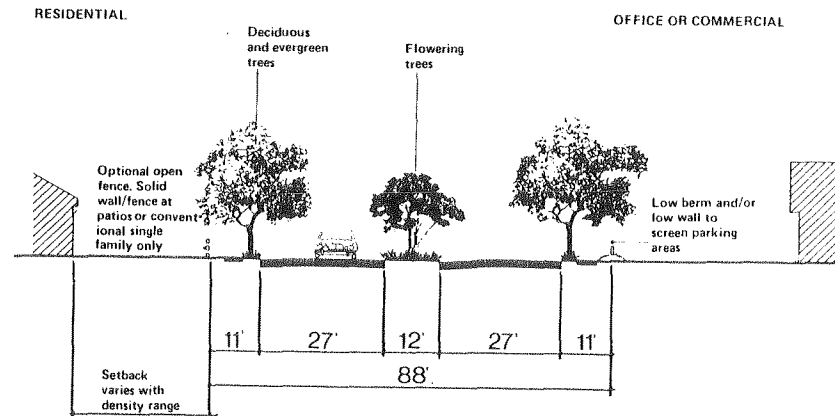


FIGURE IV-13
Landscaping and Edge Conditions on Loop Parkway-Window Effect

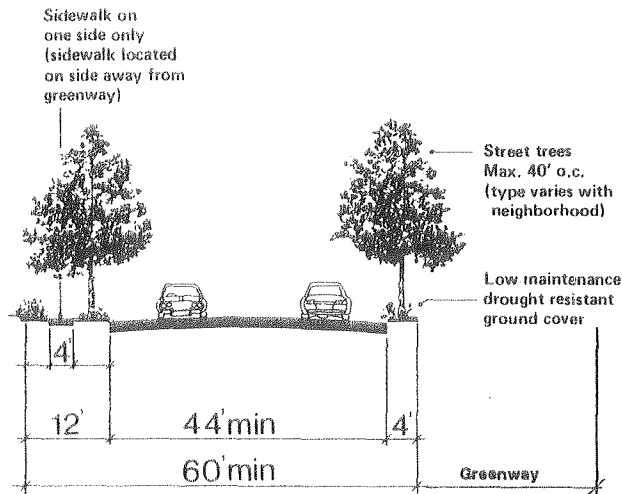


FIGURE IV-14
Landscaping and Edge Conditions on Collector
Adjacent to Greenway

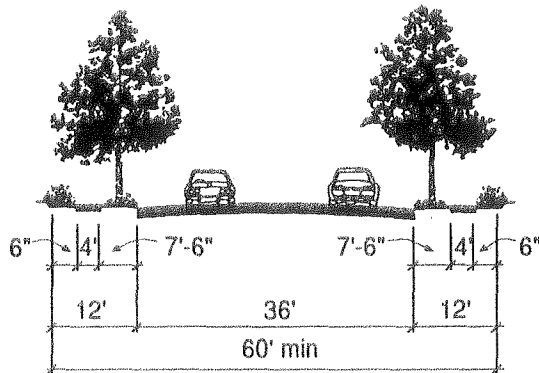


FIGURE IV-14A
Local Public Streets in Residential Areas

Setbacks along the loop parkway vary according to the adjacent land uses. The landscape concept for both medium-high and high-density residential uses and parks along the loop parkway calls for lawns to merge with the landscaping of the parkway. For this reason, solid fences and walls at the property line are permissible only occasionally at private patios, office, or commercial areas; to screen views of parking areas; or where conventional single family development occurs. Plantings or low berms at the property line, rather than walls, are preferred for screening purposes.

Neighborhood Collectors. Landscaping along the four major collectors, one for each neighborhood, will help establish neighborhood character and identity. One distinctive street tree will be used for each collector. Sidewalks will be provided only on the side of the collector opposite the greenway in order to provide maximum landscaped area (Figure IV-14). Sidewalks will be provided on both sides of collectors in cases where a greenway does not run approximately parallel to the collector. Although collectors will not generally be used for direct means of access to individual lots, this condition may occasionally be encountered.

Local Streets in Residential Areas. Local residential streets will have a variety of landscape treatments to set the theme for the individual area (Figure IV-15). Sidewalks may be on either one side (on private streets) or both sides. Wherever a four-foot sidewalk is adjacent to the curb, it is understood that the four-foot dimension will be measured from the back of the curb. For setbacks from local streets, refer to the individual land use discussions in Chapter V. Local public street design will conform to City of Rancho Cucamonga standards (Figure IV-14A).

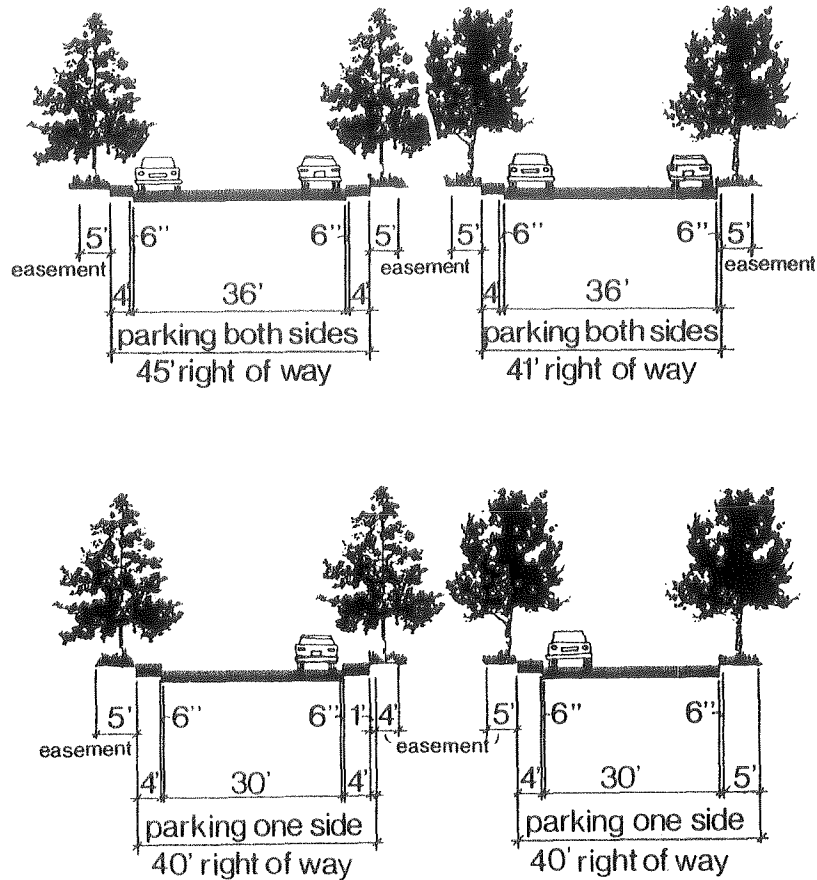


FIGURE IV-15
Landscaping of Local Streets

Buffer and Edge Conditions

The Terra Vista landscape concept affects more than the visual and recreational aspects of the community. Landscaping and open spaces also are used in buffer and edge conditions to enhance relationships between land uses and to soften the edges of the community.

The need for extensive buffering devices is minimal in Terra Vista, since land use adjacencies have been planned for high compatibility. In general, adjacent residential land use parcels are identical in density category or differ by only one density category, unless separated by landscape elements, trails or streets.

However, landscaping as well as fences and walls will offer an appropriate buffering for common conditions throughout the community. The residential development section of this chapter offers illustrations of many of these conditions. This section covers the following conditions:

- Buffer between residential and neighborhood commercial
- Buffer between residential and community commercial
- Buffer between uses in mixed-use parcels
- Edge conditions along Deer Creek and the Southern Pacific Railroad

Buffer Between Residential and Neighborhood Commercial. A minimum of ten feet of landscaping and a six-foot solid wall will buffer residential from neighborhood commercial uses (Figure IV-16).

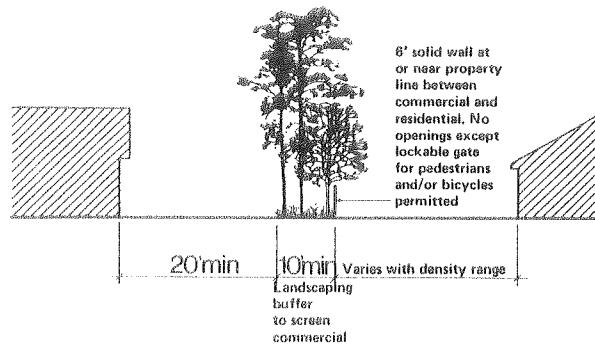


FIGURE IV-16

Buffer between Neighborhood Commercial and Residential

Buffer Between Uses in Mixed-Use Parcels. Mixed-use parcels require less extensive or no buffering between land uses due to their integrated designs and their compatibility of functions. Residential land uses are permitted in the MFC mixed-use parcels along Foothill Boulevard. Residential units in this parcel may be oriented alongside commercial and/or other uses; alternately, the residential units may be stacked on top of these other uses vertically. Buffering techniques for more conventional horizontal arrangements are illustrated in Figures IV-17 through IV-19.

Buffer Between Residential and Community Commercial. Extensive buffering is required between residential and community commercial users. The intent is to ensure that residents will not be impacted by undesirable elements in the back of a commercial center, such as the unsightly view of roof-top equipment, the light and glare from the parking area, or the noise or odor generated from commercial activities related to truck traffic, loading and unloading. For residential structures, a minimum 35-foot building and landscape setback is required from any property line that abuts commercial use. Where commercial use abuts residential use, a minimum 20-foot landscape setback is required from the

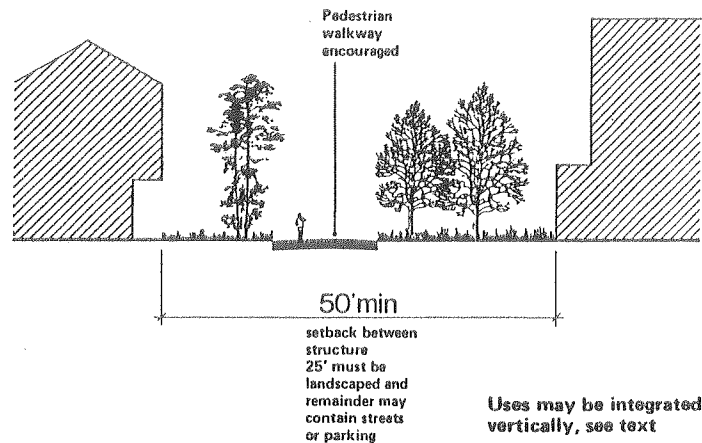


FIGURE IV-17

Buffer between Residential and Office/Commercial in Mixed-Use Parcels (Horizontal Integration)

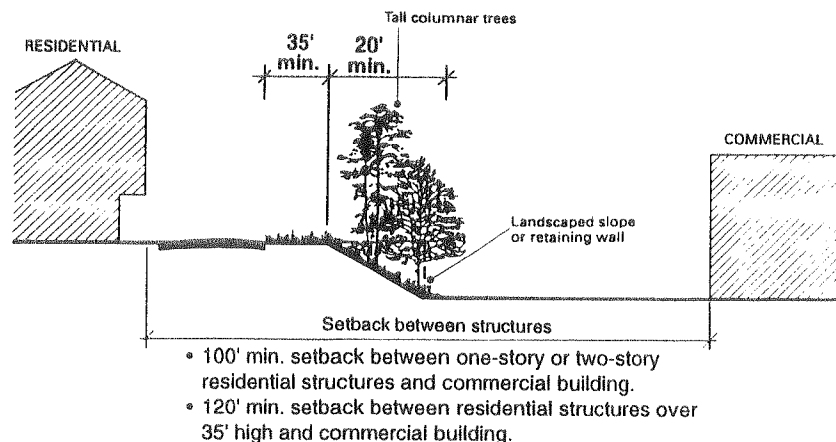


FIGURE IV-18

Buffer Between MH and H Residential and CC Commercial (Horizontal Integration)

property line that abuts the residential use. The minimum building separation between one-story or two-story residential structures and commercial buildings shall be 100 feet. The minimum building separation between residential structures that are over 35 feet in height, as measured to the plate line of the top story, and commercial buildings shall be 120 feet. Buffering techniques may include, but are not limited to, one or a combination of the following: building orientations for dwelling units and parking structures, decorative fences, walls, extensive landscaping with rows of trees, and sound attenuation remedies. Additional buffering and/or increased setbacks may be required through the design review process.

Edge Conditions Along Deer Creek and the Southern Pacific Railroad. Figure IV-20 illustrates the condition where residential use abuts the Southern Pacific Railroad right-of-way at the extreme northern boundary of Terra Vista.

The edge condition within residential parcels adjacent to Deer Creek is shown in Figure IV-21.

FIGURE IV-19
Deleted no longer applicable

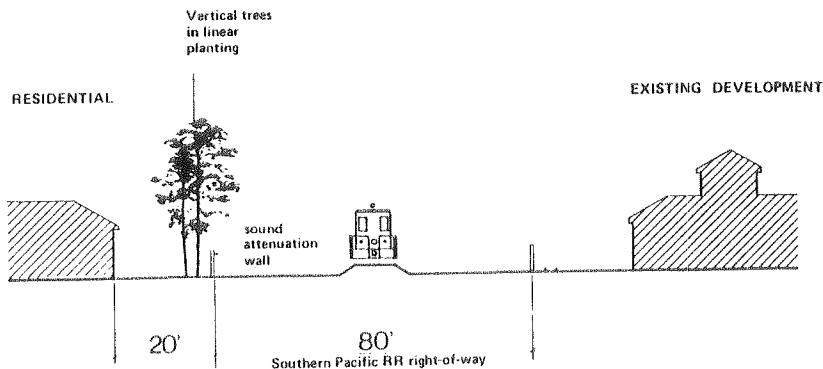


FIGURE IV-20
Edge Condition at Railroad

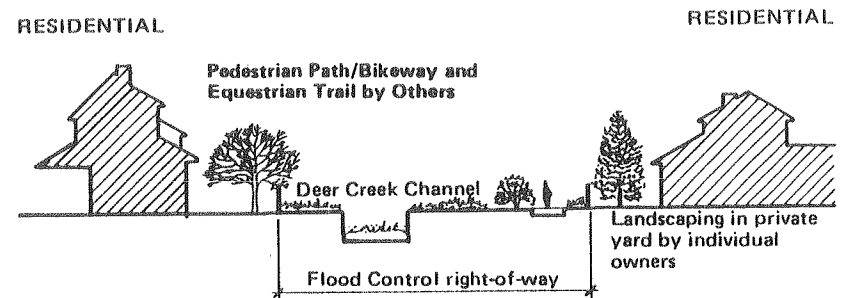


FIGURE IV-21
Edge Condition at Deer Creek

Park and Greenway System Design Guidelines

As discussed in Chapter III, the Terra Vista plan is built around the park and greenway system which contains a network of bicycle paths, pedestrian trails, and numerous parks and schools.

The Greenway

The major greenway traverses the site diagonally from northeast to southwest, providing a parklike environment for pedestrians and bicyclists with minimal exposure to motor vehicle routes. The greenway links together the Town Center at the property's southwest corner, elementary school sites, park areas, and residential developments. The linear portion of this greenway typically averages 40 feet in width with a minimum width of 30 feet; adjacent dwellings will generally be oriented to greenway views. Walls or fences are not required between the greenway and adjacent development. If fencing is desired, it should preferably be open or partially open to permit viewing of the greenway. The use of solid walls should be limited.

The circulation system has been planned so that there will be no street crossings of the major greenway other than the major roads shown on the land use and circulation plans (see Figure IV-23). The greenway spine will be interrupted by road crossings only four times over its one-and-one-half mile length: at Terra Vista Parkway, Spruce Avenue, Church Street, and Town Center Drive. (See Circulation section of this chapter for street crossing details.) A pedestrian signal and street crossing delineation (special trail pavement) will be provided at the mid-block greenway crossing of Terra Vista Parkway East at the park.

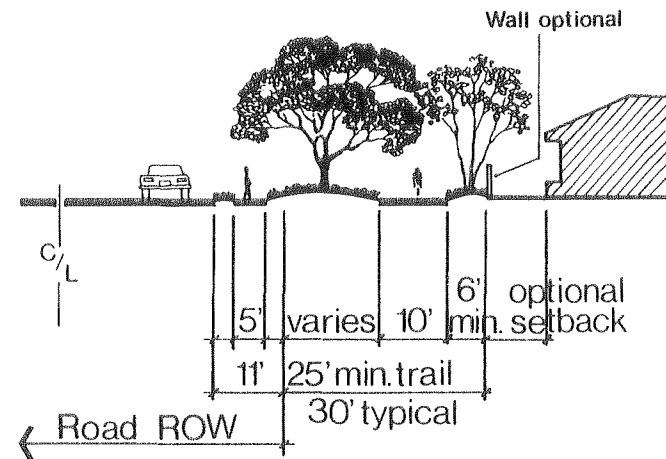
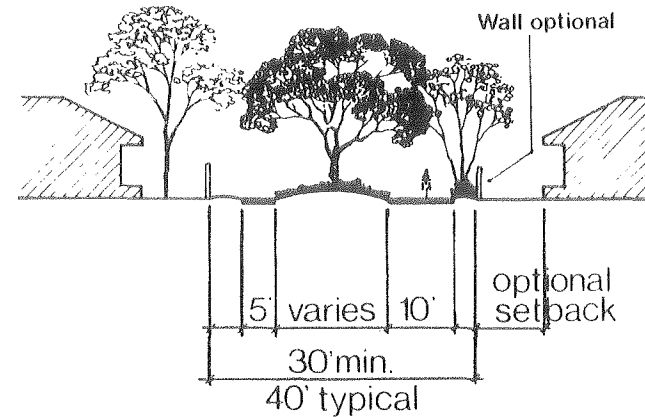


FIGURE IV-22
The Greenway—Trail Type "A" and "B"

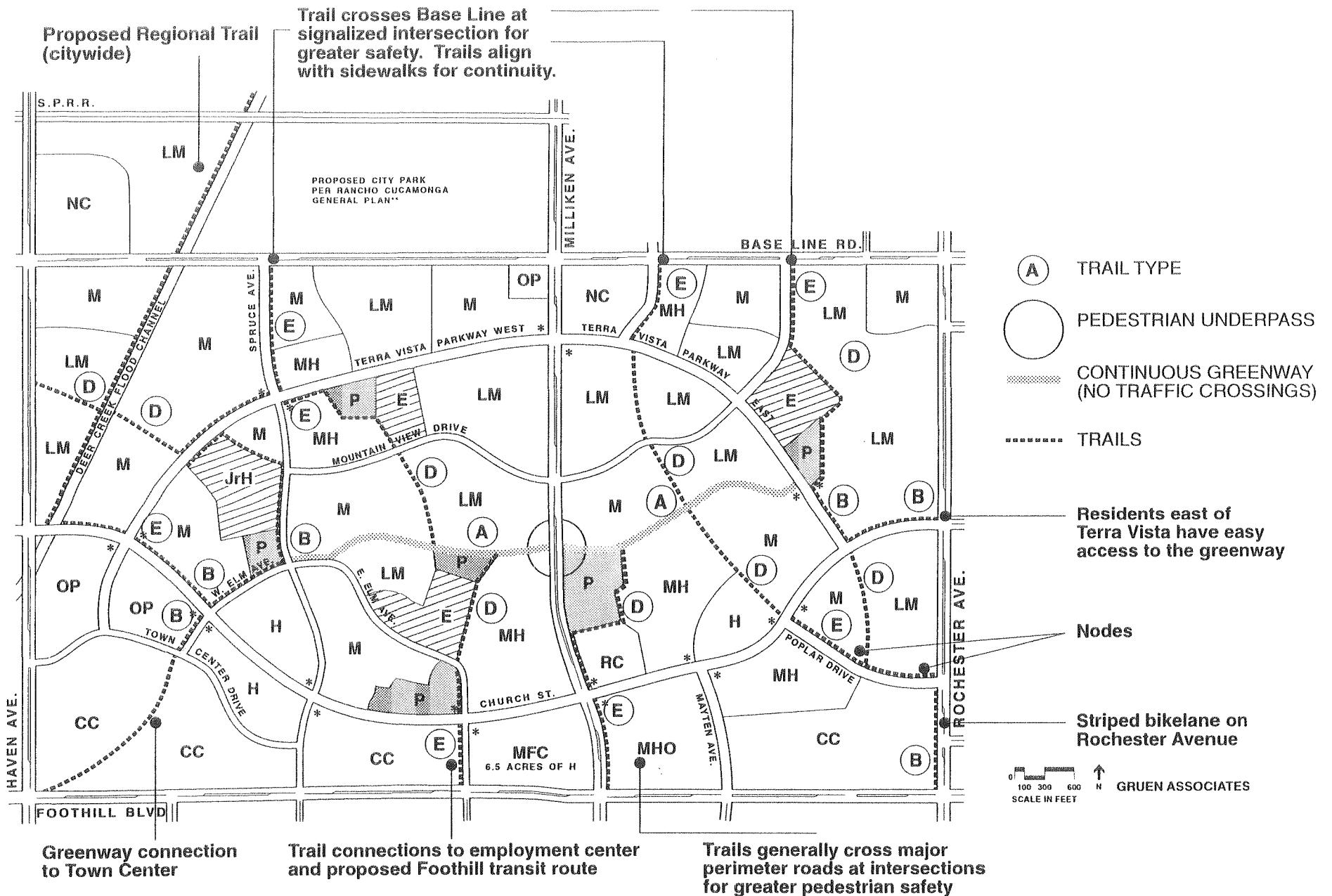


FIGURE IV-23

Greenway System Design Features

REVISED Amendment Nos. 1, 2, 5, 6, 7 & 9

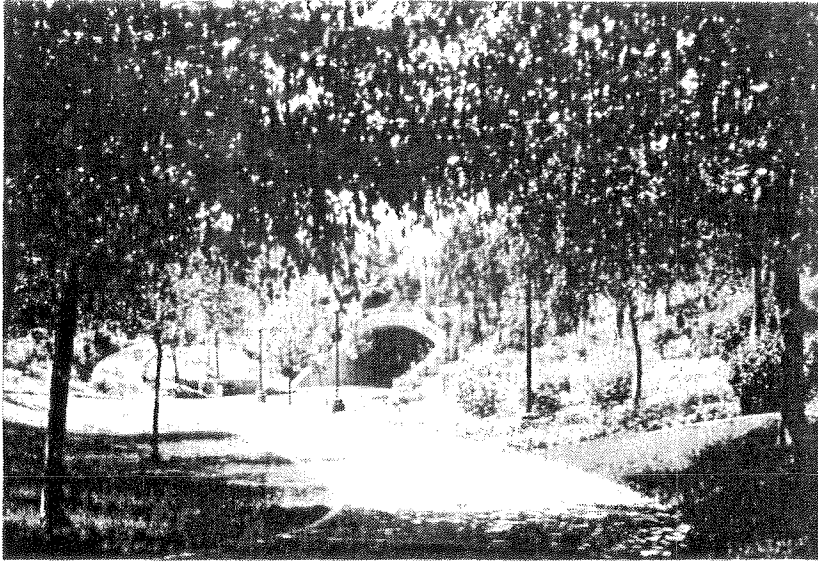


FIGURE IV-24
 Milliken Avenue Pedestrian Underpass—Possible Design Treatment

On Milliken Avenue, the most heavily traveled street internal to the site, a pedestrian underpass will be provided so that bicycle and pedestrian trails can continue without interruption (Figure IV-24). This will make it possible for a walker or cyclist to start from Terra Vista Parkway on the east and travel nearly a mile without encountering automobile traffic. The greenway spine will also be widened at Milliken Avenue to provide views of the greenway extending into the distance.

In some instances, local roadways are to be designed to parallel or terminate at (but not cross) the greenway in order to provide maintenance, police, and fire access as well as to provide a sense of security and a variety of experiences along the trail (Figure IV-25). Lighting will also be provided along the greenway system to enhance nighttime security.

Greenway Landscape Concept

The objective of landscaping within the greenway system is to provide a visually and environmentally cool effect within the community, in response to the usually arid climate of the region. Specialized landscape features will provide variation in experience for those using the greenway system. The primary components of the greenway landscape concept (Figure IV-26) are:

- Nodes
- Corridors
- Parks

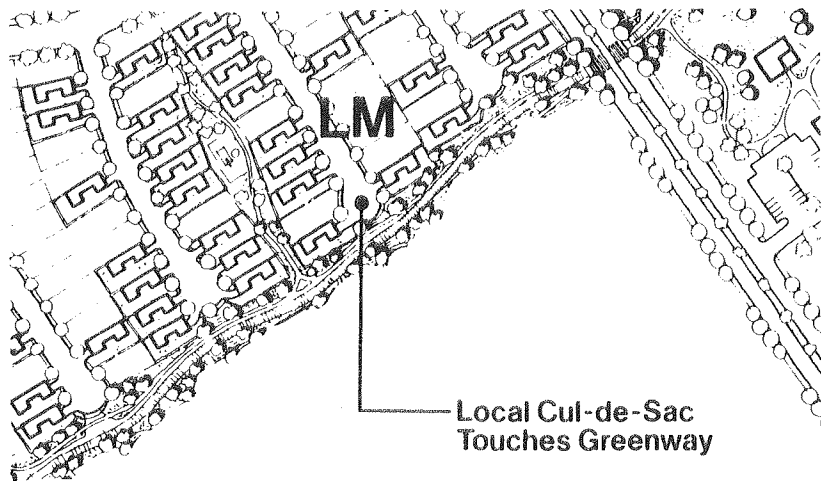


FIGURE IV-25
 Typical Condition in which Local Streets Touch Greenway

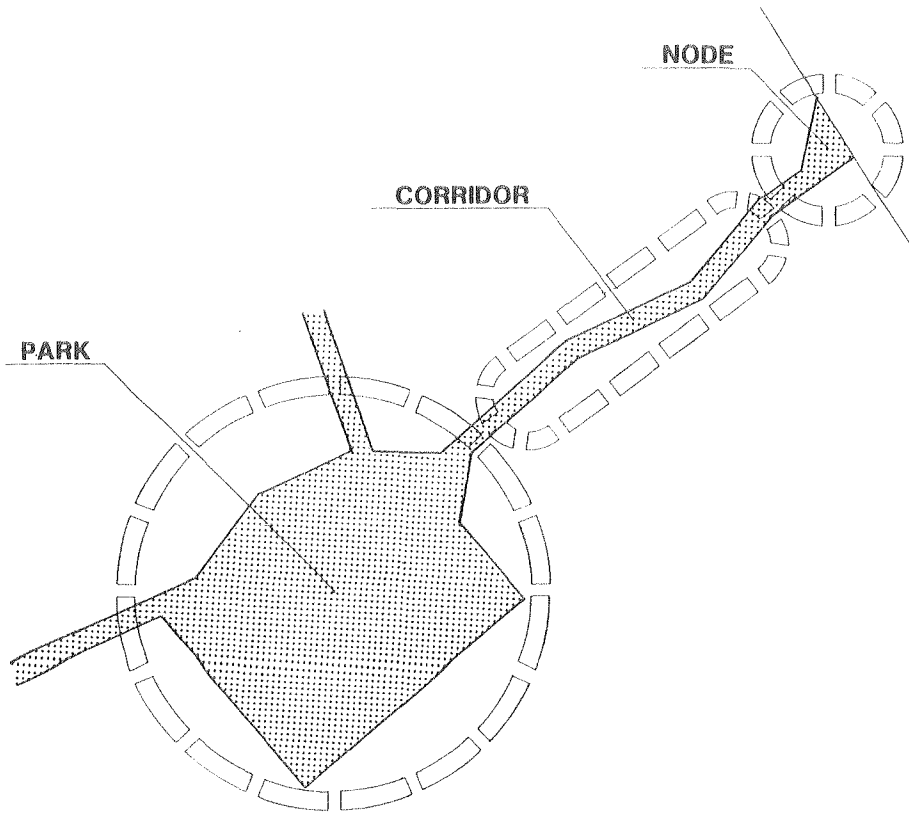


FIGURE IV-26
Components of the Greenway Landscape Concept

Figure IV-27 illustrates the integrated greenway landscape concept in plan form.

Nodes. Nodes along the major greenway will be designed to reflect the unique qualities of the neighborhoods between which the system passes. Each neighborhood will have a specific theme tree and the greenway node landscaping concept will feature the dominant use of these specific neighborhood trees (Figure IV-28). Selection of neighborhood theme trees with a broad, tall (upon maturity) canopy form is recommended but not mandatory.

The selection and placement of landscape features within the nodes will help to provide a sense of enclosure and entry in the following manner (Figures IV-27, IV-28, and IV-29):

- Shade structures will provide a location for passive recreation and a transition into the greenway system.
- Shrub and ground cover material will complement the materials of surrounding walls and/or buildings.
- Turf will be used in the center of the node for aesthetic and passive recreational purposes.
- The tree specimen used in the greenway corridors (see Figure IV-28) will appear only in selected locations within the node to provide landscape continuity throughout the system.

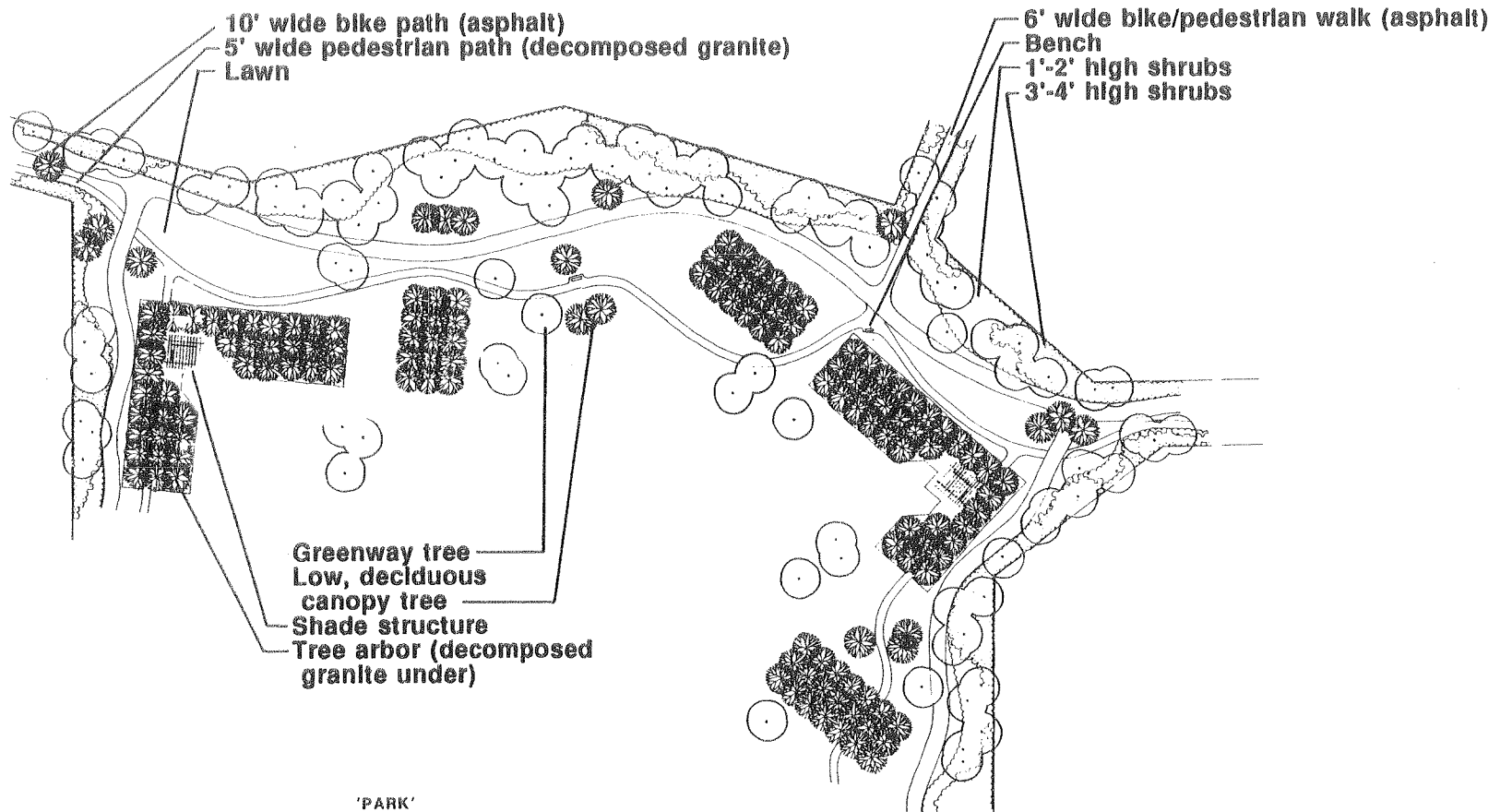


FIGURE IV-27
Greenway Landscape Concept Plan

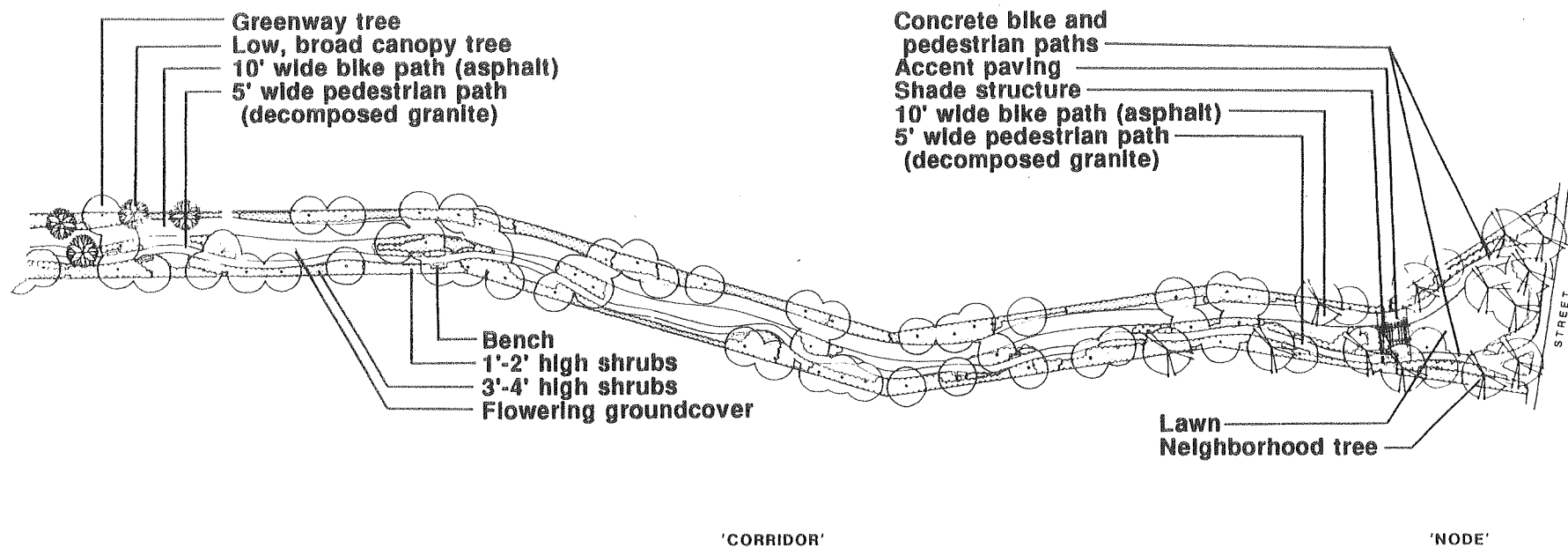


FIGURE IV-27 (continued)
Greenway Landscape Concept Plan



FIGURE IV-28
Sketch of Typical Node along the Greenway

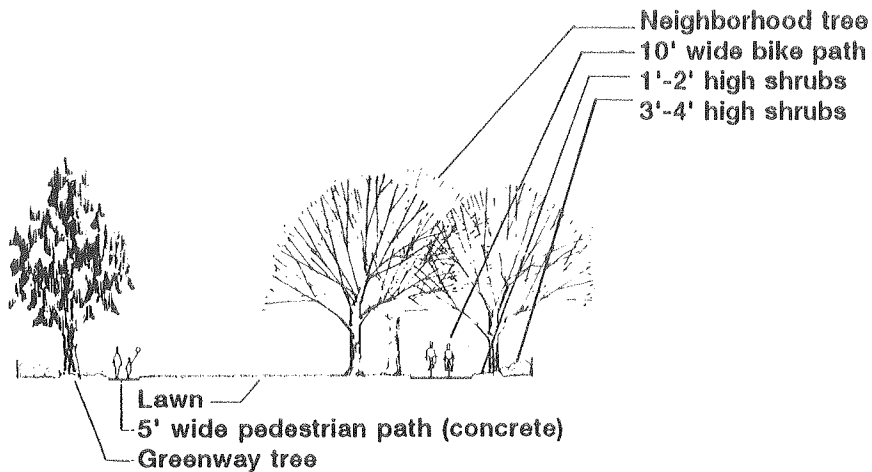


FIGURE IV-29
Cross Section of Greenway Node

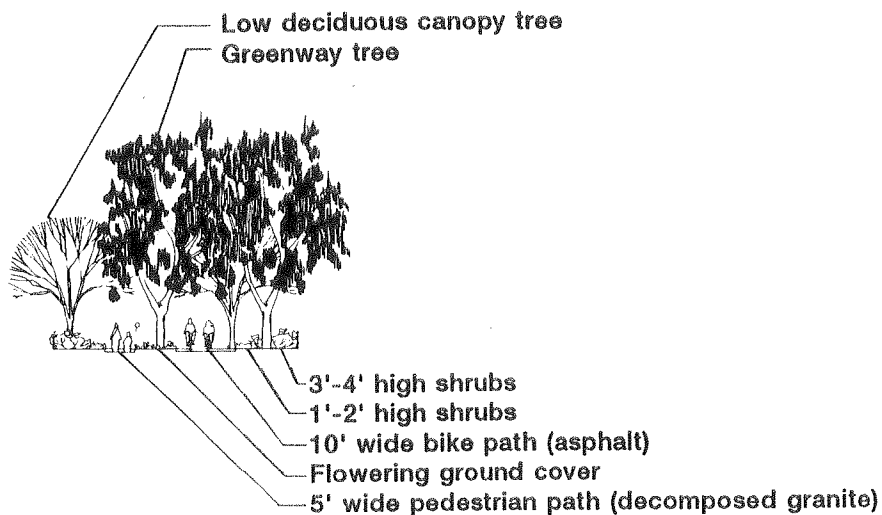


FIGURE IV-30
Cross Section of Greenway Corridor

Corridors. Greenway corridors will provide the linkage between the various nodes and parks (Figures IV-27, IV-30, and IV-31). They will be the backbone of the greenway system and will provide community-wide landscape continuity in the following ways:

- A specific greenway tree will be the dominant tree within all corridors. It is recommended that a tall, arching, evergreen tree be selected.
- Neighborhood trees will be used in selected locations in close proximity to nodes.
- Low, broad canopy trees specific to adjoining parks (see below) will be used in selected locations near those parks.
- Turf areas will be confined to parks and nodes in order to encourage active recreation at those locations, rather than in the corridors.
- Benches along pedestrian paths will allow people to rest and visit with their neighbors.
- Shrub and groundcover material will be drought-tolerant and will be selected and placed to complement adjacent wall materials.

Parks. The greenway system provides entrances into parks at Terra Vista (Figures IV-31 and IV-32). Landscape elements within the park will be designed and situated in conjunction with the park's recreational features, as follows:

- Tree arbors will help to develop a sense of entry into each park. These low, broad canopy trees will establish each park's landscape identity.

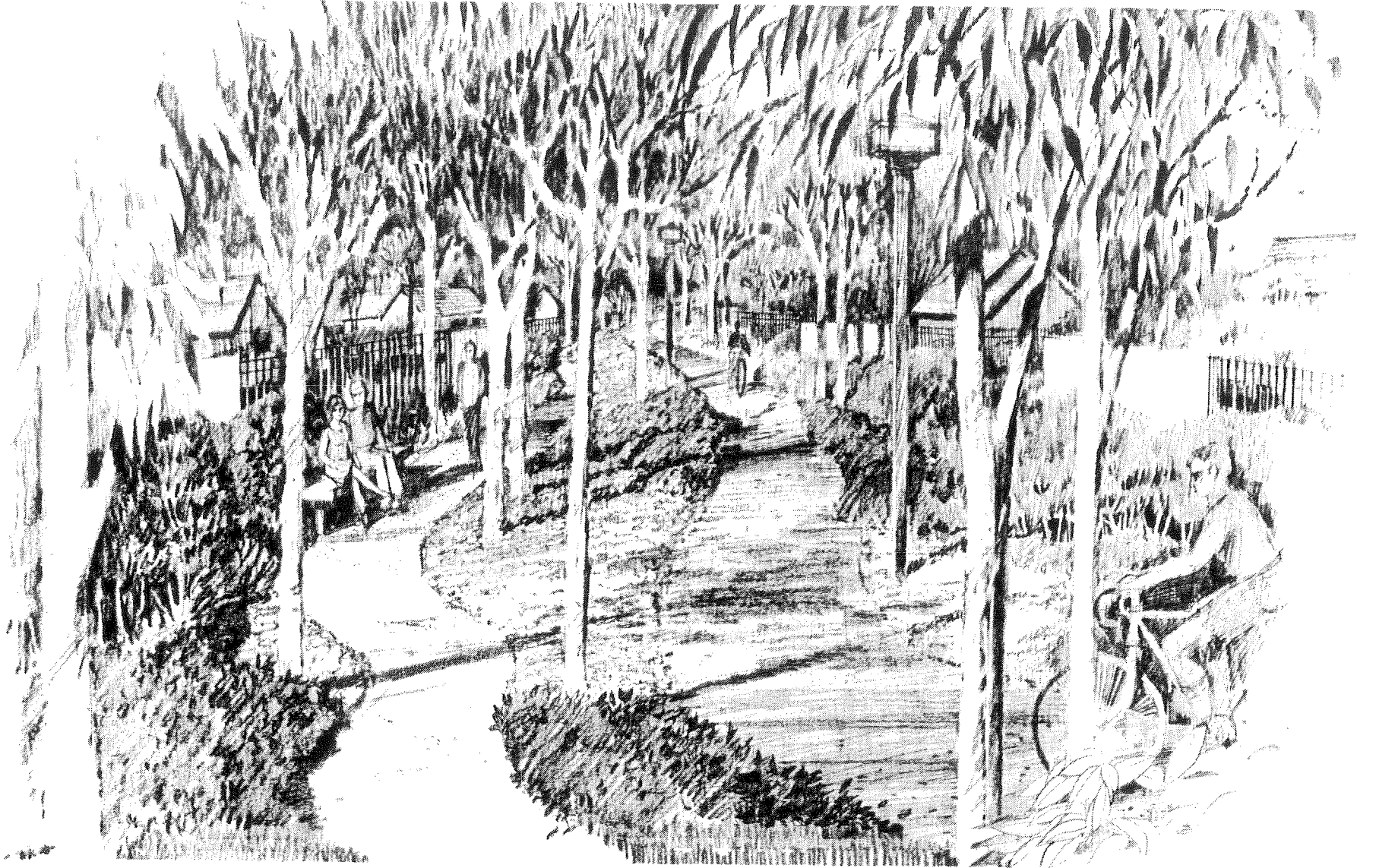


FIGURE IV-31
Sketch of Typical Corridor along the Greenway

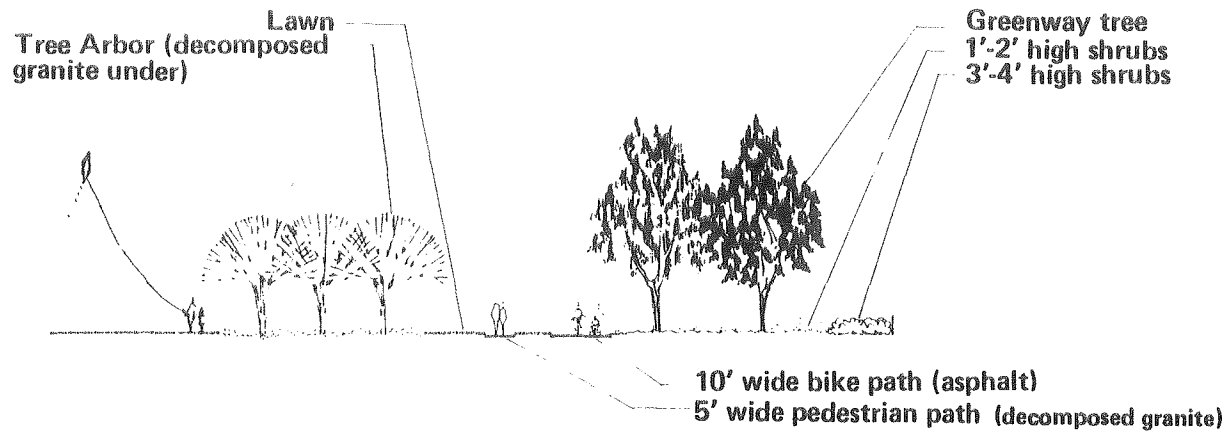
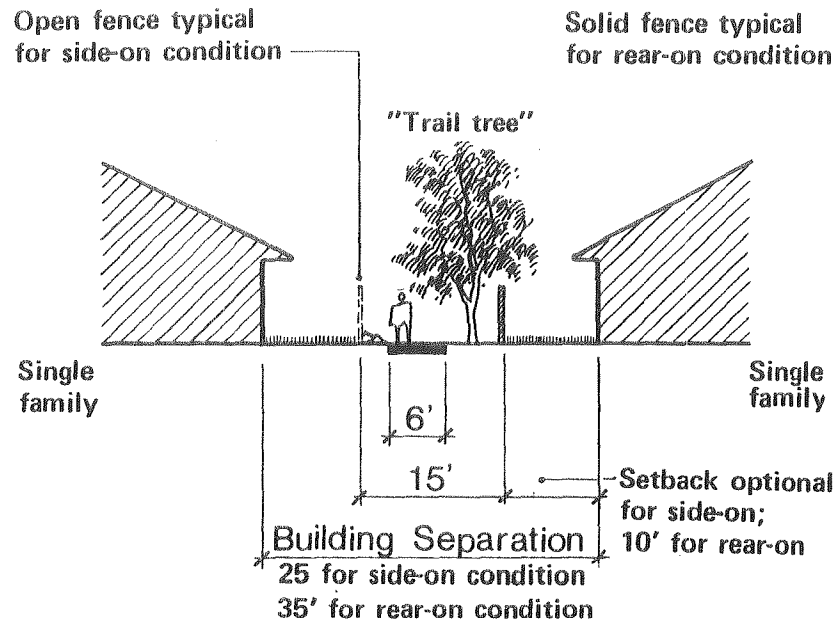
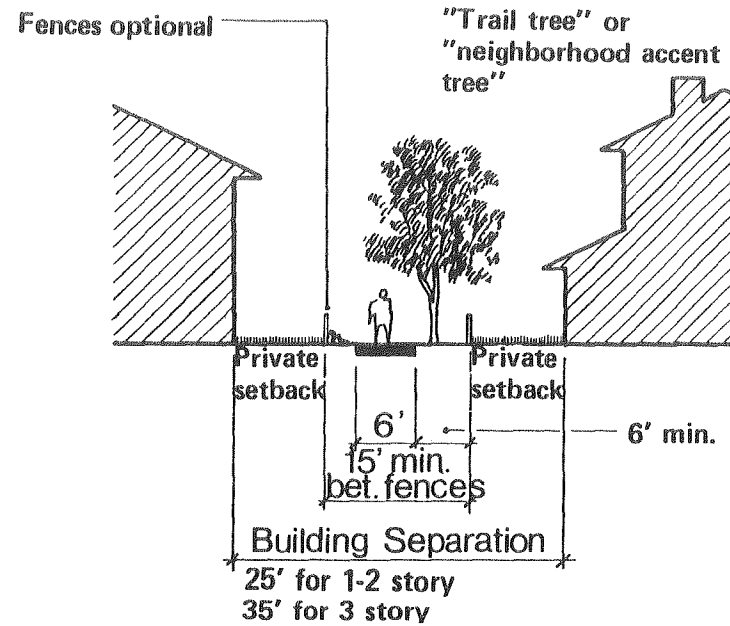


FIGURE IV-32
Cross Section of Greenway at Park

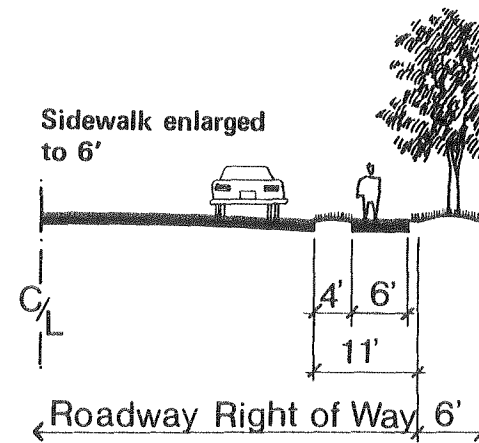
- The dominant greenway corridor tree will be used within parks where the greenway and its pedestrian and bicycle trails penetrate the park. Corridor shrub planting will continue at the park perimeter to provide the greenway corridor with visual continuity.
- Turf will be the major groundcover within the parks to allow active recreation.
- Shade structures near tree arbors will provide for passive recreational activities.
- As at the nodes and along the corridors, shrub and groundcover material will be selected to complement surrounding walls and housing.



Trail Type "C"
Single family both sides



Trail Type "D"
Multifamily Adjacent to One or Both Sides of Trail



Trail Type "E" – Adjacent to Roadway

FIGURE IV-33
Trail Types "C," "D," and "E" – Secondary Trail Cross Sections – Combined Bicycle and Pedestrian Paths

Trails

In addition to the primary greenway spine, secondary greenway trails, generally running in a north-south direction, extend into and through all parts of the community, as described in Chapter III. These secondary linkages have separate bicycle and pedestrian paths in some instances and combined paths in other instances. The minimum unobstructed width of all trails (i.e. not interrupted by walls, fences, or buildings) is 15 feet. The minimum building setback varies depending on the situation. All trails will contain as a minimum a six-foot paved walk with adequate side access for maintenance via cul-de-sacs or easements. A tall, arching evergreen tree will be utilized throughout the trail system to provide a consistent image. Figure IV-23 indicates the location of each trail type.

Figure IV-33 illustrates Trail Types C, D, and E, the secondary trails with a six-foot wide combined bicycle and pedestrian path. Three conditions are shown for secondary trails:

Conventional Single Family Adjacent to Both Sides of the Trail (Trail Type C). Trails are 15 feet wide and typically fenced off from single family lots. The side-on condition is preferred but not mandatory. In the side-on condition, there is no minimum setback; however, the minimum building separation remains 25 feet. Wall conditions vary among 3-foot solid walls, 3-foot walls topped by an open fence, and open fences or solid walls 5-feet 6-inches high. In a rear-on condition, the typical situation along trails is a 5-foot 6-inch wall; the building separation is increased to 35 feet; and the minimum building setback from the trail is 10 feet.

Trail Type C also typically occurs adjoining schools.

Cluster Multifamily Adjacent to One or Both Sides of Trail (Trail Type D). The intent of this condition is to minimize the use of solid walls or fences along the trail system, allowing the common landscaped areas of the multifamily projects to touch the trail. Minimum clear trail width remains 15 feet; however, the actual right-of-way is six feet. Minimum building separation is 25 feet for buildings of one or two stories and 35 feet for buildings of three stories. The minimum building setback from the trail is six feet. Both the selected trail tree and the neighborhood accent tree will be used in landscaping the edge of the trail adjacent to multifamily projects.

Trails Adjacent to Roadways (Trail Type E). When trails adjoin public roadways, six feet are added to the landscaped public area beside the roadway outboard of the sidewalk. The width of the sidewalk is also increased from four to six feet to accommodate the bicycles and pedestrians.

Figure IV-34 illustrates landscaping of Trail Types D and E in plan form.

Nodes Along the Trail System. Parklike nodes occur at various locations along the secondary trail system. These nodes are frequently smaller than those on the major greenway. Landscaping of trail nodes will be similar to that of greenway nodes (see above) except that shade structures and similar amenities are not required.

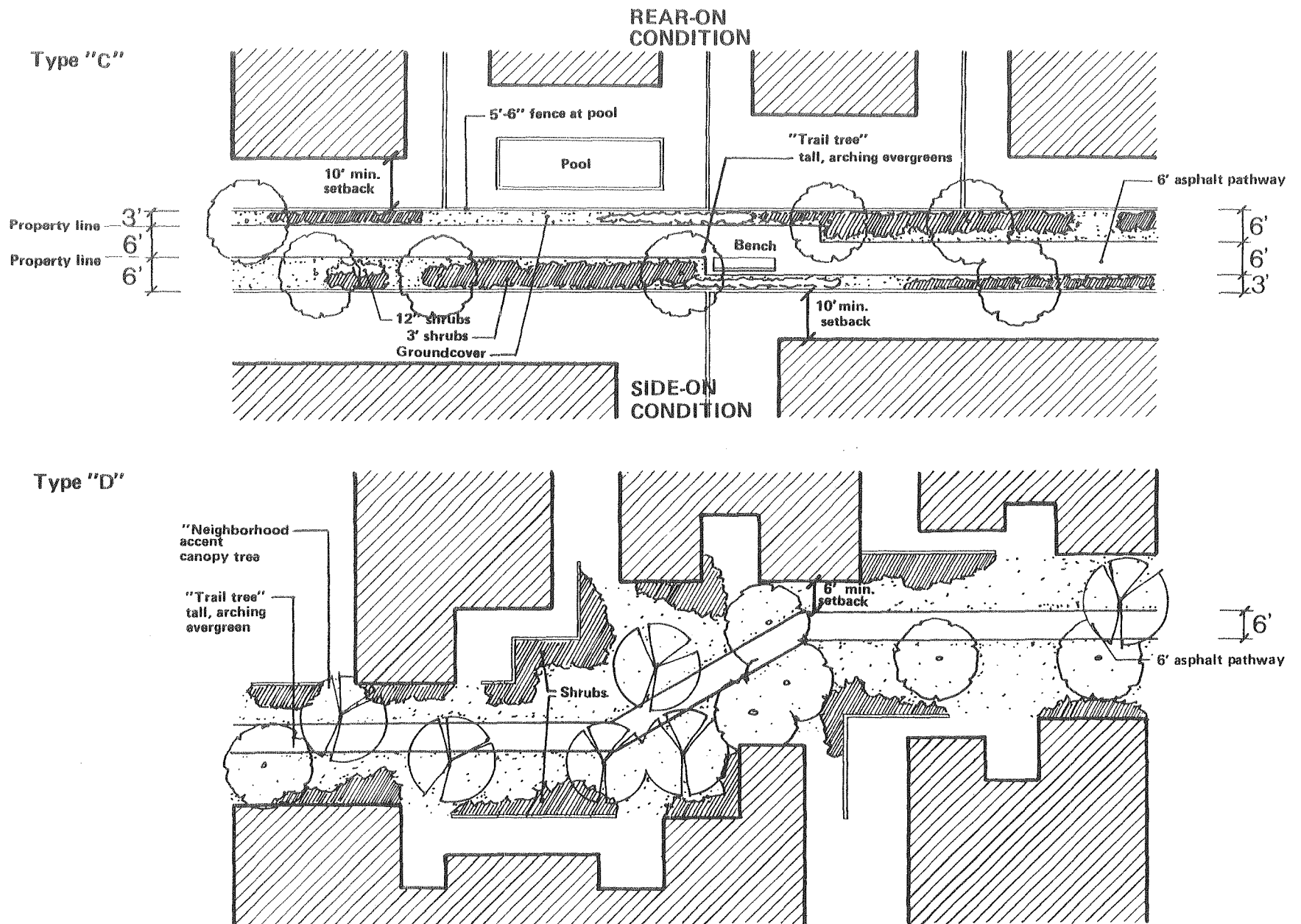


FIGURE IV-34
Landscaping Concept for Trails

Local Parks

The Terra Vista Community Plan contains a series of parks along the greenway and trail system (Figure III-1). Each park will be designed in cooperation with the City of Rancho Cucamonga to serve the specific needs of its neighborhood.

Parks will provide much of the green image of the Terra Vista Community. The Community Plan calls for parks to be distributed around the site so as to provide easy access to open space for all community residents. All the parks are tied in with the greenway system.

All elementary schools and one junior high school are situated adjacent to public parks to facilitate joint use of parks for school and general recreational purposes. Parks adjacent to elementary schools are five acres in size. Figure IV-35 indicates the type of development that can occur within this area.

In addition, Chapter III presented the capability of one of the community parks to serve on very rare occasions as supplement to the storm drain system by storing water for a number of hours during exceptionally heavy storms. This extra function will probably go unnoticed by nearly all community residents for three reasons: (1) it will occur only rarely; (2) people are unlikely to visit a park during the most massive downpours -- the only times water would ever be introduced to the parks; and (3) the park will be gently and subtly contoured.

The contouring of that park also lends itself to numerous recreational activities. The broad, flat areas of the park will serve as excellent playfields; the knolls and terraces will be ideal for

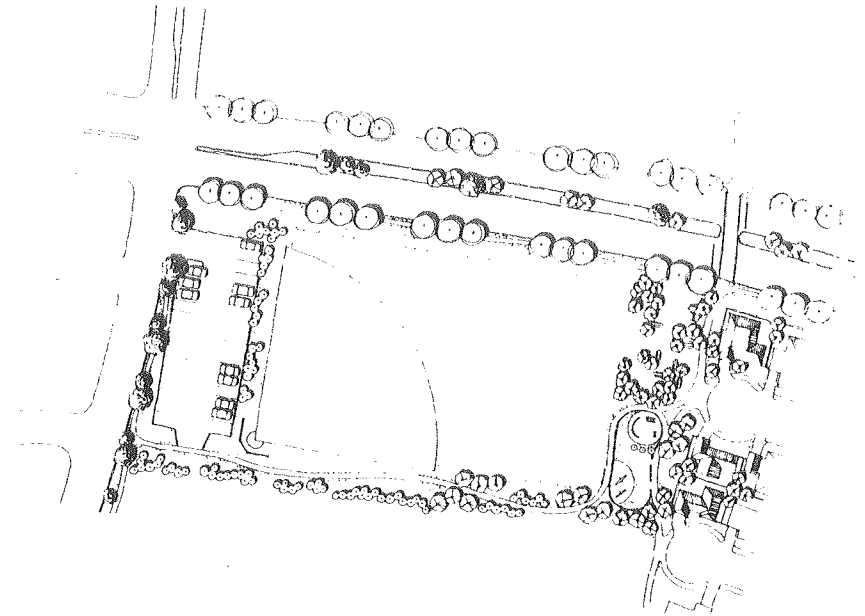


FIGURE IV-35
Five-Acre Joint Use Park Adjacent to School Site

picnicking, walking, jogging, and exercise; and the gentle slopes -- a maximum of one foot in five -- will allow easy viewing of park activities.

Figures IV-36 and IV-37 present alternative development concepts for a typical joint use park of this sort. One concept (Figure IV-36) features a hard surface recreational field--in this case a series of basketball courts--at the park's lowest elevation in order to avoid maintenance problems altogether except after the most infrequent storms. The other concept (Figure IV-37) features a turfed recreational area at the lowest elevation; many parks throughout the country feature such a configuration and experience with maintenance has been very favorable. The only facilities not recommended to occupy the lowest elevations of a joint-use park are tennis courts, since the court surfaces are extremely sensitive to environmental conditions.

Proposed City Central Park Per Rancho Cucamonga General Plan

The area bounded by Milliken Avenue, Base Line Road, the SPRR right-of-way, and the Deer Creek Channel has been identified in the City's General Plan as the proposed location of a City central park. A portion of this park site is to be provided by the Terra Vista Planned Community, as will be explained in the park implementation plan supplementing this text. The balance of the site is designated as a proposed City park in this plan solely as a result of the expressed intention of the City to purchase and develop this site. However, the Community Plan proposes development of the site in income property uses if park plans are not implemented, as explained in Chapter VI.

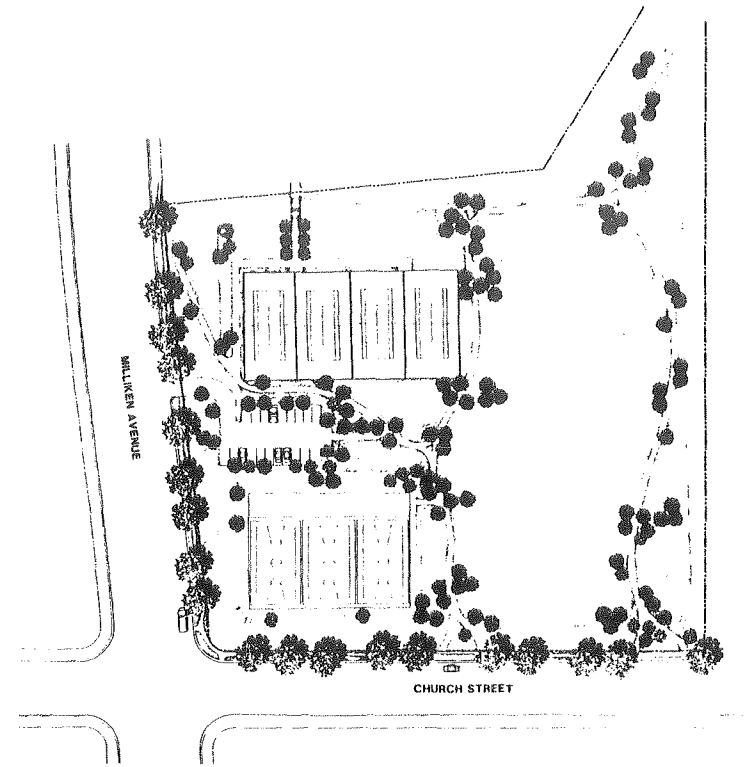


FIGURE IV-36
Joint Use Park Indicating Hard Surface Playing Fields at Lower Elevations

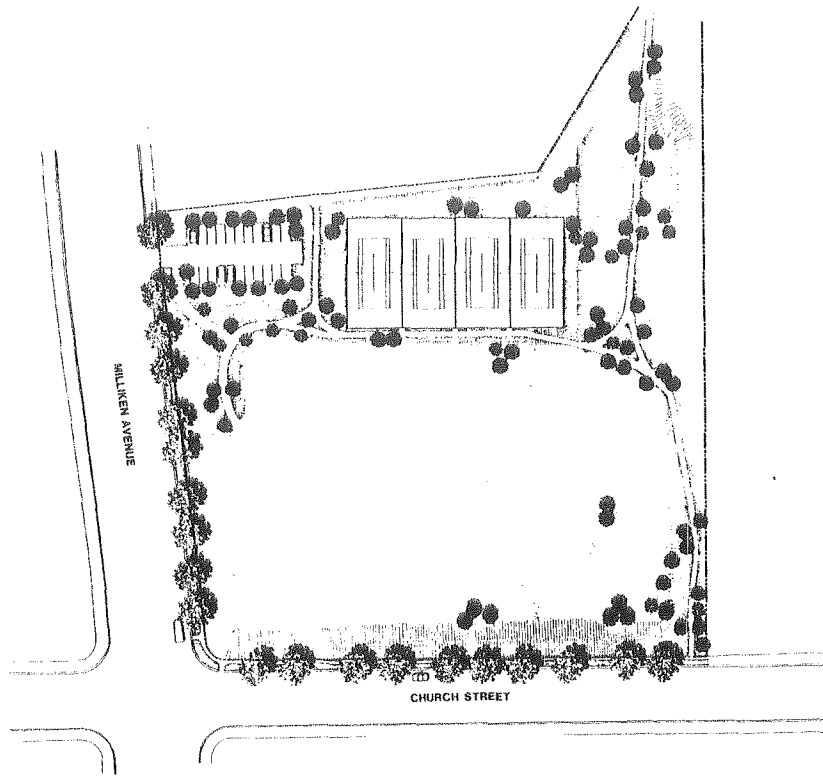


FIGURE IV-37
Joint Use Park Indicating Passive Recreation at Lower Elevations

Design and development of the proposed City park will be determined by the City. However, park development within this area by the City should be compatible with the balance of the Planned Community in the following respects:

- Functions that could have a negative impact on adjoining residences, such as those requiring intense lighting or loudspeakers, should be situated away from residential areas and, if necessary, be buffered by distance or screening.
- Access should be coordinated with planned community circulation and trails plans; specifically, the signalized intersection at Spruce Avenue and Base Line Road is recommended as a primary access point for both vehicles and pedestrians.
- Roadway improvements on perimeter highways should be consistent with the standards in this plan and the requirements identified in traffic analyses for the Planned Community.
- Roadway landscaping should be compatible with the Planned Community treatments.

Circulation System Design Guidelines

Circulation is one of the major factors that has influenced the design of the Terra Vista community. Circulation design components include:

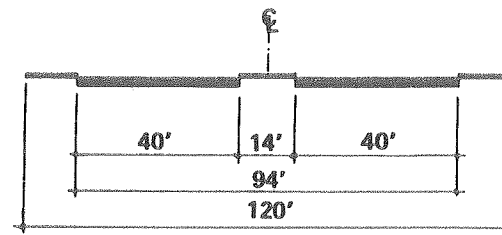
- Street classifications
- Transit facilities
- Street crossings of pedestrian and bicycle paths

Each of these components is described in the discussions that follow with specific regard toward their dimensions, landscaping, and visual qualities.

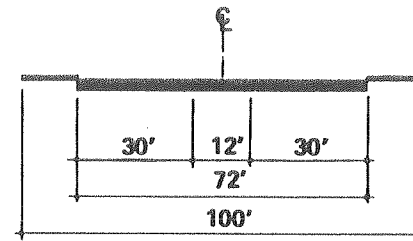
Street Classifications

A hierarchical system of roadways has been developed for Terra Vista. Figure III-16 shows the circulation network and indicates the classifications of streets. Figure IV-38 illustrates the schematic standard cross-sections for major arterials, secondary arterials, and collectors. Each cross-section is designed to consider both urban design factors (i.e., landscaping treatments, relationships to adjacent areas) and traffic movement requirements. Cross-sections may vary to accommodate technical requirements such as grade changes and drainage requirements.

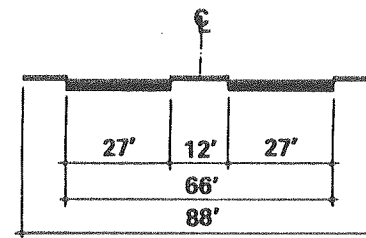
Localized Widening. Localized widening will be required at certain intersections to respond to the volume of traffic on major arterials. Additional right-of-way beyond that required for the standard cross-section will be provided at these intersections for additional left-turn, right-turn, through or right-through lanes. Details are provided in the Terra Vista Environmental Impact Report (EIR).



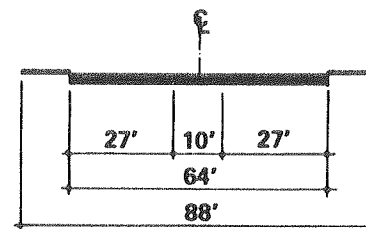
- MAJOR DIVIDED ARTERIAL**
- Foothill Boulevard
 - Haven Avenue
 - Milliken Avenue
 - Base Line Road



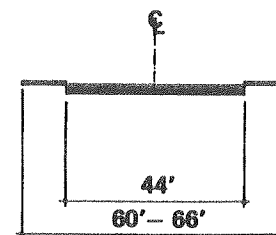
- MAJOR ARTERIAL**
- Rochester Avenue



- SPECIAL SECONDARY ARTERIAL**
- Church Street
 - Terra Vista Parkway



- SECONDARY ARTERIAL OR COMMERCIAL COLLECTOR**
- Spruce Avenue
 - Others



- RESIDENTIAL COLLECTORS**

FIGURE IV-38
Street Cross Sections

Conformance with City Standards. In general, these cross-sections are identical to those designated in the Rancho Cucamonga General Plan with the exceptions of Church Street and the collectors. Church Street will have a landscaped, raised median for safety, functional, and aesthetic reasons, and collector roads will have a sidewalk only on that side of the street opposite the greenway to encourage those near the greenway to utilize its special pedestrian amenities. Local public streets are proposed to have two types of configuration, as illustrated in Figure IV-15. One concept features sidewalks on both sides of the roadway while the other features a sidewalk on one side only. Local private streets will have a variety of configurations to respond to specific needs of each development, subject to the approval of the City Engineer.

Sidewalks will have a minimum width of four feet on all public streets. When a four-foot sidewalk is adjacent to the curb, its width is measured from the back of the curb. Where retail uses front on a sidewalk and heavy pedestrian use is expected, minimum width will be five feet (six feet if at curb and sidewalk is obstructed by street furniture).

Transit Facilities

As previously discussed in Section III, the loop parkway would be a logical internal transit corridor for the Planned Community, with anticipated interface with a future regional transit system intended to operate on Foothill Boulevard and Haven Avenue. A suggested local transit route and bus stop locations were identified in Figure III-16. The design of a typical bus pullout and its intersection are shown in Figures III-39 and III-40.

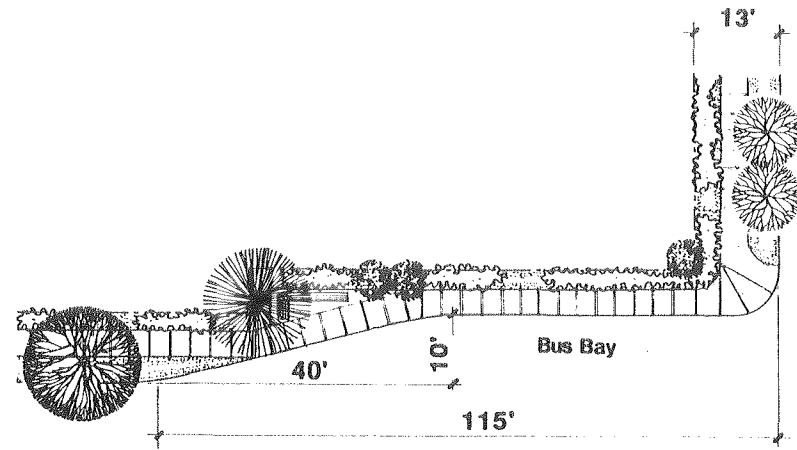


FIGURE IV-39
Typical Bus Stop at Intersection

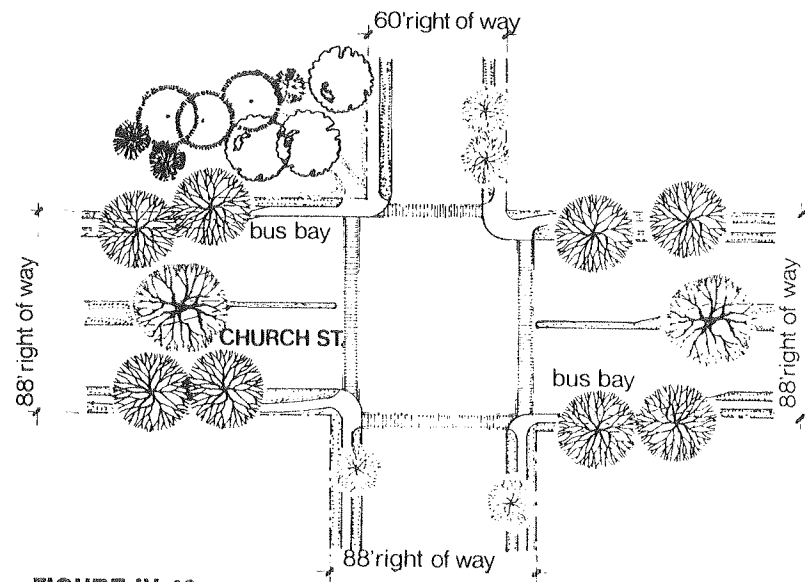


FIGURE IV-40
Typical Intersection with Bus Stops

As indicated, it is anticipated that bus pullouts would generally be on the far side of an intersection as illustrated in Figure IV-40. This configuration is preferred by local transit agencies for its reduced impact on traffic circulation.

Street Crossings of Pedestrian and Bicycle Paths

The circulation system has been planned so that the greenway spine will be interrupted by road crossings only four times over its one and one-half mile length: at Terra Vista Parkway, Spruce Avenue, Church Street, and Town Center Drive. At Milliken Avenue, a pedestrian underpass will be provided so that bicycle and pedestrian trails can continue without interruption. This will make it possible for a walker or cyclist to travel nearly a mile without encountering automobile traffic (Figure IV-41).

Pedestrian bridges or overpasses have been considered for Terra Vista but were not found to be appropriate for the following reasons:

- To take pedestrians and bicyclists up and over a roadway would require ramps approximately 200 feet in length.
- Low volumes of daily traffic (fewer than 20,000 vehicles) make it more likely that bicyclists and pedestrians would cross streets at grade, rather than to go up and down a bridge or overpass even if one were available.
- The high initial cost of such structures would contribute to higher housing costs.

The more heavily traveled streets in Terra Vista will be crossed by the greenway system at street intersections (Figure IV-42) and thus will have the

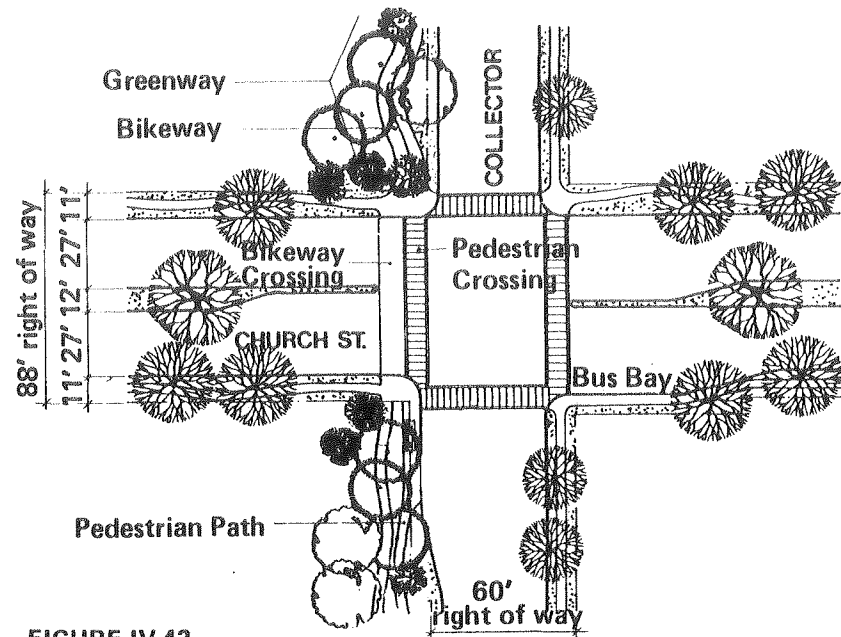


FIGURE IV-42
Greenway Crossing Street at Intersection

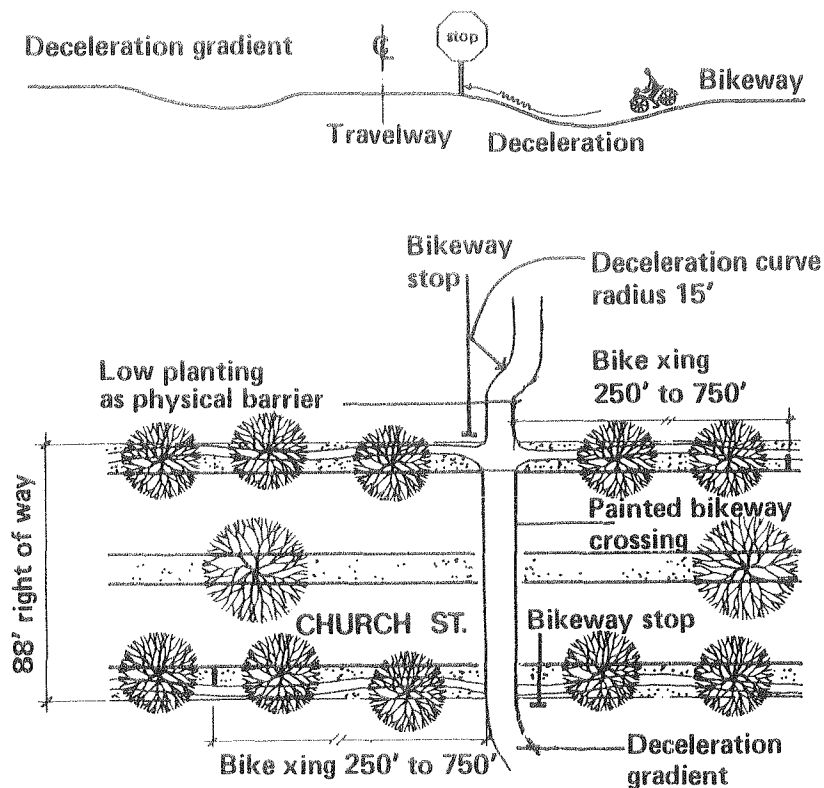


FIGURE IV-43
Crossing of Secondary Greenway Trail at Midblock

benefit of signalized crossings in most cases. These are described below. Three of the crossings of the primary greenway spine (Church Street, Spruce Avenue, and Town Center Drive) will occur at major street intersections. This situation allows pedestrians and cyclists to cross safely in a typical crosswalk condition with or without a signal. Specifically designed crosswalks will be used at these crossings.

The secondary greenway system generally crosses the loop parkway at street intersections and will receive treatment similar to that of the major greenway system. In two instances, the secondary greenway system will cross major roadways in mid-block on Terra Vista Parkway. These crossings, one between Church Street and Spruce Avenue and one between Milliken and Mountain View Avenues, will have a standard crosswalk. Signals are probably not warranted at these locations since almost all crossings will be limited to fewer than 75 students twice daily at known hours.

Figure IV-43 indicates recommended safety features for midblock crossings, including deceleration curves (to slow down cyclists), bicycle and pedestrian stop signs, and signs on the roadway to indicate a bicycle crossing. Other options exist, including the use of special design features such as arbors or special pavement treatments to alert drivers as they approach the crossing. The recommended treatment, however, is desirable in that it promotes caution among cyclists and avoids giving them a false sense of security while crossing vehicular routes.

Residential Design Guidelines

The residential guidelines that follow deal with certain special conditions that will affect individual housing projects within the Terra Vista Planned Community. Community Development Standards, Chapter V, provides more special regulations for individual lots.

Figure IV-44 is an illustrative concept for a portion of one neighborhood and indicates how individual developments can be planned and related to one another in accordance with these guidelines. This figure is provided only to illustrate concepts; the actual development plan for this area may vary substantially from this example.

In addition to the residential guidelines listed in this chapter, all multiple family development within the Planned Community area must comply with the General Design Guidelines as outlined in the Residential Section of the Rancho Cucamonga Development Code.

Connections to the Greenway

It is the intent of the Community Plan that safe and convenient connections be provided between housing areas and the park and greenway system. Creativity in relating residential areas to open space is also encouraged. The following discussions describe ways in which this can be accomplished.

- When conventional single-family development adjoins the greenway, it is preferred that local streets terminate in cul-de-sacs that touch the greenway. In this manner, all the lots on the street have access to the greenway via the sidewalk and more open views to and from the greenway are created (Figure IV-25).

- In cases when common private open space is a part of higher-density or cluster housing developments, private open space should be linked to the greenway with a minimum of street crossings whenever feasible (Figure IV-45).
- The greenway system is intended to create a variety of experiences. Therefore, although cul-de-sac streets are encouraged in single family areas with sideyards facing the greenway, the side-on condition is not required. Similarly, cluster developments may be oriented to the greenway in a number of ways.
- The staggering of cul-de-sacs along the greenway is encouraged.
- Fencing and security features, where present, should be designed to facilitate access to the greenway system.
- The relationship of homes to parks and other open space, including school grounds, can be treated in a variety of ways. Where homes directly adjoin a playfield, for instance, it is sometimes possible to delineate the boundary between them very satisfactorily using simply a meandering sidewalk and trees, rather than a wall. In such cases it is possible for some homes to front directly on the open space, thus enjoying huge "front yards." Because design solutions like this are possible and can be very desirable under the right circumstances, it is not a mandatory requirement of this plan that walls and setbacks be provided between open space and other uses.

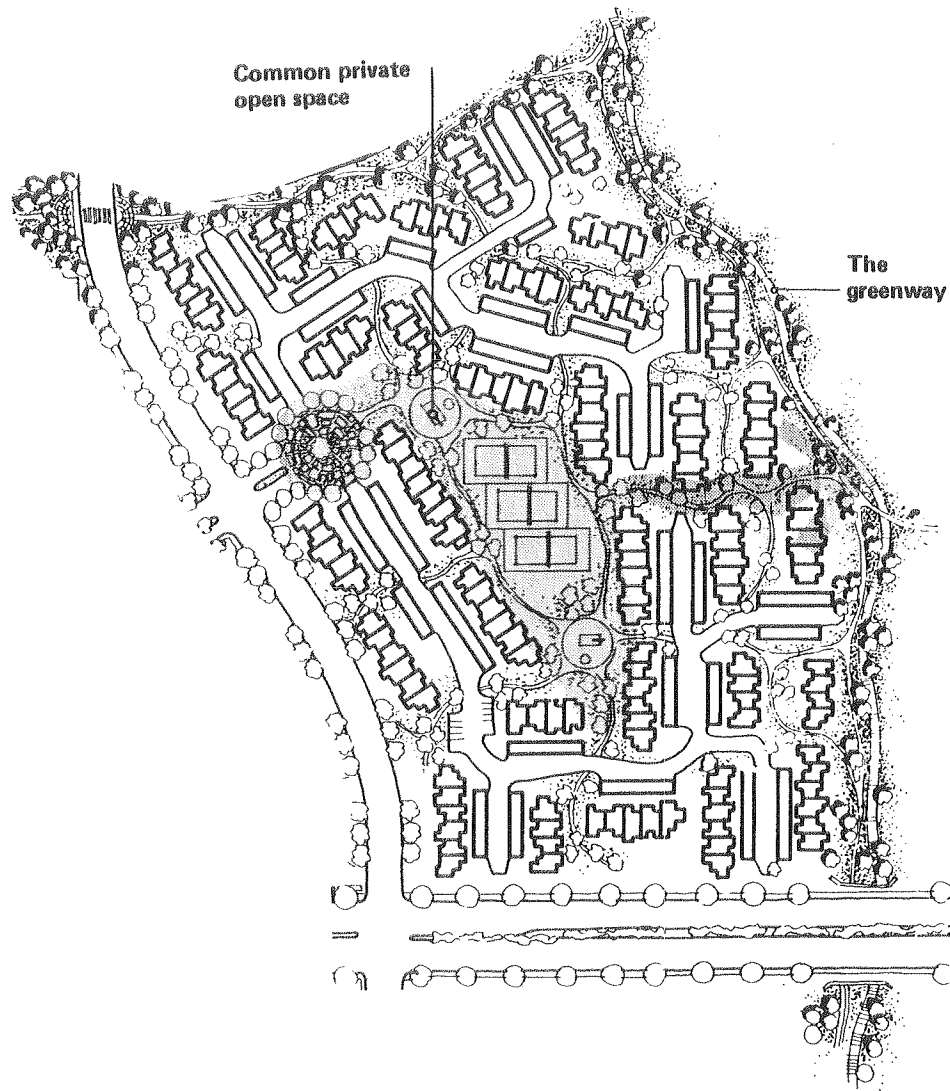


FIGURE IV- 45
Private Open Space Linked to the Greenway

Buffer and Edge Conditions

- Conventional single-family detached lots adjoining a major arterial will be protected by a fence or wall at the property line for privacy and noise protection (see pages IV-5 through IV-8).
- Cluster development is generally preferred along high traffic volume streets. Wall or fence requirements for such developments will be evaluated as part of the development review process.
- Single-family detached lots will not have access directly from a major arterial or secondary arterial. However, single family homes may front or side onto major or secondary arterials if vehicular access is provided from the rear or side. Fences or walls will be required where the rear yards of single-family detached lots adjoin these streets; in side-on or front-on conditions, the need for fences or walls will be evaluated case-by-case under the development review process.
- Residential developments bordering commercial uses will be buffered by landscaping and/or walls or fences (see Figure IV-16), except in mixed use parcels where an integrated design scheme is used.
- Transition of Density. The site plan should consider compatibility with surrounding neighborhood through providing proper transition of density, particularly on infill sites adjacent to lower densities. Comparable densities, open space buffer zones, increased setbacks and architectural compatibility are encouraged along common boundaries to provide proper transition of density. Clustering units can provide large open space areas as a buffer.

Planning for Streetscape Variety

An important goal of the Terra Vista plan is to create functional and visual variety along local streets. It is the intent of the plan to discourage subdivisions where identical homes march down long, uninterrupted straight streets, with no variation in building placement or the street scene. Variety is the watchword of Terra Vista. There will be many types of housing organized in a multitude of development patterns. Each type of housing development makes different demands on the planner and imposes different constraints. Therefore, while one neighborhood may differ dramatically from another, individual neighborhoods must retain internal consistency in terms of subdivision layout, lot size, and site planning. It is not intended that a neighborhood must "vary" internally so much that it does not look like what it is, or that homes and plotting vary so much as to appear customized. Rather, within the basic development pattern appropriate to the type of project, variety and flexibility are encouraged to avoid a sterile, monotonous appearance along the street.

Following are some of the techniques that can be used to create variety along local streetscapes. Any one or more of these will be used in each project. It is not intended that all or even most of these techniques be applied in every project; a combination of a few of them can have the desired effect. What is most effective and most appropriate will vary depending on the situation. For example, a small variation in lot widths can be quite noticeable and effective in a small-lot subdivision, while on large lots it might contribute little if any perceived variety and another technique would be more useful.

To illustrate these techniques, sketches of single family subdivisions are shown. Variety in larger-lot subdivisions and cluster housing follows the same principles, but tends to come about more naturally in those developments because the larger sites offer more opportunities.

- Varied front setbacks. Placement of homes and garages close to or back from the street creates different patterns of visible open space. The buildings themselves, when close to the street, also add diversity to the view (Figure IV-46).
- Interruption of straight streets. On straight roads, knuckles or cul-de-sacs can be introduced to limit the length of straight stretches (Figure IV-47).
- Varied garage placement and orientation. When lot size permits, garages can be on the front side of the house and can be entered from the front or side. They can also vary in size. Garages can be detached and connected to the home by breezeways. Attaching a detached garage to the garage on the adjoining lot creates interesting relationships between buildings and open space (Figure IV-48).

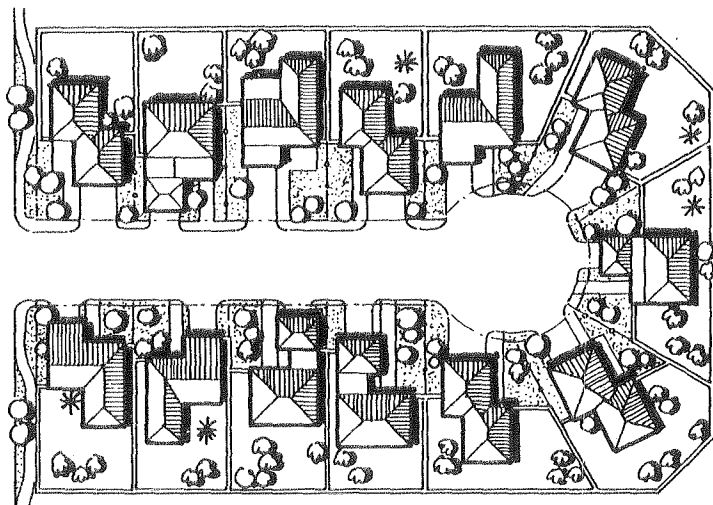


FIGURE IV-46
Varied Front Setbacks

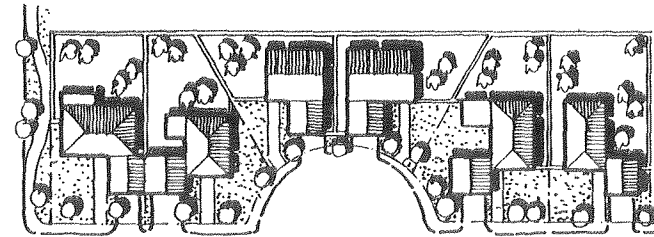


FIGURE IV-47
Interruption of Straight Streets

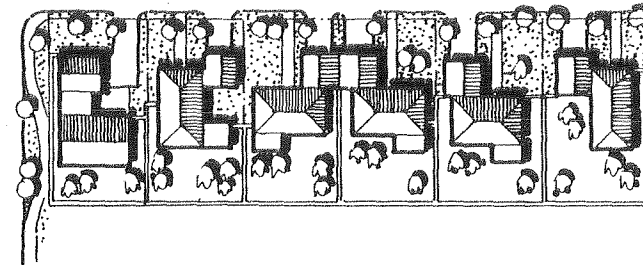
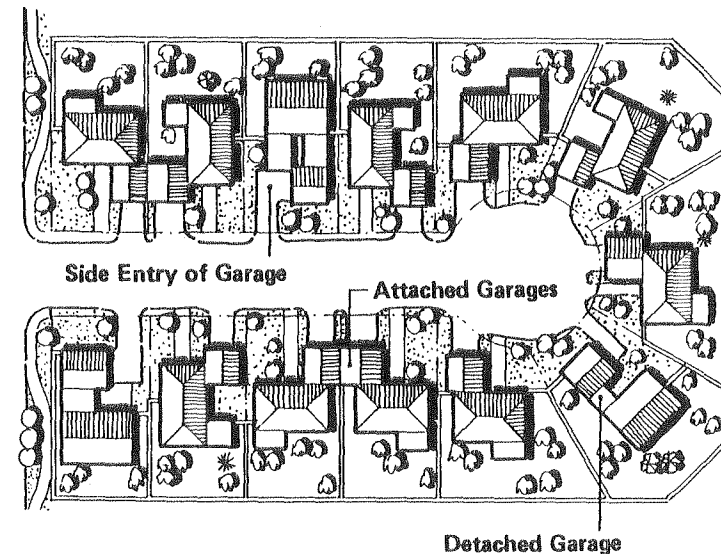


FIGURE IV-48
Varied Garage Placement and Orientation



Detached Garage

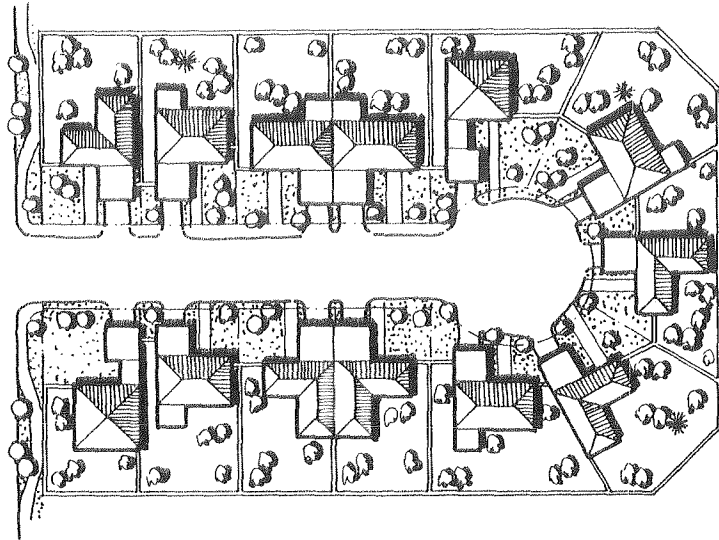


FIGURE IV-49
Varied Side Yard Setbacks

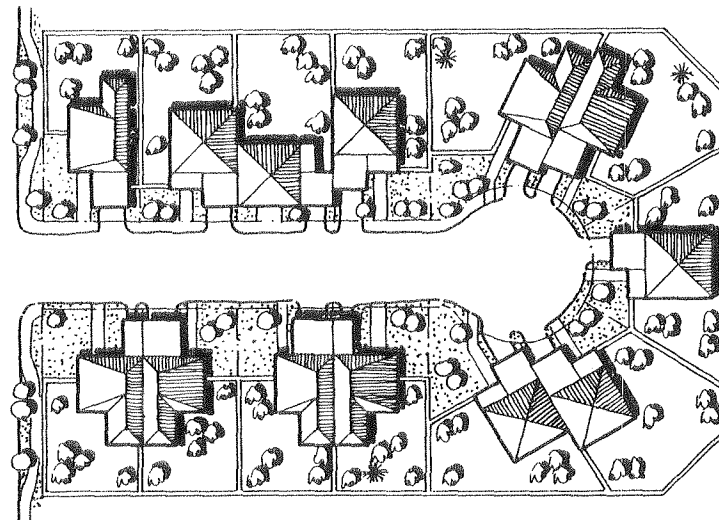


FIGURE IV-50
Attached and Detached Housing

- Varied side yard setbacks. Varying the distance between adjoining homes, or between homes and fences, results in different types of yards and private patio areas (Figure IV-49).
- Attached and detached housing. Attached homes can look like two or more separate dwellings, or can look like one large home, depending on how they are treated. Attached housing can yield larger open areas between buildings (Figure IV-50).
- Center plot and zero lot line. Zero lot line homes (flush to the lot line) lend themselves well to creation of courts and patios that a traditional center plot may prevent. The two yield different street scenes (Figure IV-51).

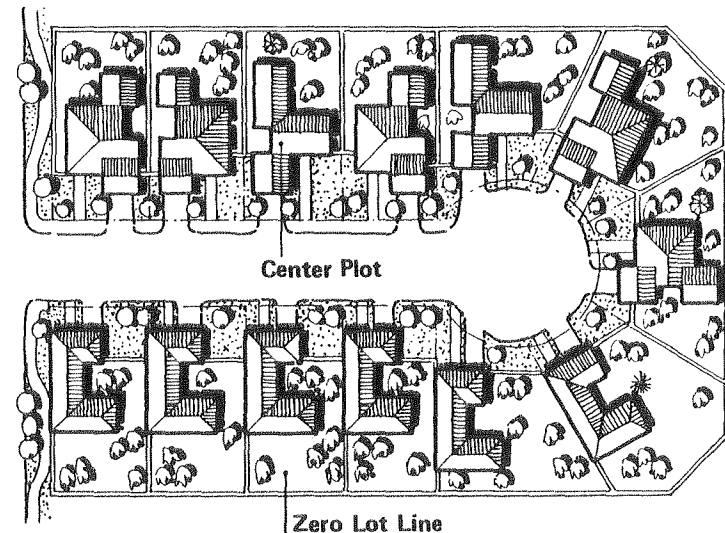


FIGURE IV-51
Center Plot and Zero Lot Line

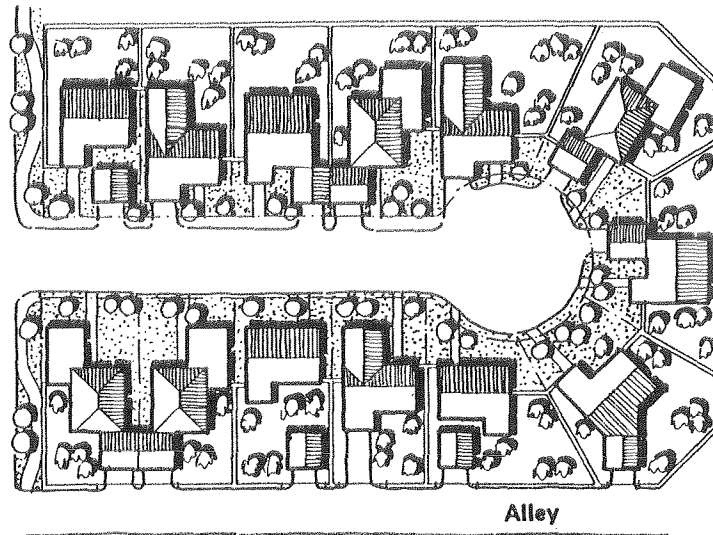


FIGURE IV-52
Front Load vs. Rear Load

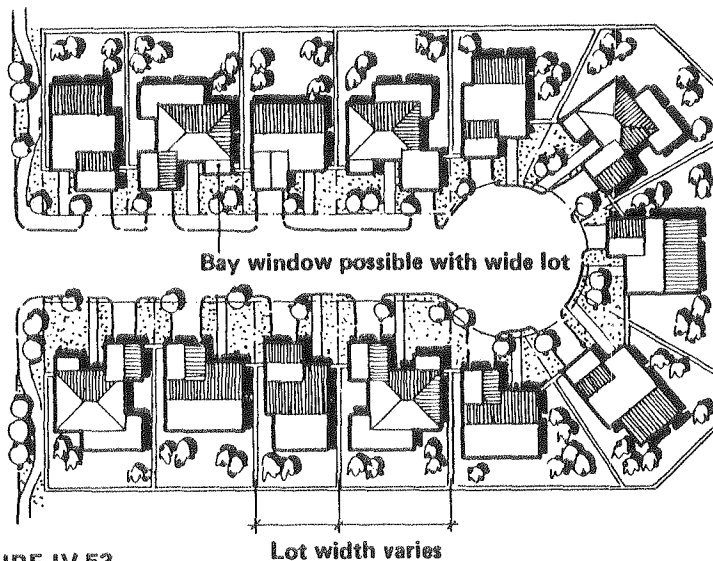


FIGURE IV-53
Varied Lot Widths

- Front load vs. rear load. Garages can be entered from the street in front (front load) or from the street or alley behind (rear load) (Figure IV-52).
- Varied lot widths. Making some lots wider, and some narrower, than the average can provide different amounts of open area between buildings. It also allows placement of different shapes and sizes of homes. On narrow lots, a variation of only three or four feet can make a perceptible difference (Figure IV-53).
- Lot orientation. On curves or at corners, lots can often be oriented in a different direction than those at midblock. In these cases some lots can be non-rectangular and angled to the street (Figure IV-54).

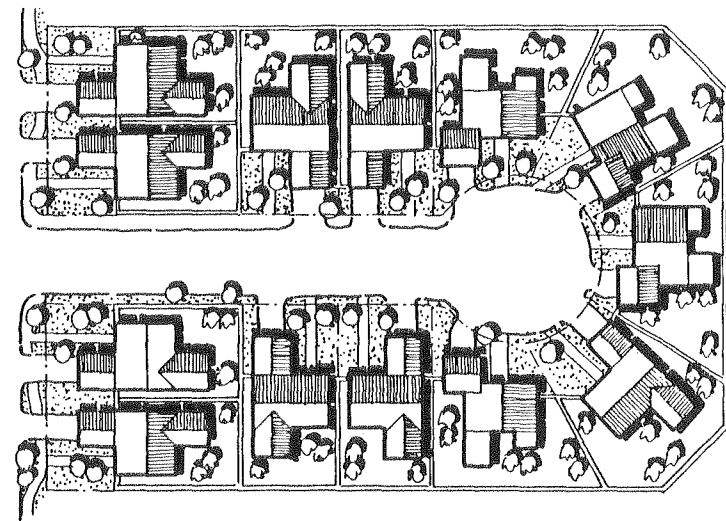


FIGURE IV-54
Lot Orientation

- **Building orientation.** Buildings need not necessarily be built perpendicular to street or property lines. With flexibility in setback and building separation criteria, siting for solar energy purposes can be improved (Figure IV-55).
- **Architectural style and massing.** The presence of 2-story, 1-story, and split elevations creates interest in a neighborhood. Such features as porches, trellises, and balconies also lend interest, as do variations in roof line and pitch (Figure IV-56).
- **Cul-de-sac treatments.** Closed cul-de-sacs, cul-de-sacs with openings for paseos or with open fences, and knuckles (facing open space, for instance) are some of the possibilities (Figure IV-57).
- **Straight and curved streets.** While straight streets are the most efficient, occasional curves can provide changing street scenes. Curves that seem very slight in a bird's-eye view are readily perceived by the driver and interrupt the line of sight (Figure IV-58).
- **Exterior building treatments.** Different colors, materials, and styles of exterior treatments, and such things as window placement, can give a neighborhood a personalized look.
- **Landscaping.** Trees, turf, ground covers, shrubs, flowers, and planting treatments can vary along the street (or even within the pavement, as with medians or turnaround planters).

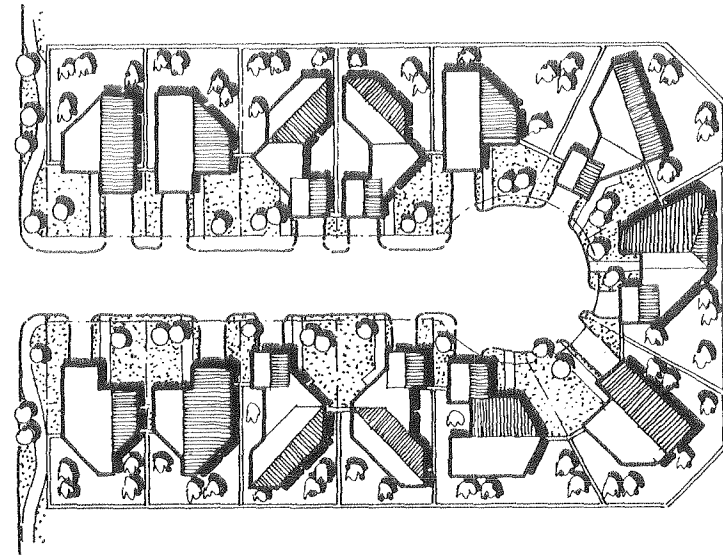


FIGURE IV-55
Building Orientation

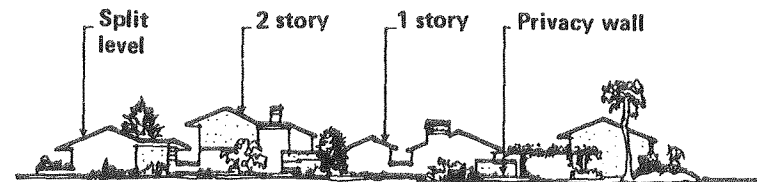


FIGURE IV-56
Architectural Style and Massing

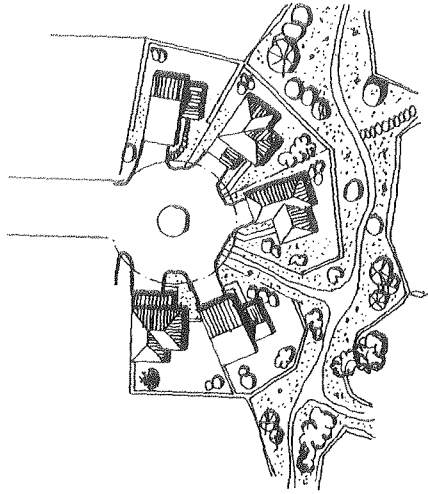


FIGURE IV-57
Cul-de-Sac Treatments

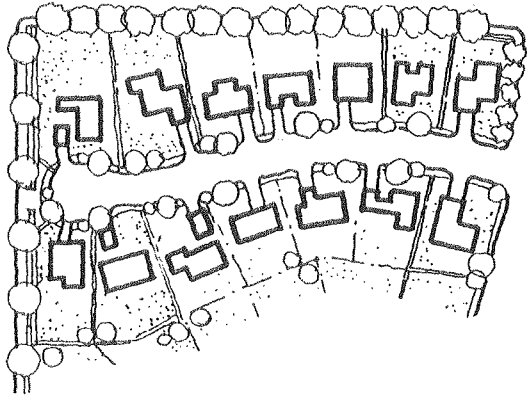
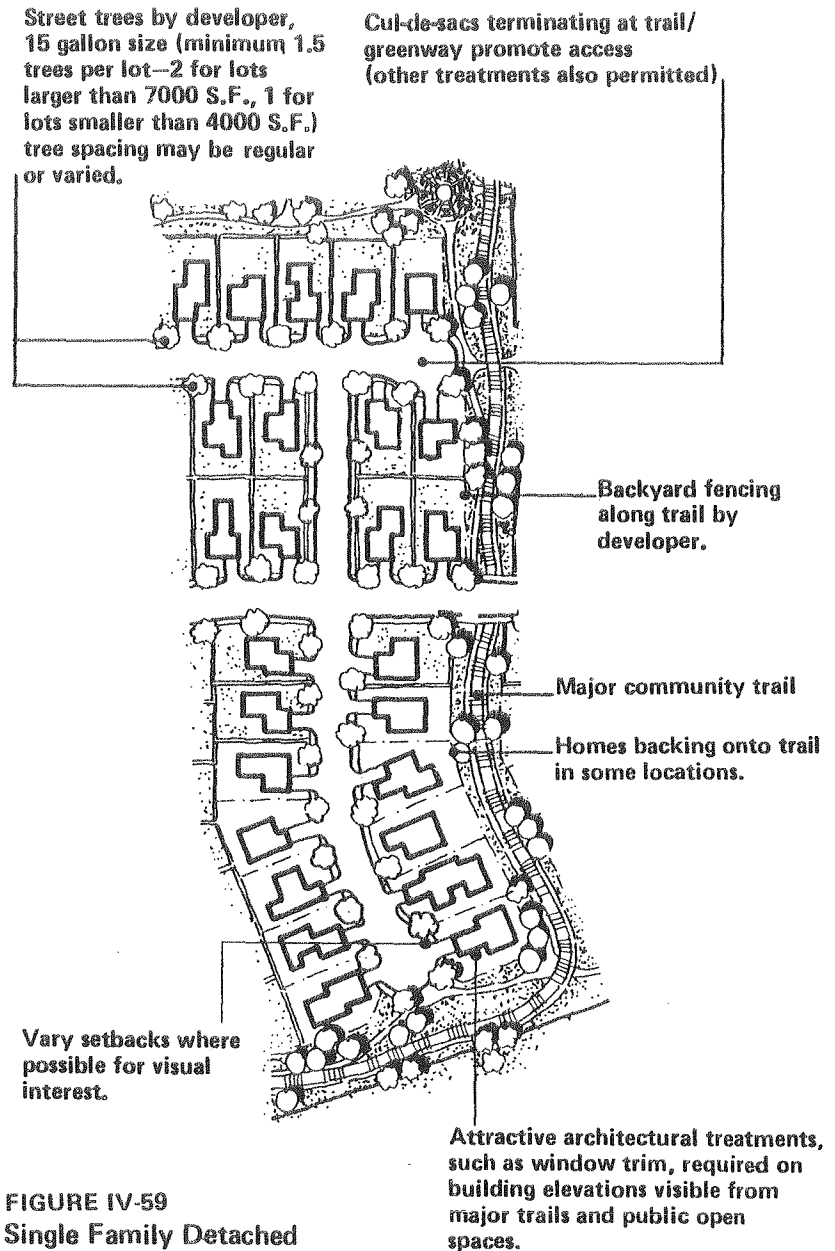


FIGURE IV-58
Straight and Curved Streets

Site Planning Guidelines

Many types of housing will be built in Terra Vista. It is the intent of the plan to encourage diversity and to provide the greatest possible flexibility for the introduction of all types of development. Each type of housing carries its own site planning imperatives. New forms of housing, not even known today, will doubtless come into existence during the life of the planned community. Therefore, it is not the role of this plan to establish rigid site planning criteria that may preclude innovation and variety. The goal is, rather, to ensure that every development, no matter what form it may take, will be compatible with the planned community and will respect the features unique to Terra Vista.

The following sketches (Figures IV-59 to IV-63) are conceptual site plans of various types of housing that are being developed today. Obviously, each of these plans represents only one out of thousands of possible arrangements of buildings, streets, and open space. These sketches are provided to illustrate the various forms of development that can be anticipated in Terra Vista and to show how each can relate well to Terra Vista's special elements.



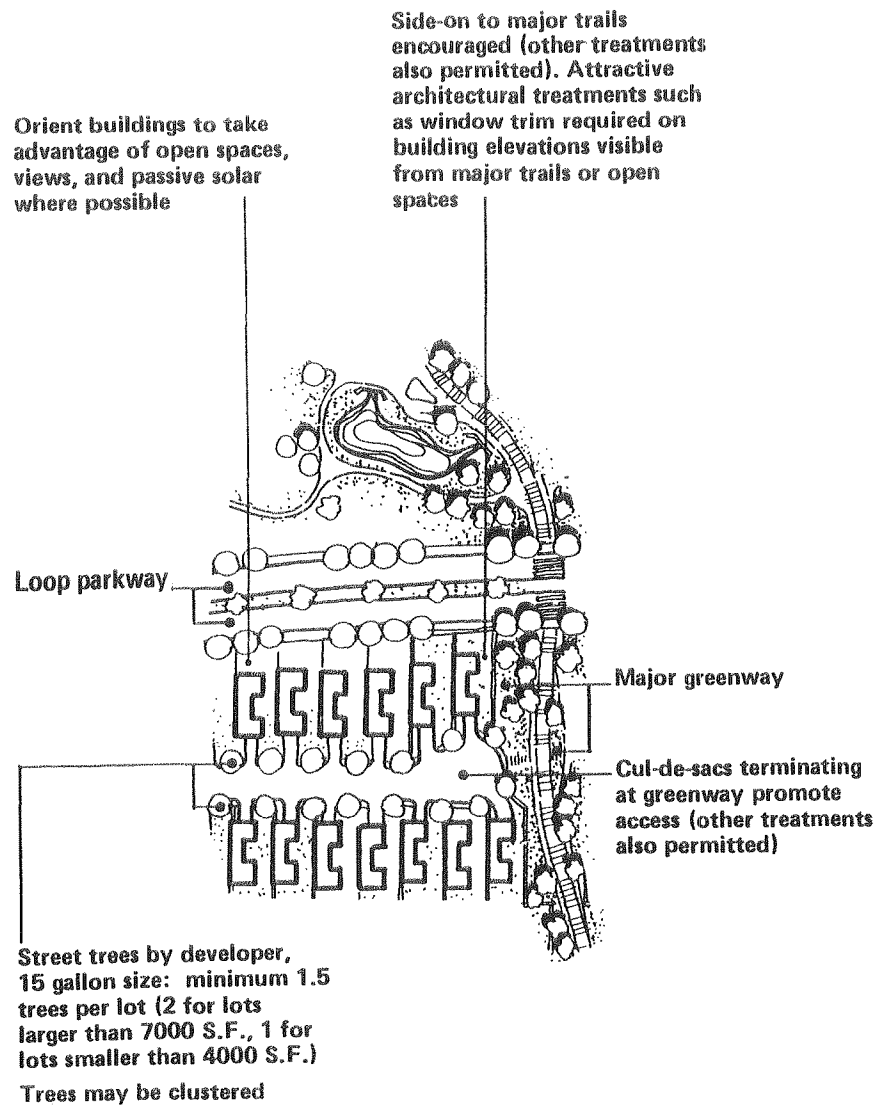


FIGURE IV-60
Single Family Detached Zero Lot Line

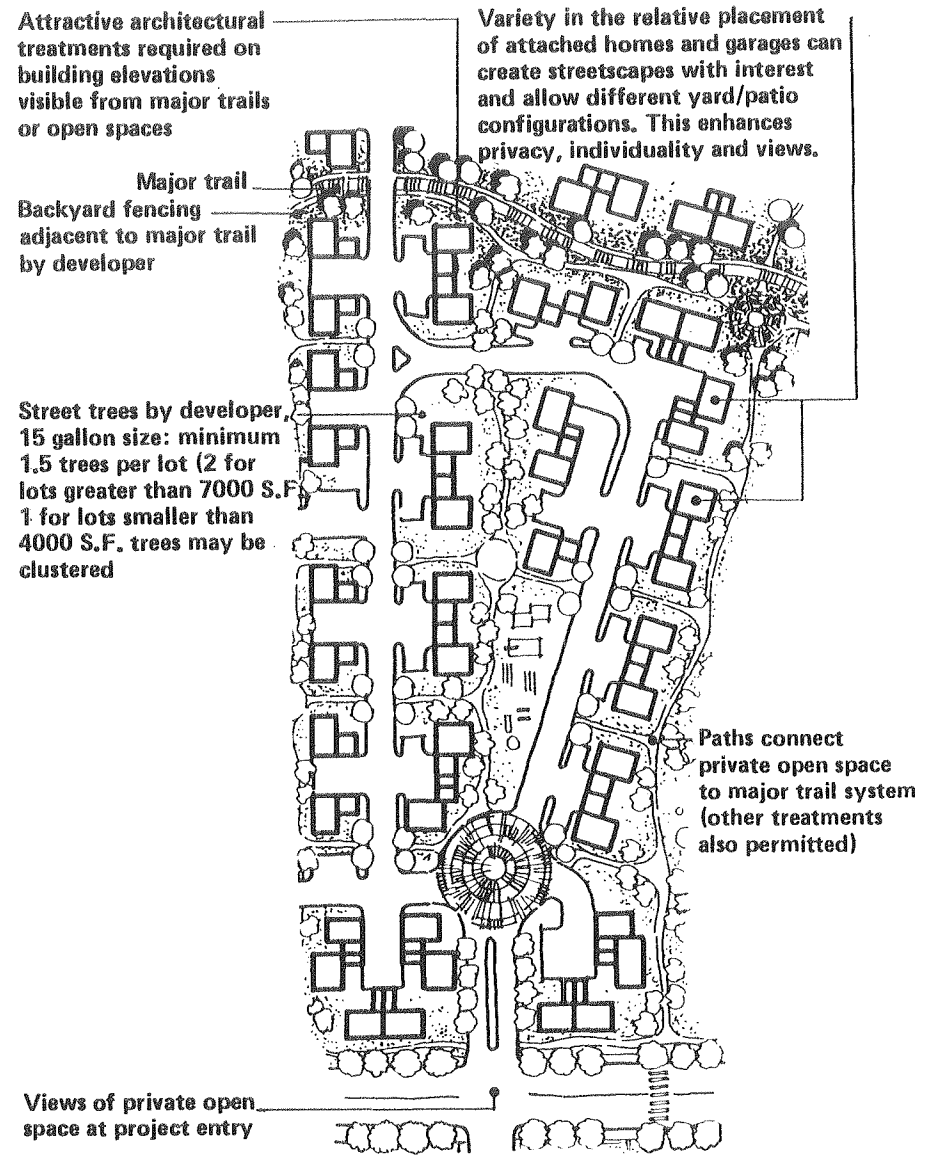


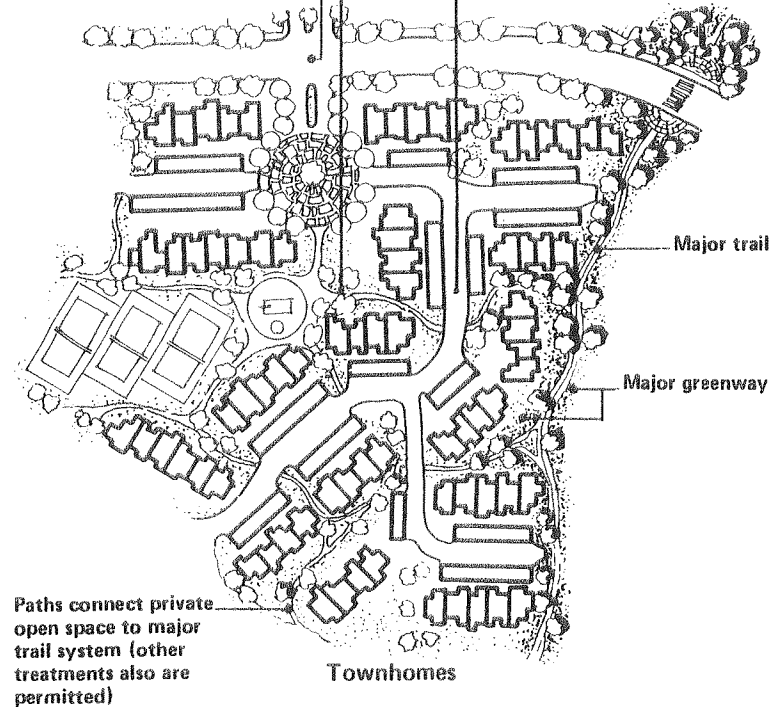
FIGURE IV-61
Single Family Attached Duplex

Street trees can blend with other landscaping. Landscape treatment subject to design review

Avoid large parking areas visible at main entry

Special landscape treatment at main entry

Where possible, use open spaces, landscaping, and/or curved streets to interrupt long, straight corridors of parking spaces



High density developments typically will include some parking under the dwellings

Fencing treatments may vary (or be absent) depending on noise constraints and the need to screen parking areas or private patio - see buffer guidelines

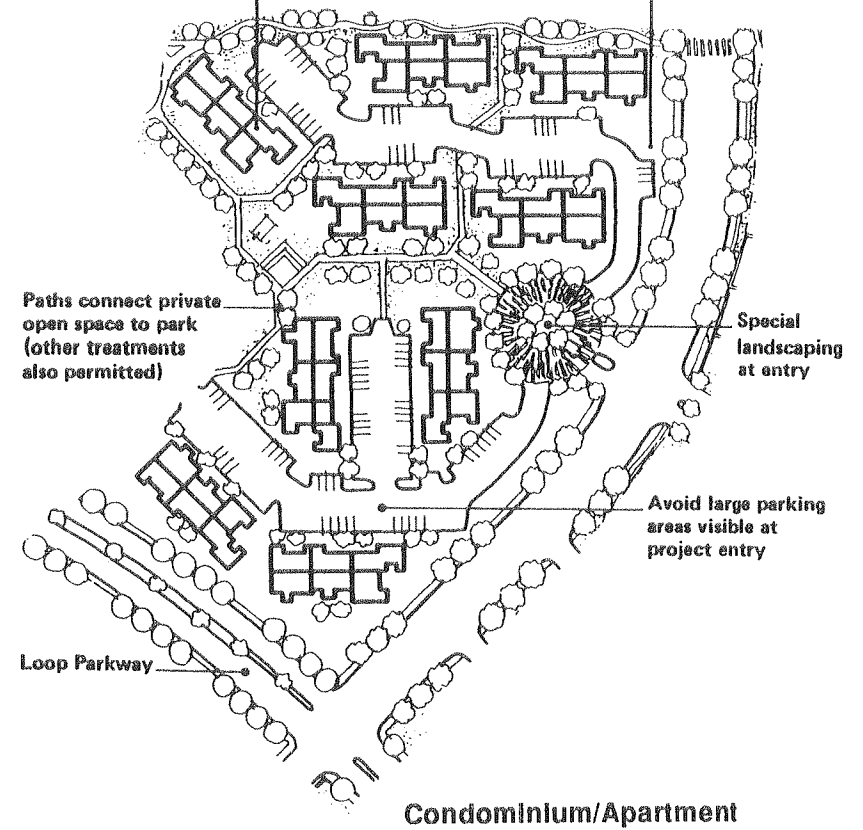


FIGURE IV-62
Multifamily

Foothill Boulevard Centers Concept

Special attention has been focused on the character of development along the southern boundary of Terra Vista, the community's frontage on Foothill Boulevard. This special attention is warranted for the following reasons:

- Foothill Boulevard is a primary travel route in the region; more people will be viewing Terra Vista from this vantage point than from any other.
- Since Terra Vista is at the geographical heart of the City of Rancho Cucamonga, views of Terra Vista will contribute greatly to people's impressions of the City as a whole.
- Land along Foothill Boulevard, by virtue of its exposure to thousands of passers-by, has the potential of contributing substantially to the City tax base if developed to its full commercial potential.
- Foothill Boulevard represents a transition from residential development north of the boulevard to non-residential development south of the boulevard, and, consequently, development along Foothill Boulevard in Terra Vista should act as a buffer between--and should complement--these land uses.

As the design guidelines are put into effect with each development along Foothill Boulevard, the result should be a visual impression of a variety of building types and scales, a comfortable rhythm of open and developed spaces, and melding of business, institutional, and residential functions.

As described in the commercial land use section of Chapter III, the Foothill Boulevard frontage has been organized into a series of centers, each intended to feature a unique mix of land uses, complementary building shapes and sizes, and distinctive character. These centers allow for many efficiencies, including:

- Shopping and business in a central location, providing maximum convenience and minimum travel for Rancho Cucamonga residents and employees
- Easy access from both Foothill Boulevard and Church Street, resulting in smoother traffic flow and greater convenience
- Direct pedestrian access from the Terra Vista greenway system along trails, bikeways, and sidewalks, allowing community residents to eliminate many automobile trips
- Location on primary local and regional transit routes, allowing convenient bus travel
- Primary vehicular access to each center from cross-streets, rather than from Foothill Boulevard, allowing a smoother flow of traffic along the boulevard
- Opportunities for the sharing of parking areas and other facilities by complementary uses

The result is that the centers along Foothill Boulevard can function as true "one-stop" multi-purpose attractions for areawide residents. The fact that the centers will be well planned, attractively landscaped, and easily identified will help to make

the centers the place to go rather than one or two individual entities within them. Because of this, all the concerns located in the centers should benefit.

The overall design concept for the centers along Foothill Boulevard is best described by breaking down the centers' design guidelines into the following four components:

- Center identity and specific uses
- Access and community gateways
- Site planning and pedestrian network
- Landscape treatment

Each of these components is described below in terms of how they will affect the image and appearance of Terra Vista from Foothill Boulevard. Following this discussion are individual profiles of each center.

Center Identity and Specific Uses

Each center along Foothill Boulevard is intended to cater to a different mix of business, office, professional, and residential functions. For convenience, each has been identified by a preliminary name reflecting the types of uses currently expected to predominate within it (Figure IV-63):

- Town Center
- Commercial Park
- Financial/Restaurant Area

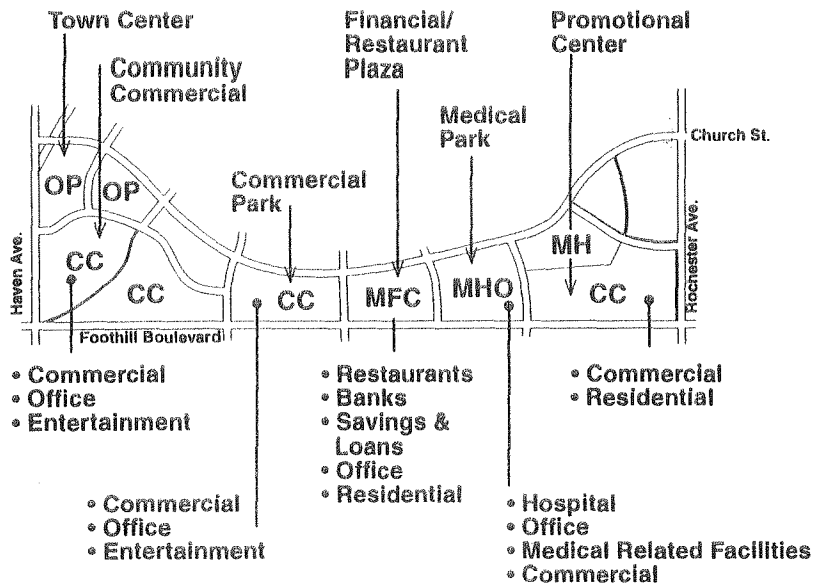


FIGURE IV-63
Foothill Boulevard Center Identity and Predominant Uses

Gateways. Gateways are another major means of punctuating the Foothill Boulevard frontage, as described in the Foothill Boulevard Specific Plan. Gateways are also located on the north side of Foothill Boulevard, as indicated in Figure IV-64. Type I gateways are provided at Milliken Avenue and Rochester Avenue. Type II gateways are located at the corners of Church Street and Type III gateways are provided at Spruce, Elm, and Mayten Avenues. Each of the Type II and Type III gateways will have a consistent treatment, as described in the landscape section earlier in this chapter.

Site Planning and Pedestrian Network

Each center along Foothill Boulevard will have a different appearance from the boulevard within the context of the uniform Terra Vista design image. As indicated conceptually in Figure IV-65, the arrangement of buildings, parking areas, and pedestrian walkways is unique to each center. For example:

- In the Town Center, smaller buildings are clustered close to Foothill Boulevard and Haven Avenue, while more imposing commercial structures are set back from these streets. The primary greenway spine leads directly to the Town Center community commercial center and to the east-west pedestrian linkage through the Foothill Boulevard centers.
- In the Commercial Park, as in the Town Center, larger buildings are set back away from Foothill Boulevard and the smaller buildings are located near Foothill Boulevard and along a central park-like entry. A large open space with a pedestrian spine traverses the site from north to south. Primary parking access will be from Foothill Boulevard, Spruce Avenue across from Town Center Drive, and from Elm Avenue. Service access faces Church Street with ample room for

landscaping screening from Church Street. The pedestrian pathway along Elm Avenue will connect with the Trail System and La Mission Park.

- In the Financial/Restaurant Plaza, buildings will encircle the site; centralized parking will serve all establishments but be hidden from view from Foothill Boulevard. The pedestrian walkway will assume a circular configuration around the parking area.
- In the Medical Park, as with the Corporate Park, a generous open space within the campus will be the focus for buildings. Larger health care facilities will be set well back from Foothill Boulevard. A pedestrian path will lead to the site and will, in turn, connect with the trail system.
- In the Promotional Center, the large buildings will be set well back from Foothill Boulevard with loading areas screened from public view. Smaller retail and restaurant buildings will be located close to the street. A pedestrian path along the storefronts will lead through the area in an east/west orientation. The landscaped pedestrian way along Rochester Avenue will help to buffer commercial and residential developments with the following features:
 1. north/south direction promenade walkways to break up parking area, and
 2. a continuous east/west promenade included on site, apart from Foothill Boulevard, to connect all buildings, and
 3. promenade linkages, similar to those in Terra Vista Town Center, with wide landscaped areas, continuous walkways, furniture, trellis and pedestrian plazas/patios.

The result of this carefully conceived site planning, coupled with detailed designs for each center that will be reviewed prior to site development, should be a development pattern that uses scale, rhythm, and variety to excellent advantage.

- Medical Park
- Executive Park
- Promotional Center

The discussion in Chapter III on commercial land uses in Terra Vista described the unique mix of specific commercial facilities in each center that complements adjoining commercial centers yet promotes variety and identity.

Access and Community Gateways

Access. Within the Terra Vista community, primary access to each of the centers along Foothill Boulevard will be from Church Street and north-south streets connecting Church Street with Foothill Boulevard, as indicated diagrammatically in Figure IV-64. The reason for this access concept is twofold:

- Smoother Traffic Flow. Anticipated traffic levels along Foothill Boulevard are relatively high; the removal of primary access points from Foothill Boulevard allows for smoother traffic flow along the boulevard. Left turns into centers will generally be confined to the intersections and locations shown, since such movements at other than strategically spaced locations can inhibit traffic flow.
- Better Traffic Distribution. Access from Church Street and from cross streets perpendicular to Foothill Boulevard will help to distribute traffic more equally between Church Street and Foothill Boulevard than would be possible with primary Foothill Boulevard access points only. Terra Vista Residents will have no need to use Foothill Boulevard for intra-community trips for shopping, entertainment, and other daily needs.

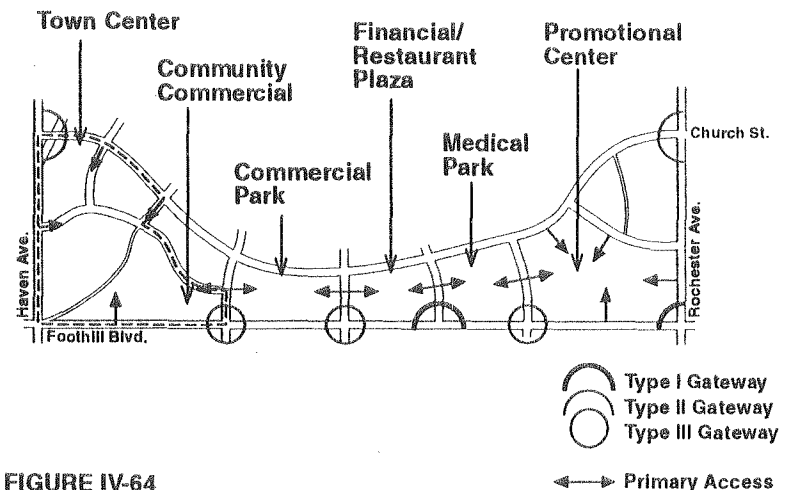


FIGURE IV-64
Primary Access and Gateways Concept for Church Street

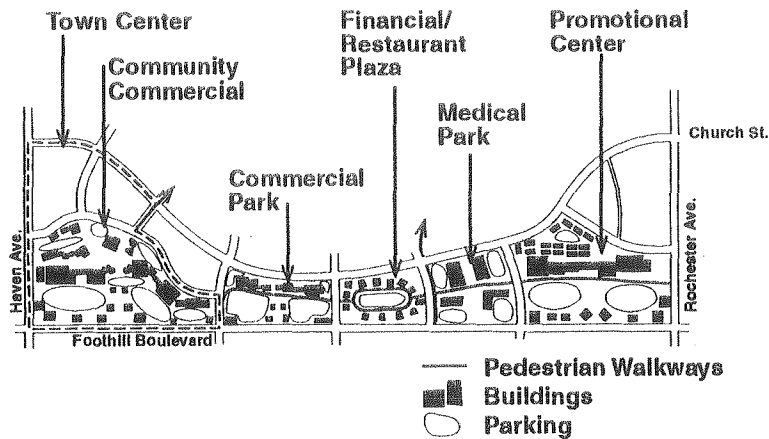


FIGURE IV-65
Site Planning Concept for Foothill Boulevard

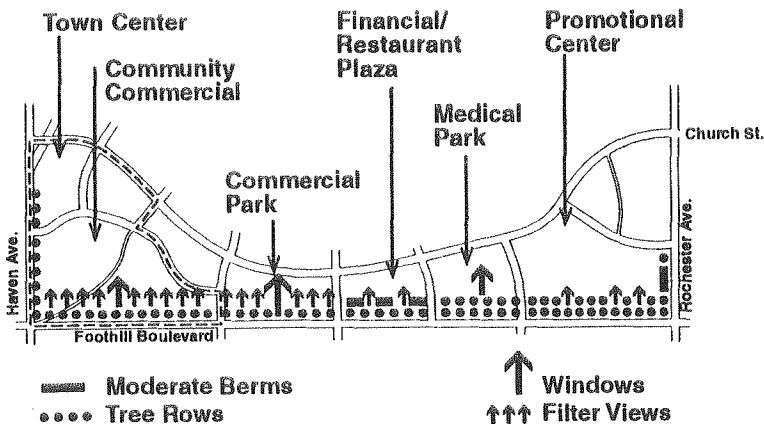


FIGURE IV-66
Landscape Concept along Foothill Boulevard

Landscape Treatment. Each center has been carefully evaluated from the standpoint of landscaping to create an attractive, manicured image for Terra Vista along Foothill Boulevard. The various components that have gone into the landscape concept include:

- Tree rows
- Berms
- View filtering devices (planting and hedges)
- View opportunities for "windows" to the shops and mountains beyond

As indicated diagrammatically in Figure IV-66, trees symbolizing the Terra Vista community will line Foothill Boulevard, in conformance with the Foothill Boulevard Specific Plan. In addition, each center along Foothill Boulevard will have a unique mix of landscaping treatments in conformance with the building arrangement within the site. For example:

- At the community commercial center within the Town Center, edge plantings will be provided to filter views of the landscaped parking areas but will still allow the larger buildings beyond to be clearly seen by motorists along Foothill Boulevard.
- There will be three primary landscape features at Commercial Park in addition to parking lot landscaping: 1) a landscaped area bisecting the site from north to south containing special decorative paving and a canopy tree-covered parklike area with outdoor dining and other pedestrian amenities, 2) edge planting along Foothill Boulevard to filter views to the buildings in the distance with a special plaza combined with a gateway at the northeast corner of Spruce Avenue and a gateway at the northwest corner of Elm Avenue, and, 3) dense planting adjacent to Church Street to soften the rear view of the buildings and service areas.

- In the Financial/Restaurant Plaza, the parking area will be screened from motorists on Foothill Boulevard by the buildings themselves. Informal plantings will be provided around and between the buildings.
- In the Medical Park, there will be two rows of trees, plus landscaping in the parking areas along Foothill Boulevard. Landscaping and low berms may also be provided to screen parking areas further.
- In the Promotional Center, landscaping along Foothill Boulevard will offer windows into the commercial complex. Plantings along a portion of Rochester Avenue and a Type B trail will screen parking and loading areas and serve to reduce the scale of the large buildings on the site. Additional setback areas will buffer the project from the adjacent residential neighborhood to the east. The use of palms at project entries will tie this site with the other retail projects along Foothill Boulevard and reinforce community identity.
- In this last regard, the setbacks for both parking areas and buildings along Foothill Boulevard have been specially selected to address the Boulevard's unique role as a primary travel corridor in Rancho Cucamonga.

As noted in the section earlier in this chapter on landscaping, setbacks along Foothill Boulevard from the curb to parking areas will be 28 feet minimum (43 feet average), and from the curb to buildings, 38 feet minimum (43 feet average).

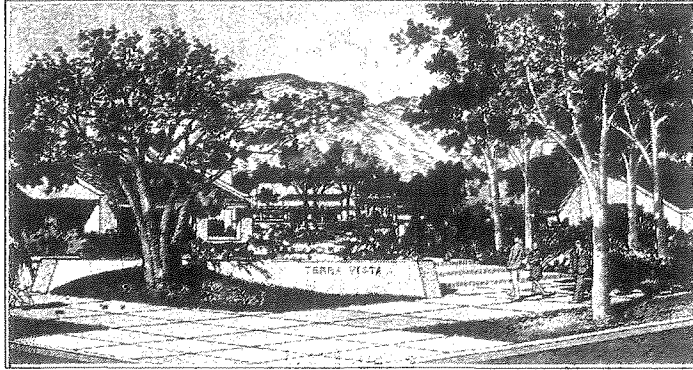
It was felt in planning the Foothill Boulevard frontage that the objectives of variety and visual interest would

be best served by a combination of techniques that would be used in conjunction with setbacks. These techniques, which are described elsewhere in this section, include:

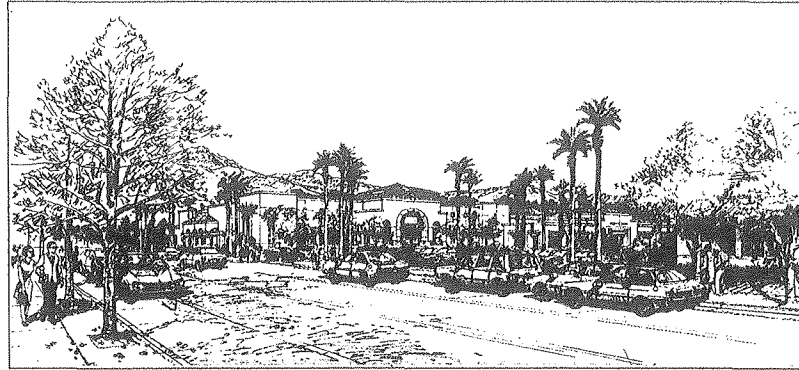
- Specifying average setbacks (43 feet from the curb to parking areas or buildings) in addition to minimum setbacks, assuring that many buildings will be set well back beyond the minimum required distance
- Site planning guidelines for several of the parcels along Foothill Boulevard calling for very generous setbacks, taking on the image of landscaped parks
- Berming that occurs along portions of the boulevard frontage within the Planned Community to soften views and screen parking areas
- Major "windows" into the projects periodically along the boulevard frontage, constituting large setbacks in special situations.

Summary. To provide an overview of the image one will have driving along Foothill Boulevard past the various commercial centers, a "panorama" of development along the boulevard has been prepared. Figure IV-67 offers a conceptual plan of the entire Foothill Boulevard frontage within Terra Vista and, in conjunction with this conceptual plan, a view or views of some of the major highlights along the corridor. (Larger reproductions of these views will follow.) As indicated by Figure IV-67, development along Foothill Boulevard--acknowledging that actual development may differ according to specific development programs but will adhere to the spirit and quality portrayed--will be epitomized by variety, attractive clusterings of buildings and open spaces, and an image that builds on the unique heritage and qualities of the City of Rancho Cucamonga.

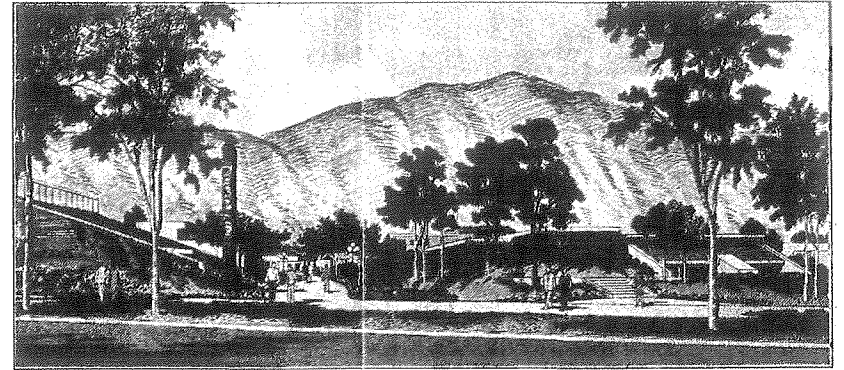
A. Town Center



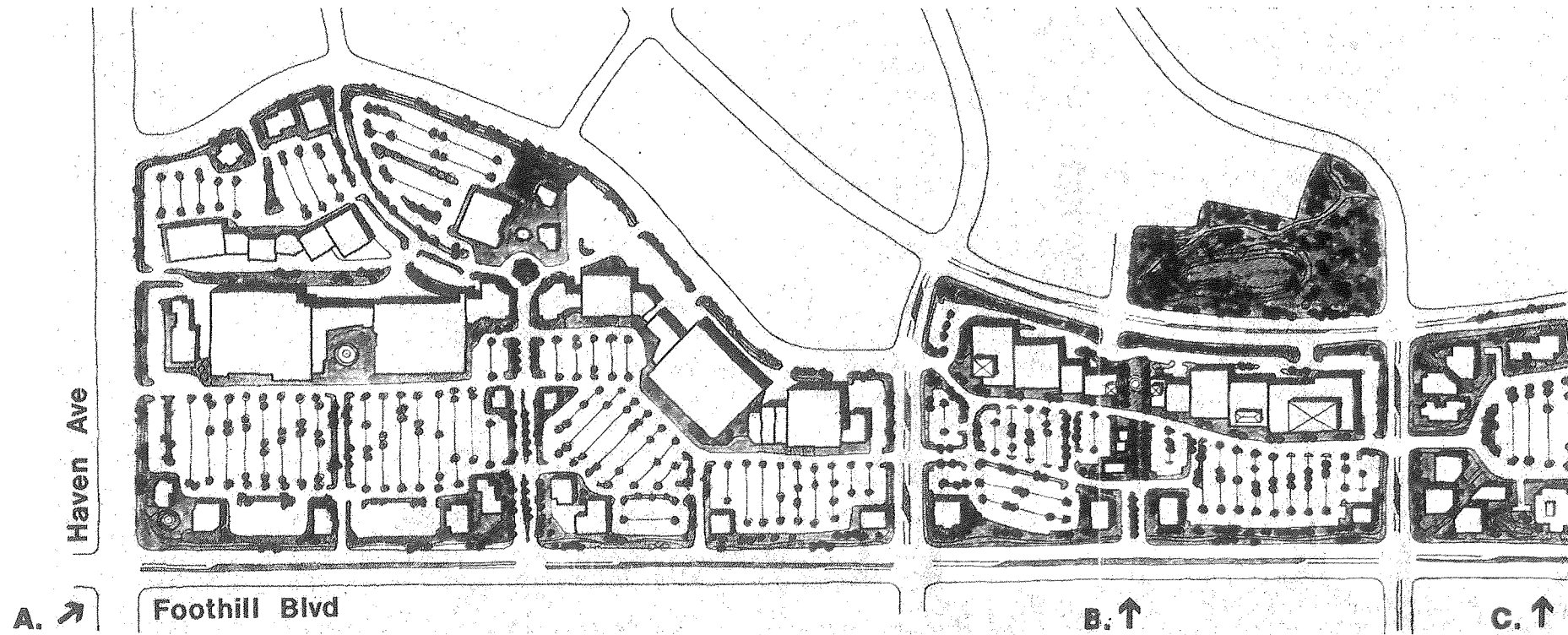
B. Commercial Park



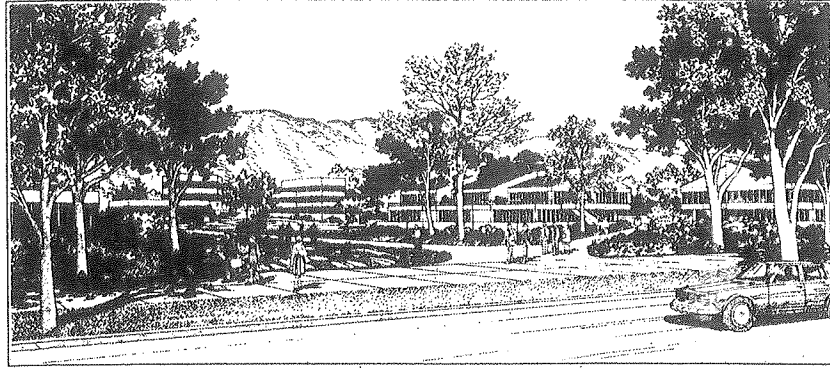
C. Financial/Restaurant Plaza



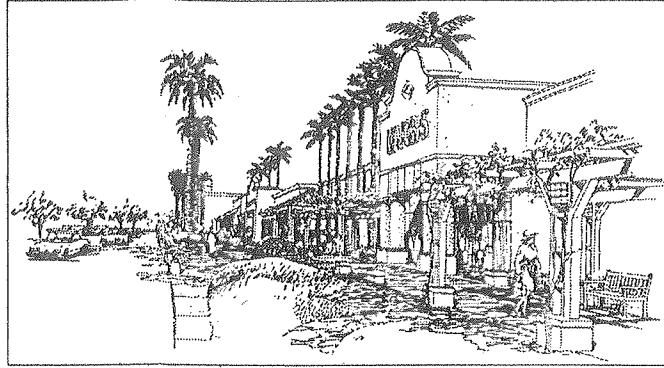
D. I



D. Medical Park



E. Promotional Center



F. Promotional Center

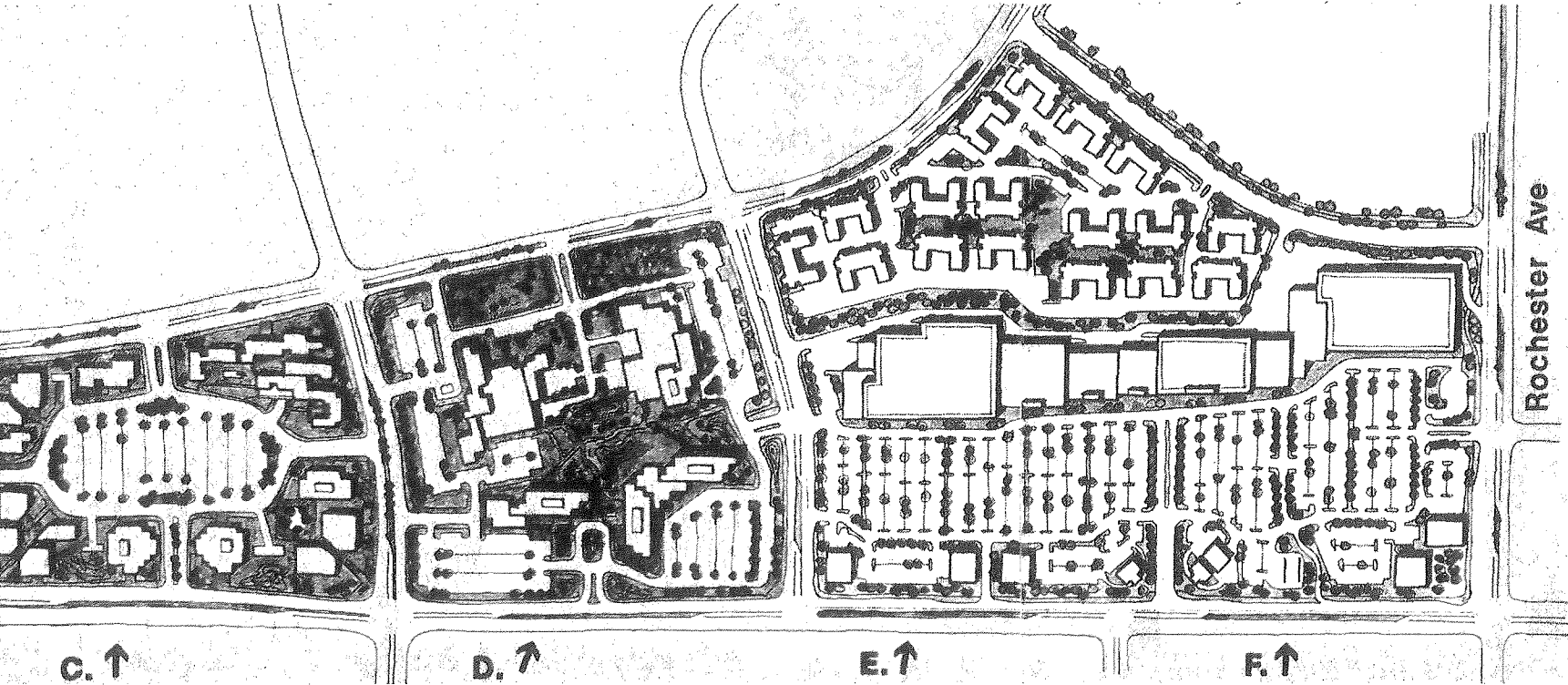
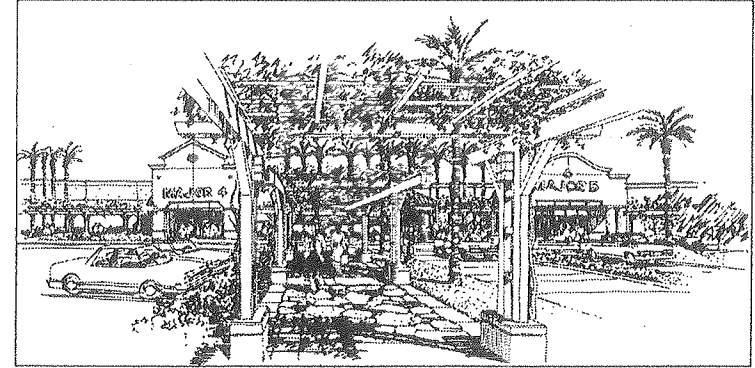


FIGURE IV-67
Panorama of Foothill Boulevard

Community Commercial Center (Town Center). The Community Commercial Center located on Foothill Boulevard between Haven Avenue and Spruce Avenue will be a one-stop commercial center serving Terra Vista and Rancho Cucamonga, supplementing other neighborhood/village commercial centers with a broader range of stores (Figure IV-68). The center as currently envisioned would have at least two department stores, numerous tenant stores, and recreational and eating facilities. The site is also expected to have offices integrated with the community commercial development.

The center will offer an attractively landscaped environment that will encourage strolling, window shopping, and pausing to relax at shaded seating clusters. The various buildings that make up the center can be joined together by covered walkways sheltering pedestrians and conveying themes such as arbors, trellises, and vines. Through this architectural treatment, which will be a low profile and set off by open space, the architectural heritage of Rancho Cucamonga will be recalled.

Landscaping will play a major role in views of the center from Foothill Boulevard. Earth berms will minimize views of parking areas adjacent to the boulevard, while still allowing the buildings to be seen from the road. The masses of the various buildings in the center will be divided so as to be human scaled and not to appear monolithic. Through the rhythm that will be created by the interaction of landscaping, buildings, and landscaped parking areas, no one element will dominate views from Foothill Boulevard and a varied visual image will result.

Furthermore, by orienting a number of smaller buildings around an open space at the corner of Foothill Boulevard and Haven Avenue, an attractive image of the center (and of the community) is provided to people driving along both arterials (Figure IV-69). Through the use of landscaped pedestrian connections through the center and parking areas, the major greenway spine, which terminates at the northeast side of the community commercial parcel, is effectively brought through the center all the way to the corner.

Commercial Park. Commercial Park will provide a wide variety of services complementing those found in the Town Center, located directly to the west. Major stores, small shops, offices, restaurants and entertainment facilities will offer an exciting mixture of shopping, employment and recreation activities. Commercial Park's architectural character, landscape and streetscape will complement and be compatible with the Town Center. Through the center of the site, a north-south view corridor links Foothill Boulevard, La Mission Park, and the mountains. Within the view corridor is a wide landscaped open space with a decoratively paved pathway lined with palms and shady canopy trees, leading into an intimately designed plaza featuring a splashing fountain, clustered outdoor seating and possibly a sculpture or other work of art (Figure IV-70). The plaza will be an exciting, aesthetically pleasing place for activities such as meeting, relaxing, or dining at sidewalk cafes.

An interesting and varied atmosphere is created through careful building placement (Figures IV-71 and IV-72). More massive structures will be set away from the street

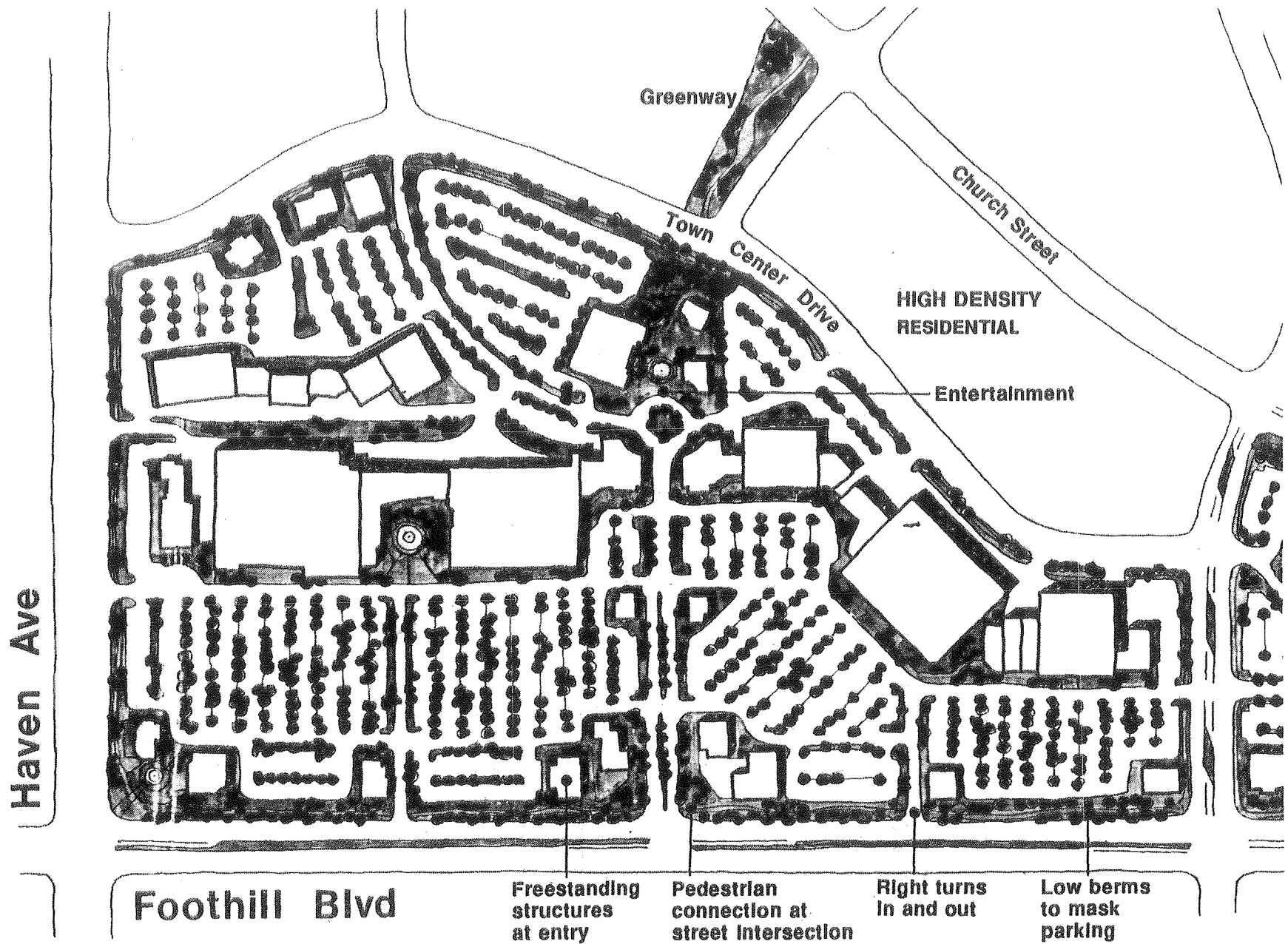


FIGURE IV-68
Conceptual Site Plan of Community Commercial Center

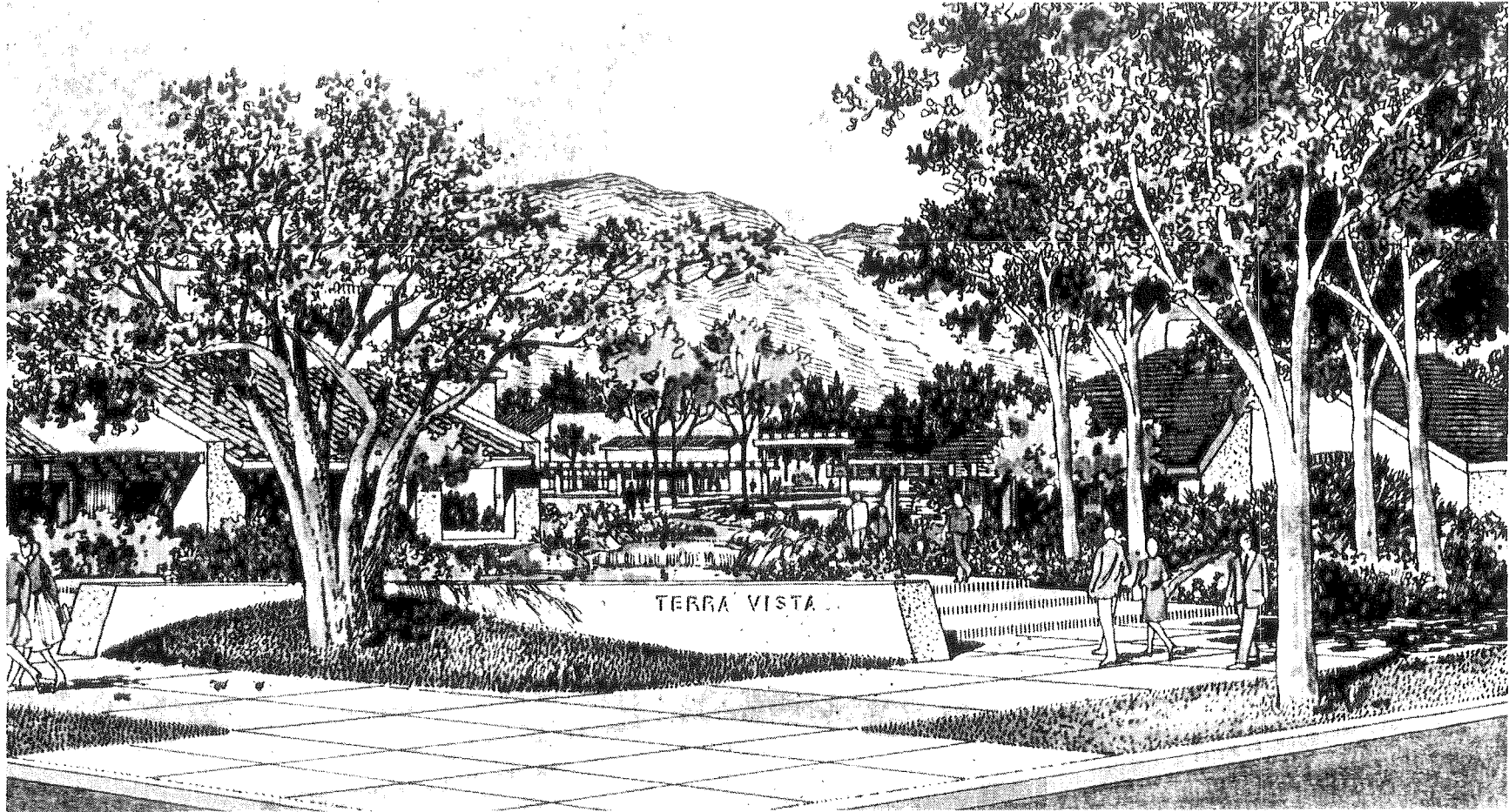


FIGURE IV-69
View from Foothill Boulevard of Community Commercial Center

while smaller buildings will be located along the "pedestrian spine" and on corner lots adjacent to Foothill Boulevard. To maintain a pedestrian-friendly environment, parking and service entrances will be screened by dense landscaping. Along Church Street, informal clusters of dense, columnar trees and a full hedge will screen service areas and form an evergreen backdrop to the Church Street streetscape with its median landscaping and informal deciduous street trees. This dense landscape screen and the placement of building pads and service areas below the Church Street elevation will provide a land use and building scale transition from the commercial uses to the medium-density housing and La Mission Park located north of Church Street. The architectural form of the buildings facing Church Street will be articulated and have a character similar to the Town Center.

Customer entrances to Commercial Park are from Foothill Boulevard, via a palm-lined entryway and from Spruce and Elm Avenues. Easy foot access is provided from the north and south by the pedestrian spine, from the Town Center to the west by a pathway running along the front of the stores, and from La Mission Park by a trail located along Elm Avenue. (Figure IV-70)

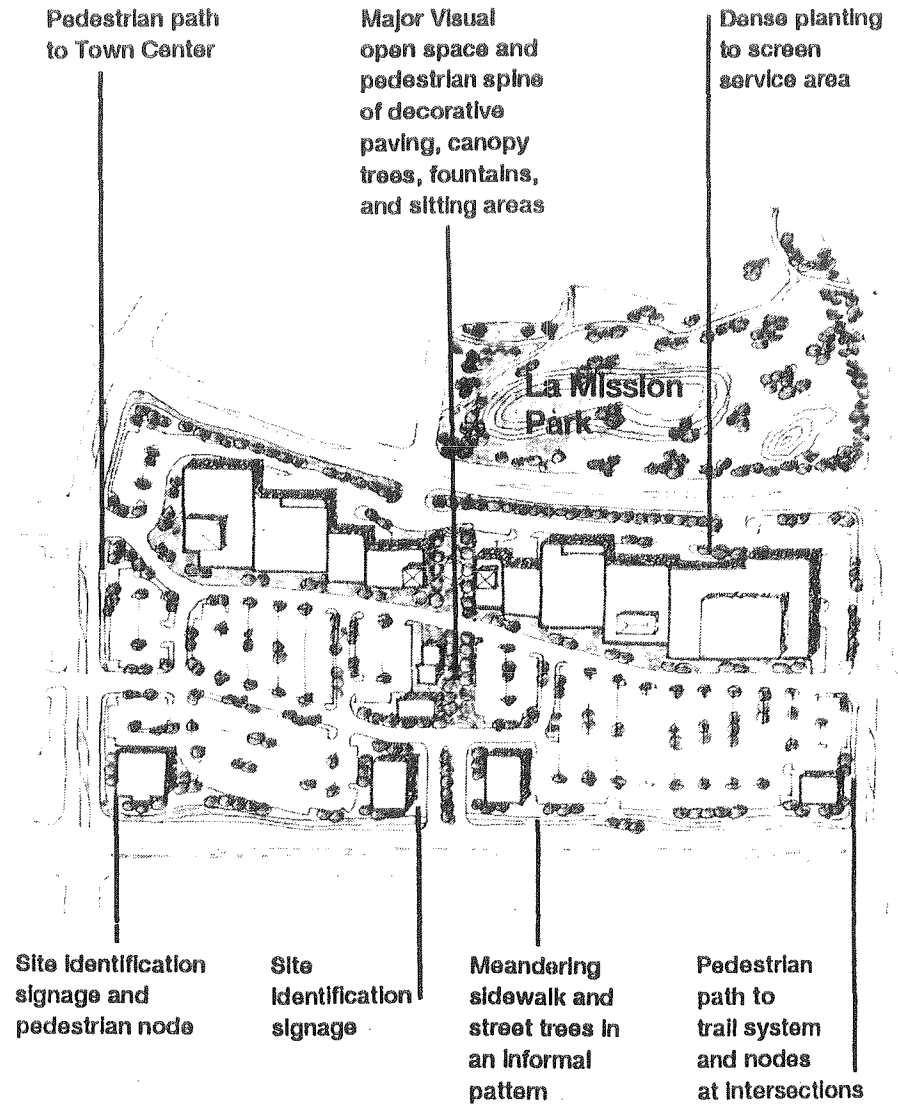


FIGURE IV-70
Conceptual Site Plan for Commercial Park

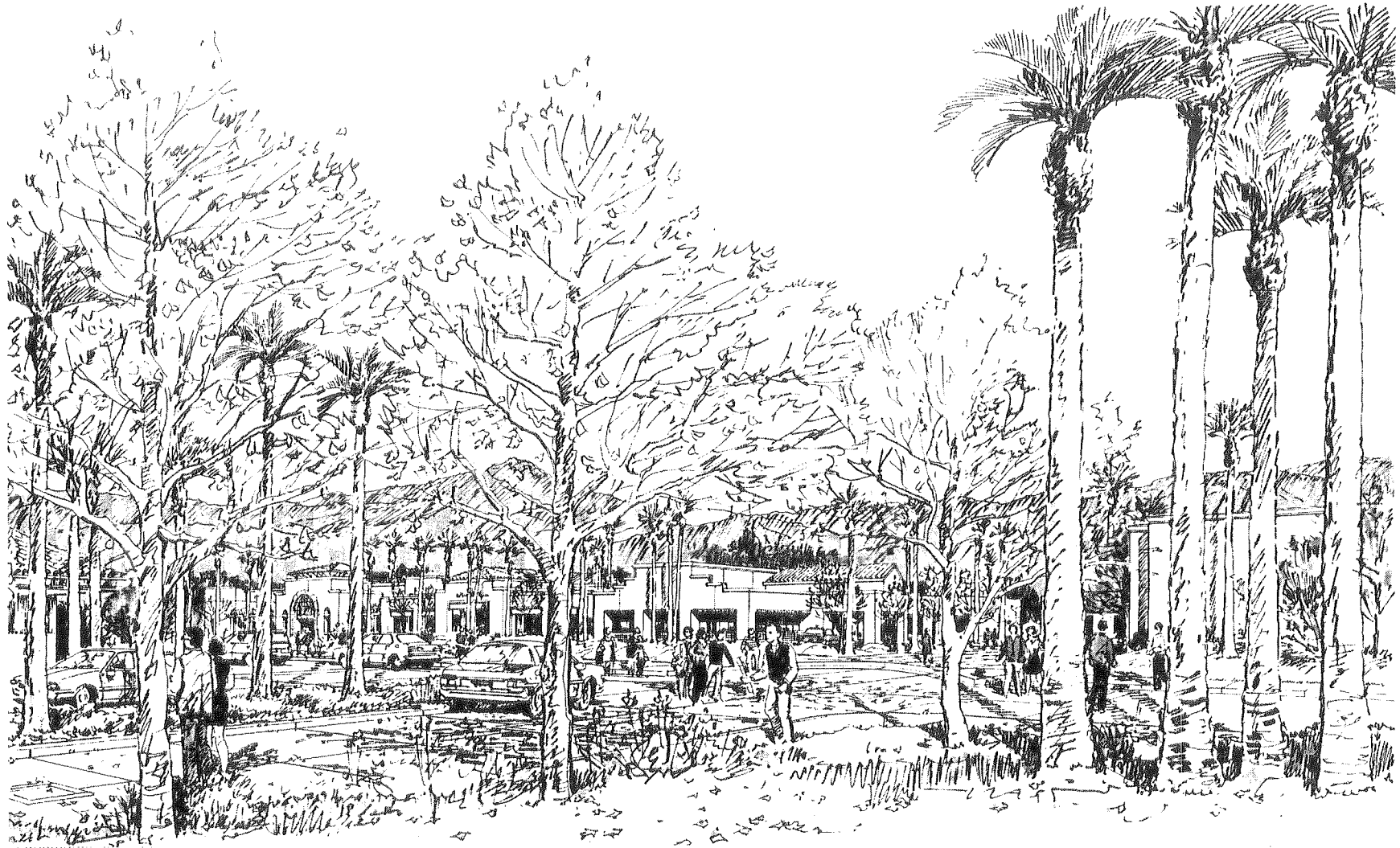


FIGURE IV-71
View of Commercial Park from Foothill Boulevard

REVISED Amendment No. 7
IV-58

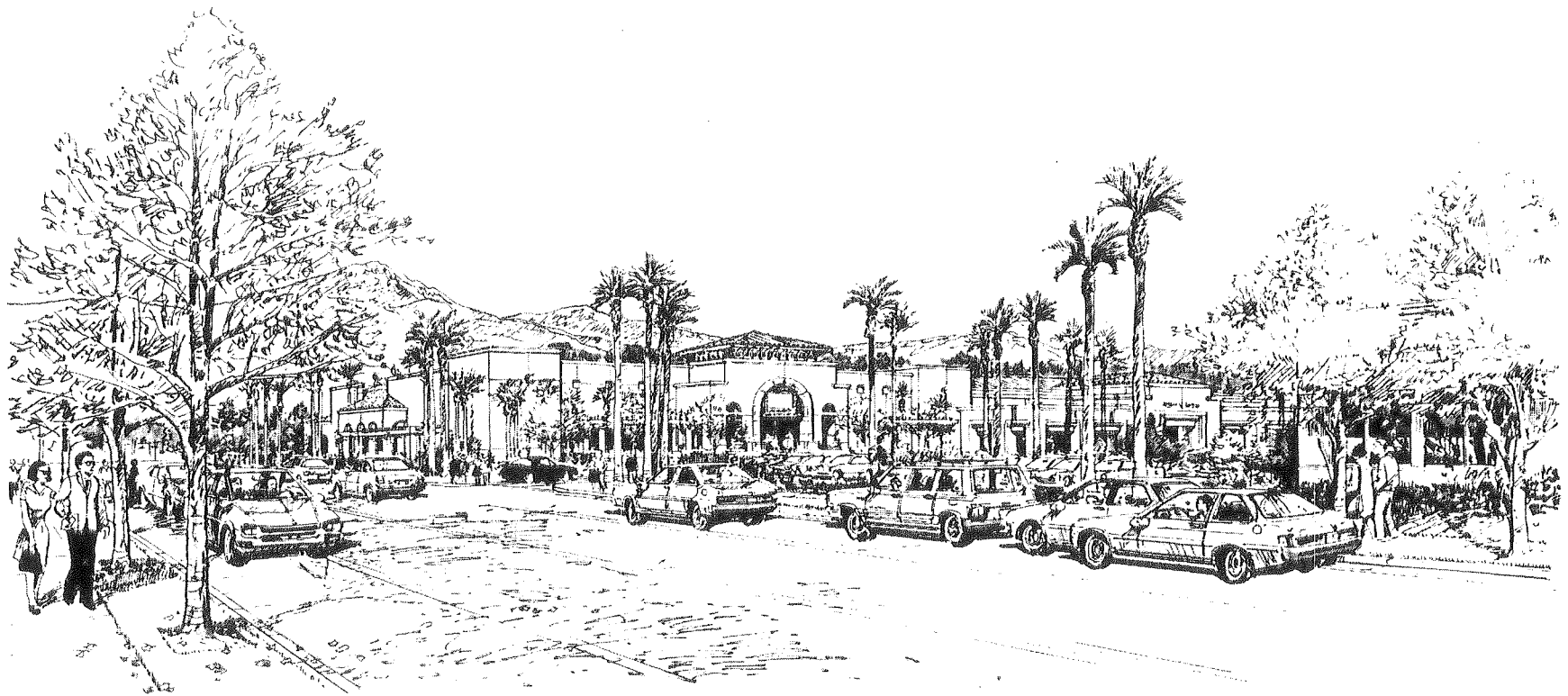
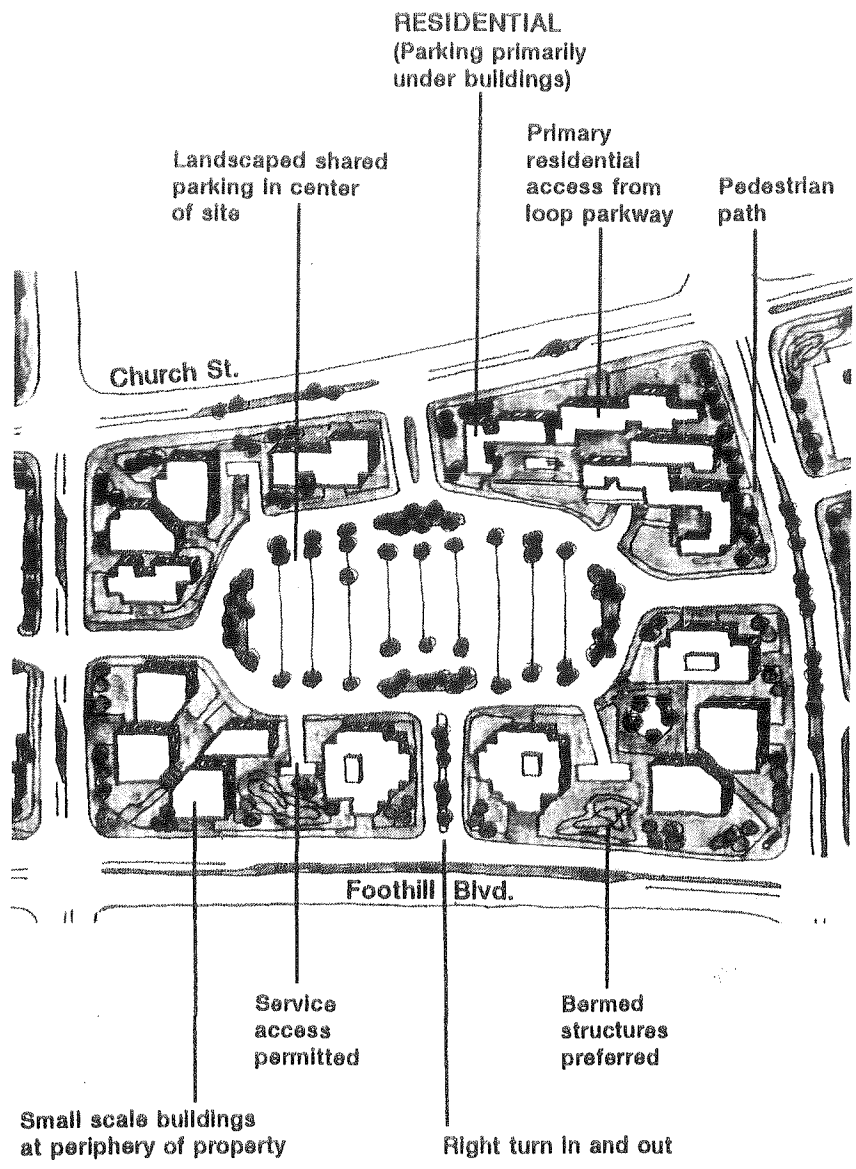


FIGURE IV-72
View of Commercial Park from Spruce Avenue



Financial/Restaurant Plaza. This mixed use parcel will include commercial, office, and residential uses. Figure IV-73 illustrates the type of development envisioned. A combination of restaurants, financial buildings, and other compatible uses would be clustered around most of the periphery, with a shared parking area in the center and residential development in one corner (Figure IV-74). Because the financial institutions and restaurants generate peak patronage at different hours and because combined one-stop eating/banking trips at lunchtime are likely, shared parking may allow a smaller overall parking area than would be required in a non-unified development. The remaining buildings, which would be landscaped and joined together with walkways, would cater to a variety of financial concerns, eating establishments (both daytime and nighttime), and small shops. The overall impression from within the site would be that of a small commercial village within which the financial buildings, depending on their design, could contribute a sense of urban variety.

The view from Foothill Boulevard of the Financial/Restaurant Plaza would convey the impression of buildings in a park. Some of the buildings are likely to be bermed so that they appear lower in silhouette (Figure IV-74). The spaces between the buildings will be landscaped so that the central parking area will not be highly visible from Foothill Boulevard.

FIGURE IV-73
Conceptual Site Plan for Financial/Restaurant Plaza

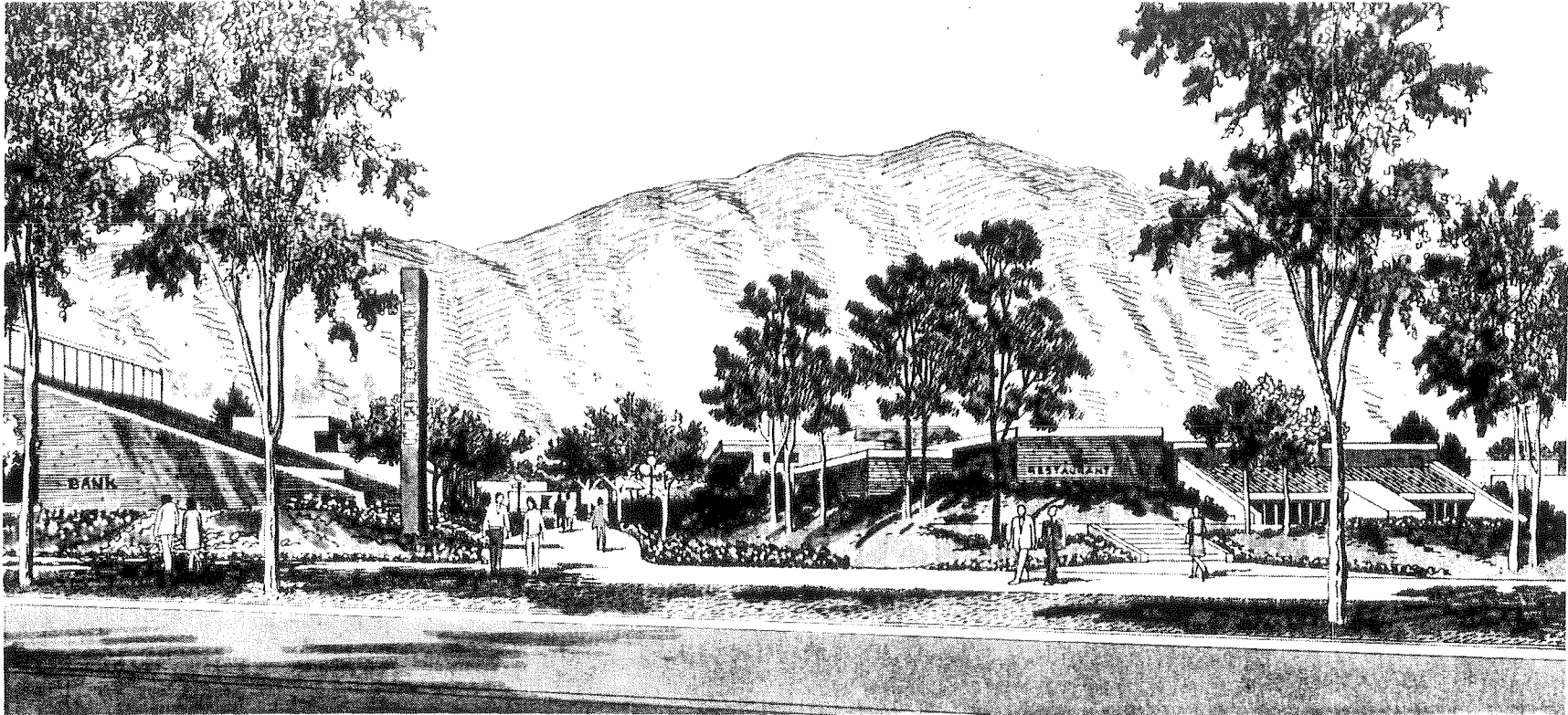


FIGURE IV-74
View from Foothill Boulevard of Financial/Restaurant Plaza

Medical Park. The Medical Park occupies the site at the northeast corner of Milliken Avenue and Foothill Boulevard. The site is devoted to a hospital and associated medical office buildings (Figure IV-80). As this site is one of the primary gateways to the Terra Vista community, the medical center will offer one of the key first impressions of the community for those arriving from points south. The hospital and medical office buildings, which would feature landscaped grounds, will create an impression of solidity and professionalism (Figures IV-75 and IV-76). They will also relate well to the residential development north, east and west of the center, as well as the public park across Church Street.

Primary access to this parcel will be from Church Street to the north and from Mayten Avenue to the east. Access from Milliken Avenue and Foothill Boulevard will be limited to right turn movements only.

The design concept for the Medical Park focuses on a central landscaped area providing pedestrian connections between all facilities and a peaceful, campus-type environment for patients.

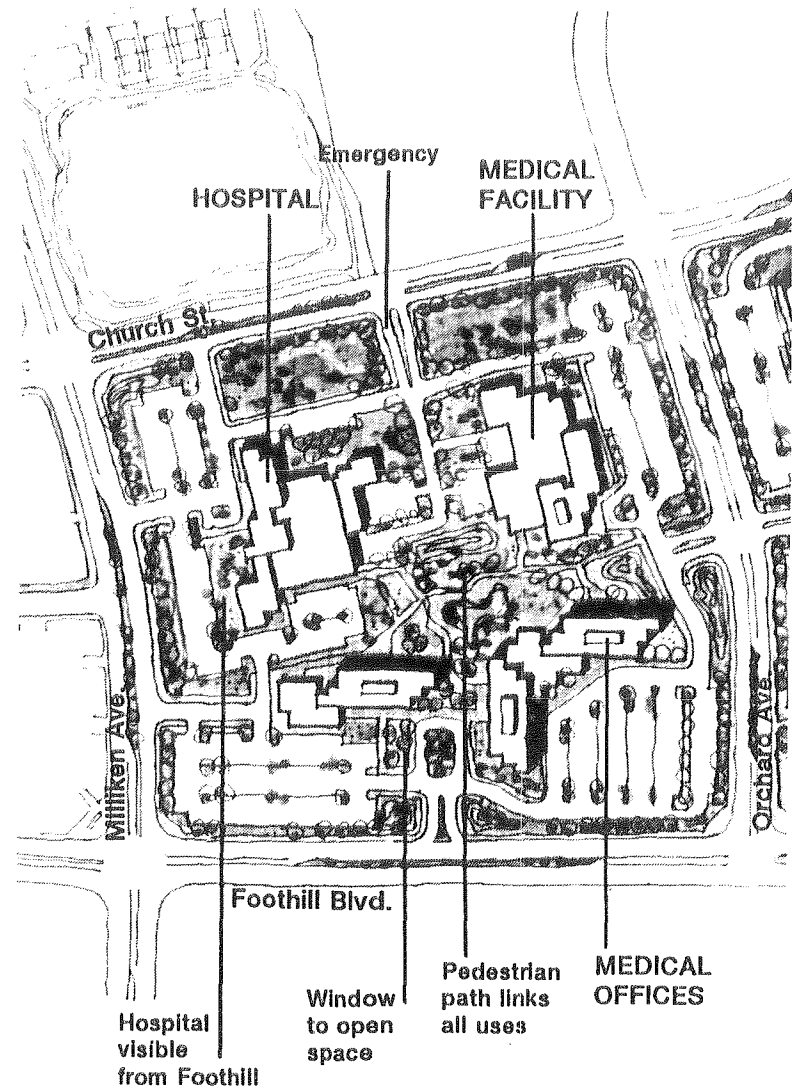


FIGURE IV-75
Conceptual Site Plan for Medical Park

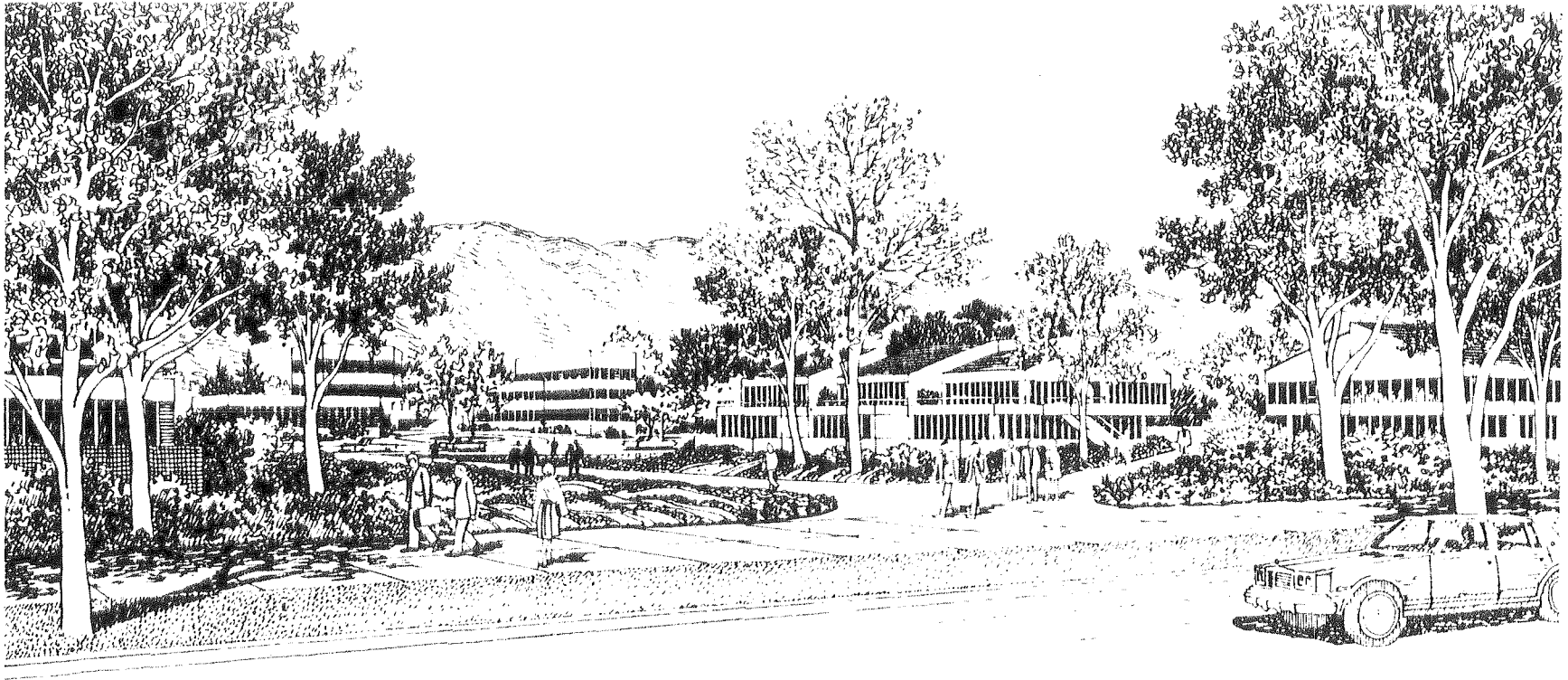


FIGURE IV-76
View of Medical Park from Foothill Boulevard

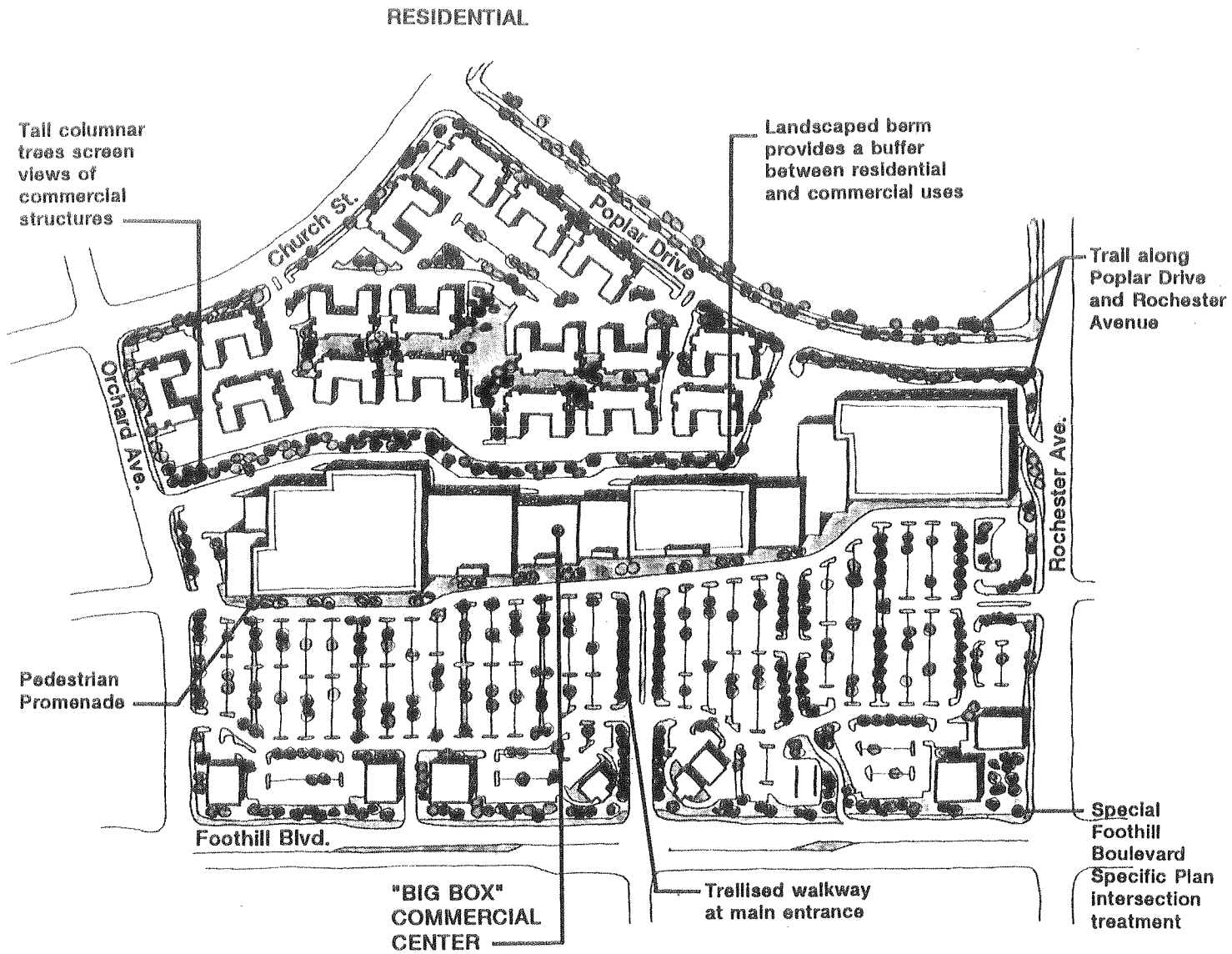


FIGURE IV-77
Conceptual Site Plan for the Promotional Center

Promotional Center. The Promotional Center, between Mayten and Rochester Avenues on Foothill Boulevard is envisioned as a mixture of commercial and residential uses. Primary access to the commercial complex will be from Foothill Boulevard, and Mayten and Rochester Avenues. Access to the residential area will be from Church Street, Mayten Avenue, and Poplar Drive.

The large "big box" commercial buildings will be set back from the Boulevard to reduce their scale and provide room for a spacious parking lot. Smaller retail stores and restaurants are located facing Foothill Boulevard and adjacent to the main entrance (Figure IV-77). The landscape treatment of the Foothill Boulevard frontage will both provide screening of parking areas and allow for filtered views into the site.

An architecturally interesting theme element will provide a visual terminus to the main entry road and establish a strong identity for the development. Pleasant pedestrian promenades with a meandering pathway and an informal paving pattern will lead through the site in an east/west orientation, providing a convenient connection to the various stores in the complex.

Two north/south promenade walkways will link the large buildings to the smaller retail and restaurant buildings along Foothill Boulevard. The promenade at the main entry will be prominent, while the promenade through the western parking area will be secondary. A continuous east/west promenade will connect all the large retail buildings. The pedestrian trail along Foothill Boulevard will provide direct access to each of the smaller retail and restaurant buildings.

The prominent promenades will be similar to those in the Terra Vista Town Center and offer expansive landscaped areas, walkways, furniture, trellises, and pedestrian plazas. The secondary promenade will feature less extensive landscaping and a walkway.

The facades of the buildings composing the commercial complex will be staggered to create variety in their massing and the entrances to each of the major buildings will be pushed outward to generate visual interest.

Between the articulated entrances of the "big box" uses, a series of vine-covered trellises, reminiscent of the area's early heritage, will provide shade and human scale elements along the promenade. Other features of the Promotional Center include decorative tile, lush landscaping and rustic-looking trellises located along the tree-lined main entry from Foothill Boulevard (Figures IV-78 and IV-79).

The loading area towards Poplar Street will be screened by a combination of high walls at the specific loading areas. The remainder will be screened by a combination of low walls, berms and landscaping. The goal is to hide parked trucks from the view of auto traffic.

Locating several "big box" uses on the same site allows for shoppers to complete several errands in one stop and the configuration of the project facilitates pedestrian movement between the various stores. Loading areas are located behind the large uses and are screened from adjacent residential uses by a dense row of tall trees and a change in grade. A covered pick up and loading area for major home improvement uses is permitted in the front of the building.

The trail system will serve the site by continuing on the north side of Poplar Drive and then traveling along Rochester on the east side of the site before connecting to Foothill Boulevard.

Higher density residential uses are clustered in the northwest portion of the site with views of the adjacent park and the mountains in the distance. Dwelling units are buffered from the commercial center by a landscaped slope or retaining wall combined with a dense row of tall columnar trees.

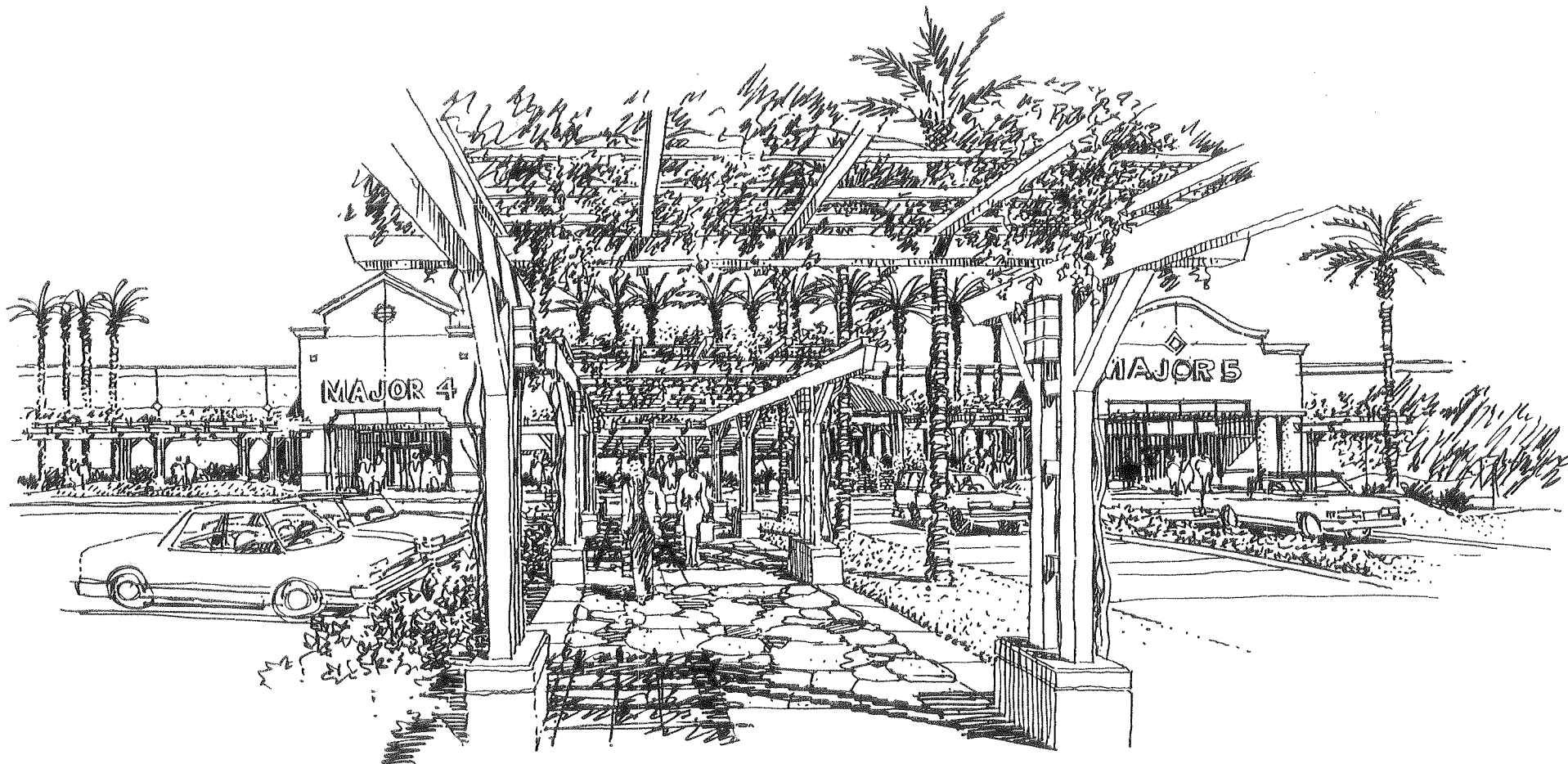


FIGURE IV - 78
View of the main entrance to the Promotional Center



FIGURE IV-79
View of the Pedestrian Promenade in
Front of the Promotional Center

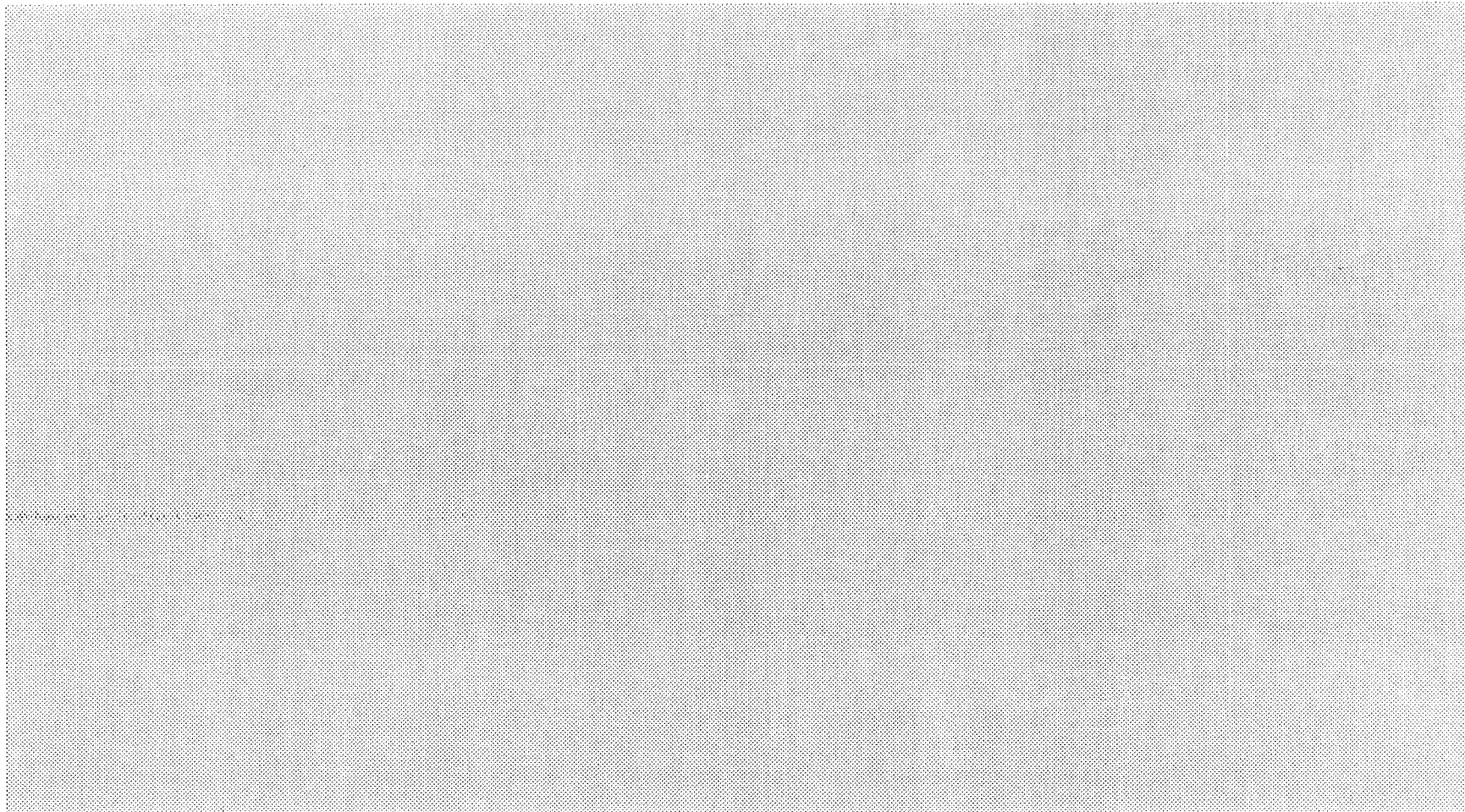


Figure IV-80
Figure IV-81
Deleted from Plan
No Longer Applicable

Revised Amendment No. 9
IV-68

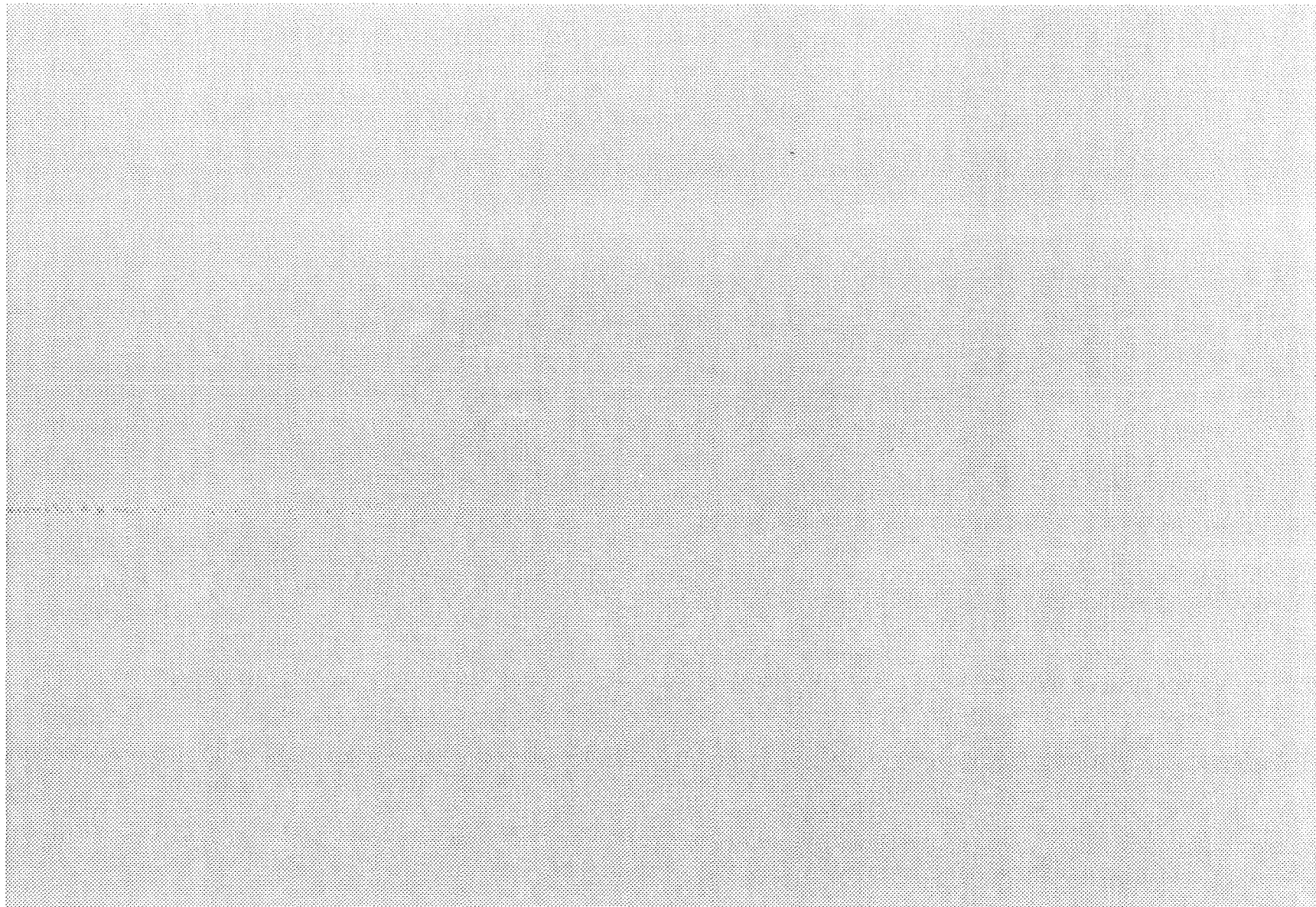


FIGURE IV-82
Deleted from the 1990 Revised Community Plan
No Longer Applicable

Energy and Resource Conservation

Intent and Objectives

The Terra Vista Community Plan takes a comprehensive view of natural resources. The plan is committed not merely to the conservation of energy; it is designed with the conviction that Terra Vista will thrive on the optimum use of all natural resources.

Natural resources are not only those convertible to energy. Resource conservation in Terra Vista is conceived as the optimum use of:

- Exhaustible energy sources, such as fossil fuels (oil and natural gas)
- Inexhaustible or renewable energy sources, such as sunlight and wind
- Water, both as a resource itself and as its transportation and use consumes other resources
- Man-made resources, such as building materials, which embody energy in their manufacture
- Land, as a limited resource, the efficient or inefficient use of which also has dramatic consequences for the conservation or waste of many other resources

Conservation of natural resources is a matter of public policy as well as a goal of this plan. It has been a major factor in the planning of Terra Vista from the beginning. However, resource conservation must be balanced against many other goals and constraints. Among these are:

- The cost/benefit ratio associated with energy conservation investments, which will vary over time reflecting user demand, market supply, regulatory intervention, and technical innovations
- The life-cycle savings in resources resulting from any given measure, weighed against the one-time cost of undertaking it
- The tremendous need for, and state policy in support of, housing that is affordable to average families
- The constraints of site planning and engineering and the design possibilities of a particular site
- The qualitative preferences and desires of those who will live with the results, both positive and negative
- The importance of variety in building styles, development patterns, and landscape treatments to give the community vitality

The following design guidelines for energy and resource conservation are the result of balancing these many goals and constraints. They are intended to serve the following objectives:

- Conserve natural gas used for space heating and water heating.
- Conserve electricity used for space cooling and appliances.

- Increase opportunities for use of inexhaustible energy sources, particularly solar energy.
- Decrease dependence on nonrenewable energy sources by taking advantage of natural ventilation, natural light, and natural heating and cooling.
- Conserve water used for both irrigation and domestic purposes.
- Make economical use of materials for buildings, roads, other paved areas, and utilities.
- Make efficient use of land.

The guidelines are organized as follows:

- Land use
- Site planning
- Landscaping
- Building design and equipment
- Other features

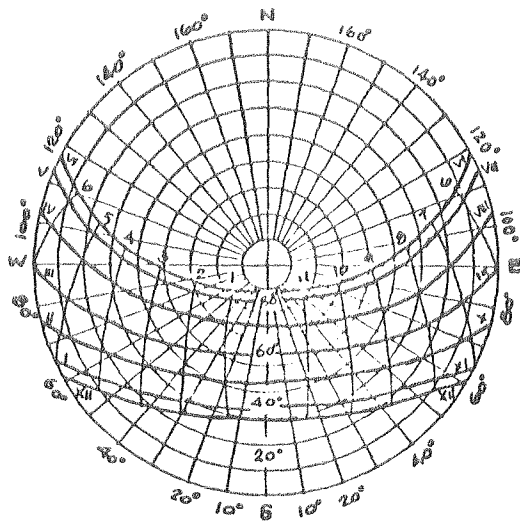
Land Use

Land is a costly and limited resource. Equally important, the efficient use of land is a critical requirement in order to reduce consumption of other resources. Inefficient land use patterns do not only waste land and drive up housing costs. When development is spread out over a wider area than it need be, and related uses are widely separated, there is a waste of paving materials for streets and parking areas (which also results in

higher heat gain and cooling needs); piping, wiring, and conduit for utility extensions; power necessary to supply water and lighting over longer distances; water to irrigate overly large planted areas; and gasoline for each occupant to drive farther to everyday destinations (which also results in poorer air quality).

For all these reasons, efficient and well-organized land use is the backbone of Terra Vista's resource conservation plan. Listed below are some of the major ways that this has been reflected in the Terra Vista land use plan. While the plan is certain to change and grow over time, these key elements should be preserved. It is also hoped that, as individual developments are built, these principles can be incorporated into their design in ways not foreseen at the Community Plan level.

- Adequate density is essential for efficient land use and site planning. Therefore, while individual parcels may ultimately vary in density, the Planned Community as a whole should not fall significantly below its planned base density.
- Joint use of land should be encouraged. The plan incorporates joint use of parks as school playfields; joint use of parks for occasional stormwater detention; shared parking areas in commercial developments and for community uses; and some joint use of landscaped areas for trails. Other means to get double service from land area should be explored as the community develops.
- Related uses should be combined or linked where feasible to allow one trip to serve multiple purposes. The Terra Vista plan accomplishes this on an overall planning level, as in



35° North Latitude
 Outer Circle = 0° (Horizon) Altitude in Degrees
 Center Point = 80°
 Elliptical Curves = Sun's Path on the
 21st of ea. Month
 (shown by Roman
 Numerals)
 Arabic Numerals = Time of Day

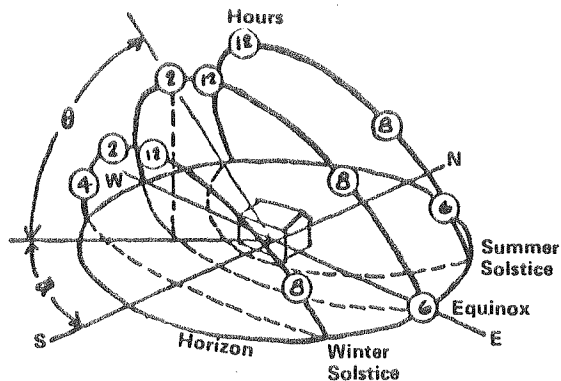


FIGURE IV-83
 Summer and Winter Sun Paths in Rancho Cucamonga

the trail and road connections that link the entire community, and the mixed uses foreseen for the Foothill Boulevard centers. Flexibility should be allowed for individual developments to carry the concept further.

- Alternatives to automobile travel need to be planned for and facilitated. The land use plan as a whole promotes future transit possibilities by concentrating potential riders around the loop parkway. While individual parcels may vary, this overall pattern of density concentration should be preserved. The trail network is also important as it provides the alternative of avoiding vehicular travel altogether for local trips.

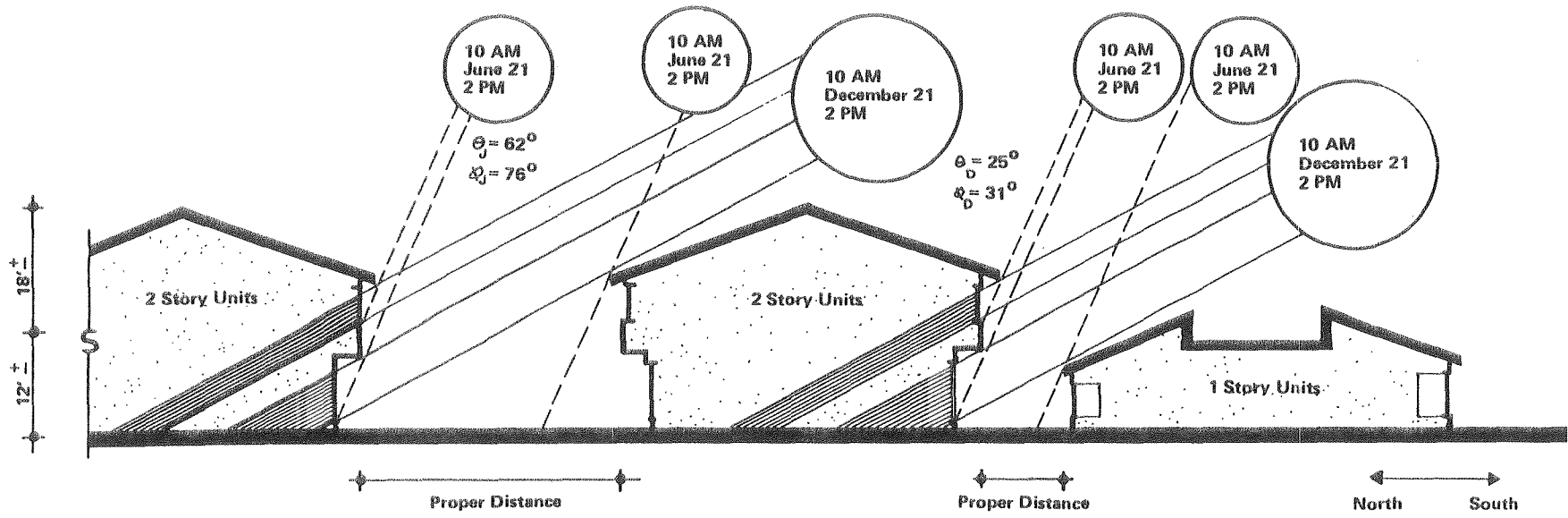
Site Planning

Passive Solar. Passive solar techniques, which involve little or no special equipment, offer the most year-round comfort at the lowest cost. Passive solar site planning consists of orienting buildings properly with respect to the sun. The path of the sun as perceived from Rancho Cucamonga is shown in Figure IV-83. Figure IV-84 indicates the angle of sunlight in Rancho Cucamonga at the summer solstice (when the sun is highest in the sky) and the winter solstice (when it is lowest), as it would affect typical homes.

The most commonly cited passive solar technique in site planning is to run streets east-west so that the long dimension of homes, containing most of the windows, will face south (see Figure IV-85). This allows natural sunlight from the south to help heat the home in the winter. Eave overhangs are necessary to block direct sunlight from heating the home in the summer, as shown on Figure IV-84.

θ_J = Altitude \angle June
 α_J = Azimuth \angle June

θ_D = Altitude \angle December
 α_D = Azimuth \angle December



The area shaded by a residential dwelling varies by season and time of day. The maximum shaded area at mid-day occurs on the winter solstice (December 21), when the sun is low in the sky. The shaded area is smallest on the summer solstice (June 21).

One-story buildings cast shorter shadows than taller ones and can therefore be closer to another building on the north without impairing the northerly building's solar access.

Eave overhangs allow the winter sun to penetrate but blocks direct sun rays in summer, when the sun is higher in the sky.

FIGURE IV-84
Solar Access and Shadow Patterns

Using the sun to help heat a home in the winter is quite valuable in the North and East, where extremely cold winters occur and heating costs are high. In Southern California, winters are short and mild; our primary concern during most of the year is to remain cool in the face of our hot, desert climate. Therefore, a simplistic rule that "windows should face south to save energy" is not necessarily appropriate in Southern California and can in fact result in higher energy use by increasing cooling needs during most of the year. The ideal window orientation to minimize cooling costs is to the north, not the south. It should also be kept in mind that window placement in buildings can vary, regardless of the orientation of the street and lot, depending on the architectural design.

In Terra Vista, the general slope of the land is from the north to south. To avoid excessive grading resulting in steep slope banks, and to provide positive drainage, it is preferable for local streets to run generally north-south. Therefore, in conventional single-family developments, it will often be difficult to orient windows to the north and south (although patio homes and zero-lot-line homes can retain a southern exposure, as shown in Figure IV-86). This is not necessarily energy-inefficient, as explained above. Also, variety is important; not all homes should be oriented in the same direction, even if it were physically possible to do so. In cluster and higher density developments, there is more flexibility to achieve north-south orientations.

Site planning in Terra Vista should attempt to maximize north-south window exposure, within the constraints imposed by engineering needs. To the extent it cannot be achieved, it will be compensated

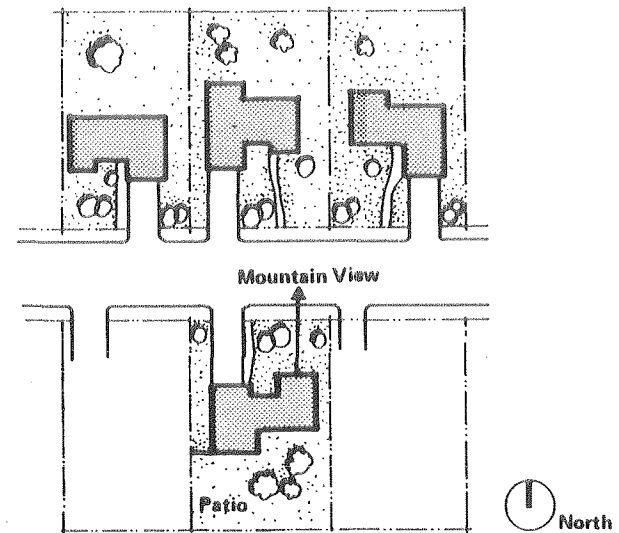


FIGURE IV-85
Conventional Development with Long Axis of Building Oriented East-West (Windows South)

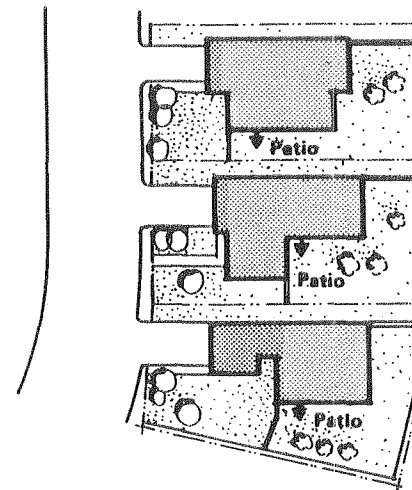


FIGURE IV-86
Patio Home Development on North-South Street

for (in energy terms) through architectural and landscaping techniques, as explained below.

Active Solar. Active solar involves the collection and conversion of solar energy for use in water heating, space heating, and other applications. Active solar technology is continually changing. At present, it is relatively expensive and financially beyond the reach of many home buyers; its cost effectiveness is expected to increase over time.

Active solar energy use will be facilitated in Terra Vista through the preservation of solar access. This means avoiding placing homes where they will fall within the shadow of other buildings or tall trees between the hours of 9:00 a.m. and 3:00 p.m. Most homes in Terra Vista should have ample roof area with full exposure to the sun, for the following reasons:

- Most development in Terra Vista will be low-rise and therefore cannot physically block the path of sunlight to the roofs of adjoining homes. The potential for midrise development has been confined to the commercial and mixed-use parcels fronting on Foothill Boulevard. In those locations, the setbacks and street widths already provided in the plan should be more than adequate to assure solar access to adjoining homes. Any potential partial encroachment within the mixed use parcels, while unlikely, will be evaluated at the time of development.
- The planting of tall trees is required only along the major arterials. Since most of these streets run north-south, the shadows of the trees will generally be confined to the roadway or the immediately adjoining area, where dwellings are

prohibited by the street setback requirements. The locations where these tree shadows could encroach onto adjoining roofs of homes are very few.

- Tree planting on other streets can be controlled to avoid shadow encroachment onto roofs, as explained below under landscaping guidelines for energy conservation.

The provision of active solar equipment in homes, while encouraged, is not required by this plan as it is best left to the workings of the market.

Development Patterns. Cluster development, integrated mixed-use development, and other nonconventional development patterns are encouraged. Such configurations optimize the use of land, streets, infrastructure, materials, water, and energy. In conventional single-family developments, resources can be conserved by the use of smaller lots, where appropriate, and narrower lot frontages, where consistent with the building type and design considerations.

Landscaping

- Local street trees planted by the developer near south-facing windows or roofs should either be deciduous (to provide summer shade without hampering winter solar access, as shown in Figure IV-87) or should be located so that the shadow pattern avoids potential solar collector locations, as shown in Figure IV-88.
- Shade trees near west-facing windows, preferably evergreen, are encouraged to reduce summer heat gain.

- Drought resistant plant materials should be used where appropriate, to reduce long-term water requirements for irrigation.
- Turf plantings, which have high irrigation needs, should be limited to areas necessary for lawn activities and special visual needs. Turf planting in the major greenway will be limited to selected nodes, as described earlier in this chapter.
- Mulch, water-holding ground covers, and efficient irrigation systems should be used to reduce runoff and evaporation.
- Pervious paving materials are encouraged to reduce surface runoff and aid in groundwater recharge.

Building Design and Equipment

Residential construction in Terra Vista will comply with the new state energy conservation standards provided in Title 20, Chapter 2, Subchapter 4 and Title 24, Part 2, Chapter 3-53 of the California Administrative Code. These extensive standards ensure that every dwelling will be energy-efficient. If a building's orientation to the sun is less than optimal, architectural features must be incorporated to reduce the building's energy requirements.

Energy conservation in a home can be accomplished in many different ways. The state standards provide flexibility so that no single measure must be used, but the combined effect of all features of the building design will reduce energy consumption over the life of the building. The following are some of the architectural and furnishing elements that are likely to be used in Terra Vista.

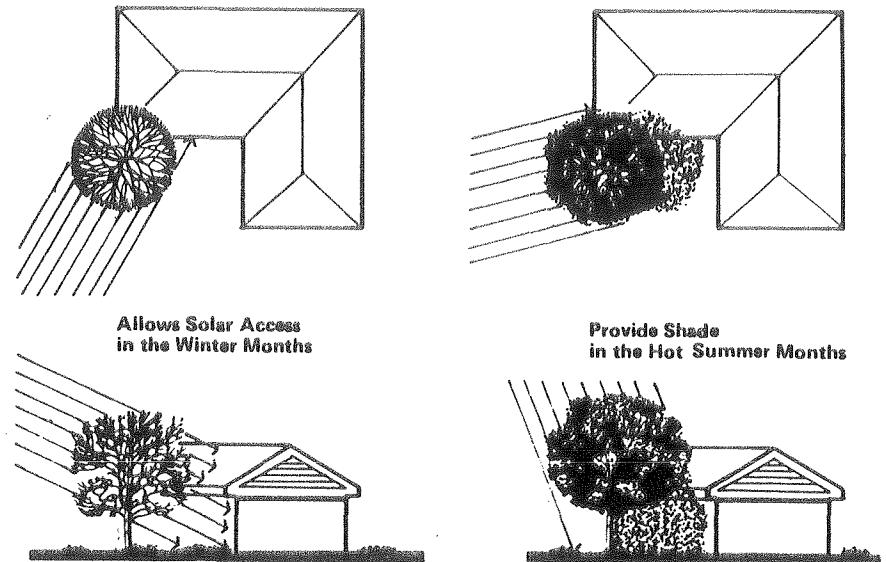


FIGURE IV-87
Deciduous Trees at South Elevations

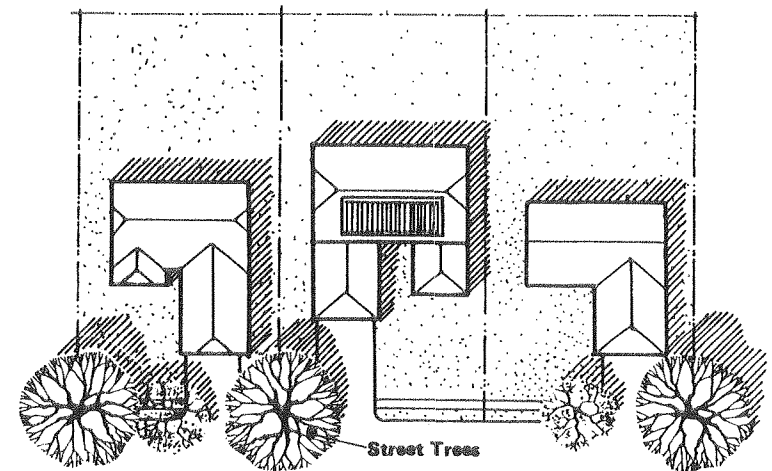


FIGURE IV-88
Trees Clustered to Protect Solar Access

- Predominant window placement should be north, south, and east. West-facing windows should be avoided or minimized. Where west-facing windows occur, they should be shaded by trees or screening devices.
- South-facing windows will be protected by appropriate eave overhangs to block summer sun rays.
- Clerestory windows, when used, should not occur on east or west-facing elevations.
- Energy-efficient appliances and heating/cooling equipment will be used. Oversized equipment will be avoided.
- Individual utility meters will be used in most cases, to encourage conservation.
- Low-flow shower heads and faucets, and low-flush toilets, will be used.
- Hot water lines in recirculating systems will be insulated.
- Total window area will be minimized, to the extent consistent with proper ventilation, use of natural light, views, and design preferences.
- Heat gain and loss can be reduced through smaller building size, shorter building perimeters, and use of party walls.
- Insulation and window glazing should be increased as necessary to eliminate undesirable heat gain and loss.

Other Features

- Public area lighting should be of a highly energy-efficient type, such as high pressure sodium. Lighting should be located, mounted, and directed so as to adequately light intended areas while not illuminating unnecessary spaces or intruding onto adjoining private property.
- The parking standards to which this plan is currently subject are not consistent with the goals of reducing use of building materials, avoiding waste of land, and avoiding unnecessary heat gain and surface runoff. It is intended that these standards will be modified in the near future.

V Community Development Standards

V Community Development Standards

The following development standards are intended to help implement the planning and design concepts for Terra Vista and to address certain aspects of development not covered elsewhere in the plan. These standards are regulatory in nature and will govern all development within Terra Vista. In most cases the standards set forth here establish minimum criteria; actual development may be expected in many cases to exceed these criteria.

The development standards should be used in combination with the design guidelines, which, although they are flexible guidelines rather than strict regulations, are also intended to shape development within the Terra Vista Planned Community. In the case of street setback standards, the reader may refer to Chapter IV for graphic depictions of the standards given here.

General Development Standards

- Whenever the regulations in this Community Plan differ from the regulations of the City of Rancho Cucamonga Zoning Ordinance, the regulations contained herein supersede those of the Zoning Ordinance. Where an issue is not addressed by the regulations in this Community Plan, the regulations of the Zoning Ordinance shall govern.
- Grading will be permitted within the Planned Community outside an area of immediate develop-

ment upon the securing of a grading permit, subject to applicable erosion control regulations.

- Site development and construction activities shall be limited to the period between 7:00 a.m. and dusk, Monday through Saturday. No activities will be permitted outside of these hours, including maintenance work that might be required on any equipment used in grading and construction, unless a temporary waiver is granted by the Building Official. No such waiver will be granted where such work is to be conducted adjacent to existing, occupied dwellings except in cases of emergency as determined by the Building Official.
- All building construction within the Planned Community shall comply with applicable building codes.
- Agricultural uses shall be permitted throughout the Planned Community as interim uses, subject to the Rancho Cucamonga Zoning Ordinance.
- Because full development of the Planned Community will take many years, interim and temporary uses other than agriculture shall be permitted, subject to approval of a Conditional Use Permit, provided that such uses and their locations will not be detrimental to any part of the Planned Community. Such interim uses may include country markets, swap meets, outdoor commercial recreation, mini-storage, parking areas, park-and-ride lots, and other uses similar in character to those listed.
- Model homes, their garages, and their private recreation facilities may be used as offices for the sale of homes within the subdivision in which

they are located, and in subsequent subdivisions in which the same or generally similar architectural designs are utilized. Such uses shall not exceed a duration of three years, except that one or more one-year extensions may be granted by the City Planner.

- Information and sales centers for home buyers may be established, either within a tract under development or at a separate location within the Planned Community. The nature of such facilities could range from a single information center to a complete information/sales/design center which might incorporate some model homes as well. Design and location of such facilities, and their term of use, shall be determined by approval of a Conditional Use Permit.
- A copy of the adopted Land Use Plan for the Planned Community will be displayed in all residential sales offices and will be provided to all prospective home buyers upon request.
- At the time of site plan review, the developer shall submit plans demonstrating conformance with the Rancho Cucamonga noise ordinance. The plans are subject to the approval of the City Planner.
- Dedication and improvement of all rights of way shall meet with the approval of the City Engineer.
- Any land use proposal not specifically covered by this Community Plan shall be subject to the regulations of the City of Rancho Cucamonga General Plan.

Residential Development Standards

General to All Residential Areas

- The following accessory uses shall be permitted in all residential areas:
 - (a) Garages, carports, and uncovered parking areas.
 - (b) Fences, walls, and trellises.
 - (c) Swimming pools and other recreational amenities.
 - (d) Guest houses, home occupations (subject to Ordinance 72), and social care facilities for six or fewer persons.
 - (e) "Granny" second units, as defined in and subject to SB 1534 (1982), or, if adopted, any City ordinance pursuant thereto.
 - (f) Solar collectors and related structures, subject to reasonable restraints with respect to appearance.
 - (g) Accessory buildings, structures, and uses where related and incidental to a permitted use.
- Fences and walls in, or adjoining, residential developments shall be permitted at property lines, except in the following conditions where setbacks or property line adjustments are required:

- (a) The location and height of fences and walls near front, side, or rear property lines on local streets will generally be governed by CC&R's. In some types of development, CC&R's may also restrict fencing in other locations. Fencing along local streets will also be subject to the provisions of easements for street trees, street maintenance, and/or utilities. Typically, street tree easements will affect the front 5 feet of each lot; other easements, where present, may be narrower.
- (b) Along trails that are less than 15 feet in width (typically 6 feet), privately owned landscaped areas are intended to also serve as landscaping for the trail system. Fences and walls shall therefore be set back from such trails as specified below, and a 15 feet minimum width measured across the trail shall be kept clear of walls or buildings.
- (c) Along major arterials, solid walls shall be limited where possible through the use of side-on cul-de-sacs, berming, landscaping, and building setbacks and orientation. Where walls are necessary for noise, privacy, or other reasons, as discussed elsewhere in this plan, they shall observe the setbacks given below.
- (d) Along major arterials, solid walls adjoining cluster/multifamily development shall be set back 28 feet minimum from the ultimate curb face. Within 15 feet of the edge of the right of way of an intersecting major arterial, secondary arterial, or collector road, solid walls shall be set back 38 feet minimum from the ultimate curb face. (Planters and associated walls for gateway treatments are excepted.)

- (e) Along major arterials, solid walls adjoining conventional single family developments shall be set back 20 feet average and 18 feet minimum from the ultimate curb face. Within 15 feet of the edge of the right of way of an intersecting major arterial, secondary arterial, or collector road, solid walls shall be set back 38 feet minimum from the ultimate curb face. (Planters and associated walls for gateway treatment are excepted.)
- Fences and walls constructed as acoustical barriers shall be of a sufficient height to mitigate noise, subject to approval of the City Planner. All other fences and walls shall be limited to 6 feet in height unless they are attached to a main building and are an architectural design element, in which case they may exceed 6 feet subject to design approval. Where grade differences or other special conditions exist, the stated height may be exceeded with the approval of the City Planner. Fences for tennis courts and similar uses are excepted from the above restrictions; their heights are subject to Development Review.
- Minimum building setbacks may be reduced for solar purposes subject to the Development Review Process.

Low Medium Density Residential

Land designated as Low Medium Density Residential is intended for residential development that ranges from 4 to 8 dwellings per gross acre. The following regulations apply:

- Uses permitted:
 - (a) Single family detached dwellings including, but not limited to, patio homes, zero lot line homes, and mobile homes.
 - (b) Single family attached dwellings including, but not limited to, duplexes, triplexes, fourplexes, and townhomes.

- (c) Cluster housing.
- (d) Community facilities as specified below.
- (e) Accessory uses as specified above.

- Site development standards for center plot development:

- (a) Building site area: 5,000 sq. ft. minimum, approximately 5,500 sq. ft. average.
- (b) Front, side, and rear building setback: As indicated in Table V-1 and Figures V-1 through V-4.
- (c) Garage and carport setbacks: As indicated in Table V-2 and Figure V-5.
- (d) Other accessory building setbacks: From major arterial highways, secondary arterials, and collector roads, setbacks are the same as those for main buildings. From local streets, adjoining residential lots and other conditions, setbacks are the same as those for main buildings except that an accessory building may encroach into any required side or rear setback to a distance of 3 feet minimum from the property line, provided the accessory building does not occupy more than 50 percent of the required rear yard.
- (e) Building site width: 50 feet minimum, provided that the building site width on cul-de-sacs and knuckles shall be 20 feet minimum.
- (f) Building site coverage: 60% maximum.
- (g) Building height: 35 feet maximum.

- Site development standards for cluster development:
 - (a) Building site area: 1 acre minimum.
 - (b) Front, side, and rear setbacks: As indicated in Table V-3.
 - (c) Setbacks for uncovered parking are as follows: 30 feet average, 19 feet minimum from face of curb of major arterial highway; parking areas less than 30 feet from curb must be screened by berms, landscaping, and/or structural elements 30 inches minimum in height. 17 feet minimum from face of curb of secondary arterial or collector road. 10 feet minimum from face of curb of local public street, subject to easements. From other conditions, no minimum.
 - (d) Building site width and depth: As permitted by required setbacks.
 - (e) Building site coverage: 60% maximum.
 - (f) Building height: 35 feet maximum.
 - (g) Private open space: 300 sq. ft. minimum.

The above site development standards apply to the building site, which is the cluster development as a whole or a designated portion of it. They govern the relationship of the cluster development to adjoining property and to adjoining streets and open spaces. Within the cluster development, the relationship of buildings to one another, to property lines, and to other features, such as open spaces and private streets, is governed by the provisions of the Uniform Building Code. The above trail setbacks, however, are applicable within the development if a master-planned trail occurs interior to the site.

- Site development standards for innovative development:
 - (a) Building site area: 3,500 sq. ft. minimum, 4,000 sq. ft. average.
 - (b) Front, side, and rear setbacks: As indicated in Table V-3.
 - (c) Building site width and depth: As permitted by required setbacks.

- (d) Building site coverage: As permitted by required setbacks and private open space.
- (e) Building height: 35 feet maximum.
- (f) Private open space: 700 sq. ft. minimum.

The above development standards apply to projects which are deemed innovative. Innovation in single family development means providing creative design solutions which address the critical concerns of neighborhood compatibility, density transition, and design quality. Innovative projects are characterized by an attractive streetscape which is not monotonous, nor is the street scene dominated by asphalt/concrete, garages, and cars. Innovative design means finding creative ways to create well-designed space, particularly usable yard space. A discussion of innovation and variety is contained in the Residential Design Guidelines section.

Medium Density Residential

Land designated as medium Density Residential is intended for residential development that ranges from 8 to 14 dwellings per gross acre. The following regulations apply.

- Uses permitted:
 - (a) Single family detached dwellings including, but not limited to, patio homes, zero lot line homes, and mobile homes.
 - (b) Single family attached dwellings including, but not limited to, duplexes, triplexes, fourplexes and townhomes.
 - (c) Cluster housing including, but not limited to, multifamily dwellings.
 - (d) Community facilities as specified below.
 - (e) Accessory uses as specified above.

(Text continued on page V-14)

TABLE V-1
BUILDING SETBACKS FOR CONVENTIONAL DEVELOPMENT (LM AND M USES)

Adjoining Condition	Front Setback	Side Setback	Rear Setback
<u>From Curb Face:</u>			
Local Public Street			
-All except mobile homes	20' min., 25' avg.	10'	15'
-Mobile homes ¹	6'	10'	10'
Collector Road	20' min., 25' avg.	10'	15'
Secondary Arterial	22'	17'	22'
Special Secondary Arterial	25'	20'	25'
Major Arterial			
-Lower than 2 story	33' min., 38' avg.	33' min., 38' avg.	33' min., 38' avg.
-2 story or higher	38' min., 43' avg.	38' min., 43' avg.	38' min., 43' avg.
-Main intersections ²	38' min.	38' min.	38' min.
Local Private Street ¹	5'	5'	5'
<u>From Property Line:</u>			
Alley	Not applicable	0'	0'
Trail Type C ³	0' with 25' building separation	0' with 25' building separation	10' with 35' building separation
Trail Type D ³	5' with 25' building separation	3' with 25' building separation	5' with 25' building separation
Open Space ^{3,4}	0'	0'	0'

TABLE V-1 (Cont'd.)
 BUILDING SETBACKS FOR CONVENTIONAL DEVELOPMENT (LM AND M USES)

Adjoining Condition	Front Setback	Side Setback	Rear Setback
Adjoining Residential -All except mobile homes	10'	5' one side Total per lot 15'	15'
-Mobile homes	10'	0' either side, 6' combined	5'
Other conditions	10'	10'	10'

- ¹ Easements along local street frontages for street trees, street maintenance, and/or utilities may require slightly greater setbacks so that eaves, etc. do not encroach into the easement area. See Figure IV-15 for various typical conditions on local public streets. Similar easements may occur along private street frontages.
- ² Within 15 feet of edge of right-of-way of intersecting major or secondary arterial or collector road.
- ³ Openings from structures onto open space are permitted subject to the Uniform Building Code.
- ⁴ Includes greenway, parks, and other public or private open space.

**TABLE V-3
BUILDING SETBACKS FOR STANDARD, PLANNED AND INNOVATIVE SINGLE FAMILY DEVELOPMENT
(LM, M AND MH USES)**

Adjoining Condition	Front Setback	Side Setback	Rear Setback
<u>From Curb Face:</u>			
Local Public Street			
-All except mobile homes	22'	22'	27'
-Mobile homes ¹	22'	22'	27'
Collector Road			
-With sidewalk or Type E Trail	22'	22'	27'
Secondary or Special			
Secondary Arterial	25' min, 32' avg	22' min, 32' avg	22' min, 32' avg
Major Arterial			
-Lower than 2 story	33' min, 38' avg	33' min, 38' avg	33' min, 38' avg
-2 story or higher	38' min, 43' avg	38' min, 43' avg	38' min, 43' avg
-Main intersections ²	38' min	38' min	38' min
Local Private Streets ¹	5'	5'	5'
<u>From Property Line:</u>			
Alley	Not applicable	0'	0'
Trail Type C ³	0 with 25' building separation	0 with 25' building separation	10' with 35' building separation
Trail Type D ³	6' with 25' building separation	6' with 25' building separation	6' with 25' building separation
Open Space ^{3,4}	0'	0'	0'

TABLE V-3 (Continued)
BUILDING SETBACKS FOR STANDARD, PLANNED AND INNOVATIVE SINGLE FAMILY DEVELOPMENT
(LM, M AND MH USES)

Adjoining Condition	Front Setback	Side Setback	Rear Setback
Adjoining Residential -All except mobile homes	10'	0' either side 10' combined	10'
-Mobile homes	10'	0' either side, 6' combined	5'
Other conditions	10'	0' either side, 10' combined	10'

- ¹ Easements along local street frontages for street trees, street maintenance, and/or utilities may require slightly greater setbacks so that eaves, etc. do not encroach into the easement area. See Figure IV-15 for various typical conditions on local public streets. Similar easements may occur along private street frontages.
- ² Within 15 feet of edge of right-of-way of intersecting major or secondary arterial or collector road.
- ³ Openings from structures onto open space are permitted subject to the Uniform Building Code.
- ⁴ Includes greenway, parks, and other public or private open space.

Medium Density Residential

(Continued from page V-5)

- Site development standards for innovative single family development:
 - (a) Building site area: 3,500 sq. ft. minimum, 4,000 sq. ft. average.
 - (b) Front, side, and rear building setbacks: As indicated in Table V-3 and Figures V-1 through V-4.
 - (c) Garage and carport setbacks: As indicated Table V-2 and Figure V-5.
 - (d) Other accessory building setbacks: From major arterial highways, secondary arterials, and collector roads, setbacks are the same as those for main buildings. From local streets, adjoining residential lots and other conditions, setbacks are the same as those for main buildings except that an accessory building may encroach into any required side or rear setback to a distance of 3 feet minimum from the property line, provided the accessory building does not occupy more than 50 percent of the required rear yard.
 - (e) Building site width and depth: As permitted by required setbacks, provided that the building site width on cul-de-sacs and knuckles shall be 20 feet minimum.
 - (f) Building site coverage: As permitted by required setbacks and private open space.
 - (g) Building height: 35 feet maximum.

- (h) Private open space: 300 sq. ft. minimum.

The above site development standards apply to projects which are deemed innovative. Innovation in single family development means providing creative design solutions which address the critical concerns of neighborhood compatibility, density transition, and design quality.

Innovative projects are characterized by an attractive streetscape which is not monotonous, nor is the street scene dominated by asphalt/concrete, garages, and cars. Innovative design means finding creative ways to create well-designed space, particularly usable yard space. A discussion of innovation and variety is contained in the Residential Design Guidelines section.

- Site development standards for cluster development:
 - (a) Building site area: 1 acre minimum.
 - (b) Building setbacks: As indicated in Table V-4.
 - (c) Setbacks for uncovered parking: As indicated in Table V-4.
 - (d) Building site width and depth: As permitted by required setbacks.
 - (e) Building site coverage: As permitted by required setbacks and private open space.
 - (f) Building height: 35 feet maximum.
 - (g) Building separations: The standards from the Rancho Cucamonga Development Code shall apply.

The above site development standards apply to the building site, which is the cluster development as a whole or a designated portion of it. They govern the relationship of the cluster development to adjoining property and to adjoining streets and open spaces. Within the cluster development, the relationship of buildings to one another, to property lines, and to other features, such as open spaces and private streets, is governed by the provisions of the Uniform Building Code. The above trail setbacks, however, are applicable within the development if a master-planned trail occurs interior to the site.

Medium High Density Residential

Land designated as Medium High Density Residential is intended for residential development that ranges from 14 to 24 dwellings per gross acre. The following regulations apply.

- Uses permitted:
 - (a) Multifamily dwellings, including, but not limited to, apartment projects, condominium projects, and cooperative apartment projects.
 - (b) Single family attached dwellings.
 - (c) Community facilities as specified below.
 - (d) Accessory uses as specified above.
- Site development standards:
 - (a) Building site area: 1 acre minimum.

- (b) Building setbacks: As indicated in Table V-4.
- (c) Garage, carport, and accessory building setbacks: As indicated in Table V-4.
- (d) Setbacks for uncovered parking: As indicated in Table V-4.
- (e) Building site width and depth: As permitted by required setbacks.
- (f) Building site coverage: No maximum subject to Development Review Process.
- (g) Building height: 45 feet maximum.
- (h) Building separations: The standards from the Rancho Cucamonga Development Code shall apply.

The above site development standards apply to the building site, which is the development as a whole or a designated portion of it. They govern the relationship of the development to adjoining property and to adjoining streets and open spaces. Within the development, the above standards govern building separation and setbacks from master-planned trails. Otherwise, within the development, the relationship of buildings to one another, to property lines, and to other features, such as open spaces and private streets, is governed by the provisions of the Uniform Building Code.

(High Density Residential development standards discussed on page V-18).

TABLE V-4
BUILDING SETBACKS FOR MULTIFAMILY DEVELOPMENT (M, MH AND H USES)

Adjoining Condition	Building Setback	Detached Garage/Carport ⁸ and Accessory Building Setback	Uncovered ³ Parking Setback
<u>From Curb Face:⁸</u>			
Major Arterial			
-Lower than 2 story	38' avg, 33' min ¹	38' avg, 28' min ^{1,2}	38' avg, 19' min ³
-2 story or higher	43' avg, 38' min	38' avg, 28' min ^{1,2}	38' avg, 19' min ³
Secondary or Special			
Secondary Arterial			
-Lower than 3 story	32' avg, 27' min	32' avg, 25' min	32' avg, 19' min ³
-3 story or higher	37' avg, 32' min		Not applicable
Collector Road			
-Lower than 3 story	27' avg, 22' min	27' avg, 22' min	27' avg, 19' min ³
-3 story or higher	32' avg, 27' min		
Local Public Street			
-Lower than 3 story	22' avg, 20' min	22' avg, 17' min	22' avg, 11' min ³
-3 story or higher	27' avg, 29' min		
<u>From Property Line:</u>			
Private Street or Alley ⁴	0'	0'	0'
Trail Type C ⁵			
-Lower than 3 story	0' with 25' separation	0' with 25' separation	0'
-3 story or higher	0' with 35' separation	0' with 35' separation	0'
Trail Type D ⁵			
-Lower than 3 story	6' with 25' separation	6' with 25' separation	0'
-3 story or higher	6' with 35' separation	6' with 35' separation	0'
Open Space ^{5,6}	0'	0'	0'
Other Conditions ⁹	10'	0' ⁷	0'

Notes to Table V-4

- ¹ Minimum setback is 38 feet within 15 feet of the edge of the right-of-way of an intersecting major or secondary arterial or collector road.
- ² The building setback may be 28 feet along no more than 35% of the major arterial frontage, provided the design of the building wall is compatible with the design of adjoining freestanding walls.
- ³ Parking areas less than 30 feet from curb face must be screened by berms, landscaping, and/or structural elements 30 inches minimum in height.
- ⁴ With 26 feet of clear area directly adjoining for back-up purposes. Setbacks are subject to easements, if any.
- ⁵ Openings from structures onto open space are permitted subject to the Uniform Building Code.
- ⁶ Includes greenway, parks, and other public or private open space.
- ⁷ An attached or detached garage, carport, or accessory building may abut a property line, or another structure on adjoining property, provided no openings are permitted on the surface abutting another residential lot, subject to the provisions of the Uniform Building Code.
- ⁸ Building setbacks shall be measured from the exterior of the wall including stairwells, patios and other architectural elements or features except for those listed in Section 17.08.060 C1 and C2 of the Rancho Cucamonga Development Code. Landing places which do not exceed a height of 36 inches above grade may project into the front and corner side yards a maximum distance of 6 feet.
- ⁹ Add 10 feet if adjacent to VL or L District.

High Density Residential

Land designated as High Density Residential is intended for residential development at densities from 24 to 30 dwellings per gross acre. The following regulations apply:

- Uses permitted:
 - (a) Multifamily dwellings, including, but not limited to, apartment projects, condominium projects, and cooperative apartment projects.
 - (b) Community facilities as specified below.
 - (c) Accessory uses as specified above.
- Site development standards:
 - (a) Building site area: 2 acres.
 - (b) Building setbacks: As indicated in Table V-4.
 - (c) Garage, carport, and accessory building setbacks: As indicated in Table V-4.
 - (d) Setbacks for uncovered parking: As indicated in Table V-4.
 - (e) Building site width and depth: As permitted by required setbacks.
 - (f) Building site coverage: No maximum subject to Development Review Process.

- (g) Building height: 65 feet maximum.
- (h) Building separations: The standards from the Rancho Cucamonga Development Code shall apply.

The above site development standards apply to the building site, which is the development as a whole or a designated portion of it. They govern the relationship of the development to adjoining property and to adjoining streets and open spaces. Within the development, the above standards govern building separation and setbacks from master-planned trails. Otherwise, within the development, the relationship of buildings to one another, to property lines, and to other features, such as open spaces and private streets, is governed by the provisions of the Uniform Building Code.

(Note: In mixed uses where residential and commercial uses are combined in a single building, the site development standards for office and commercial uses apply, subject to Development Review.)

Community Facilities

The following community support uses shall be permitted throughout the Planned Community subject to the Development Review Process. Site development standards shall be the same as those for office and commercial uses (see below). An asterisk indicates that a Conditional Use Permit is required.

- General open space uses.
- Public schools, private schools*, churches*, libraries*, civic or community clubs*, and museums*.
- Establishments for care of children* and other social care facilities*.
- Fire stations* and other public or quasi-public facilities*.
- Utility facilities such as communication equipment buildings*, electrical distribution substations*, public utility booster stations*, water supply reservoirs*, and areas for drainage retention and/or groundwater recharge (subject to the approval of the City Engineer); alternative energy generation equipment such as windmills* and major solar collection facilities*; communication facilities such as communitywide transmission receivers*; public utility offices*.
- Public or private transit-related facilities, park-and-ride lots*, transfer stations, and bus waiting areas.

* Conditional Use Permit required.

- Accessory buildings, structures, and uses related and incidental to the above.
- Mini-warehouses* and Recreational Vehicles/Boat Storage*

Mini-warehouse shall be defined as a structure, or group of structures, providing enclosed and locked compartments for the dead storage of customer goods and wares where individual lockers or stalls are rented out to different tenants for storage.

Recreational vehicle/boat storage is an area for the parking of vehicles designed for recreational use (as in camping) and for the parking, not repair, of recreational boats.

- Automobile service stations*, along any of Terra Vista's major arterials (Baseline, Rochester, Foothill and Milliken), provided that they are separated from residential uses by a street, additional landscape setback, or other "buffer" of non-residential use satisfactory to the Planning Commission.

Service stations permitted in Terra Vista shall include "full-service", offering lubrication and oil change, tire, battery and accessory sales, automated car washing etc.; and convenience retail (mini-marts), as determined during the CUP process to be appropriate for the particular location under review.

Office and Commercial Development Standards

GENERAL TO ALL OFFICE AND COMMERCIAL AREAS:

Uses Not Permitted Within The Planned Community

Massage parlors
"Adult" entertainment establishment

Shopping Centers

To ensure that the goals and policies of the General Plan and Community Plan are implemented, a Conditional Use Permit shall be required for shopping centers. In such a review, the following criteria shall be considered:

- (a) The transition from more sensitive land uses and buffering methods to mitigate commercial activities such as loading, lighting, and trash collection;
- (b) The center has been planned as a group of organized uses and structures;
- (c) The center is designed with one theme, with buildings and landscaping consistent with design (similar architectural style, similar exterior building materials, and a coordinated landscaping theme);
- (d) The center makes provisions for consistent maintenance, reciprocal access and reciprocal parking;
- (e) Vehicle and pedestrian access is coordinated and logically linked to provide a comprehensive circulation system; and

- (f) The development or approval of any portion of a center shall require the development of a conceptual development plan which shall consider such things as, but not limited to, circulation, uniform architectural design, drainage/grading, buffers, phased improvements and landscaping.

Uses Permitted in Areas Designated "CC"

The following general categories of uses shall be permitted:

Retail businesses, such as but not limited to:

Department stores
Showroom/catalogue stores
Outlet or off-price stores
Variety stores
Import stores
Delicatessens
Bakeries and other specialty food stores
Wine and liquor stores (CUP)
Drug stores
Clothing stores
Shoe stores
Jewelry stores
Book stores
Record stores
Electronics equipment stores
Radio/TV/stereo stores
Photo equipment stores
Furniture stores
Wallcoverings stores
Lighting stores
Pet stores

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Hardware stores
Sporting goods stores
Plant stores
Toy stores
Gift shops
Home improvement centers
Carpet and flooring stores
Paint stores
Lighting stores
Musical instrument stores
Home furnishings and accessories stores
Telephone stores
Auto parts stores
Nurseries and garden supply stores
Home appliance stores
Plumbing supply stores

Service businesses, (or sales-and-service businesses), including but not limited to:

Travel agencies
Beauty parlors and barbers
Printers
Dry cleaners and laundries
Photo and art studios or galleries
Locksmiths
Interior designers
Landscape architects
Pool and spa installers
Cabinetmakers and other contractors
Home security analysts
Equipment rental
Home appliance repairmen

Administrative and professional offices, including but not limited to:

Real estate brokers
Insurance agent

Optometrists
Tax preparation service
Legal or accounting offices
Medical or dental offices
Administrative offices
Employment agencies
Escrow companies

Banks and other financial institutions (including drive-thru)

Restaurants with incidental serving of beer and wine but without a cocktail lounge, bar, entertainment or dancing

Restaurants with entertainment and/or cocktail lounge, bar (CUP)

Commercial recreation and entertainment, including but not limited to:

Movie theaters
Health clubs and spas
Music, dance and martial arts studios
Facilities for the performing arts

Automobile businesses limited to the following:

Service stations and car washes
Auto parts stores

Auto sales and services businesses affiliated with a department store or similar concern (CUP)

Community facilities as specified above

Hotels and motels

Wholesale businesses

Shopping centers subject to provisions as specified above (CUP)

Accessory structure and uses necessary or customarily incidental to the above uses

Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification

Uses Permitted in Areas Designated "OP"

The following general categories of uses shall be permitted:

Administrative offices

Professional offices, including but not limited to such professions as:

- Accounting
- Law
- Insurance
- Architecture
- Engineering
- Medicine
- Dentistry
- Real estate
- Financial brokerage
- Securities brokerage

Institutional and governmental uses

Retail and service businesses serving the needs of office users, including but not limited to:

- Blueprint and photo services
- Pharmacies
- Printers
- Stationers
- Secretarial services

Commercial recreation facilities compatible with office use, including but not limited to:

- Health clubs or gyms
- Racquetball courts

Banks and other financial institutions including drive-thru
Restaurants with incidental serving of beer and wine but without a cocktail lounge, bar, entertainment or dancing

Automobile service stations (CUP)

Community facilities as specified above

Accessory structures and uses necessary or customarily incidental to the above uses

Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification.

Uses Permitted in Areas Designated "RC"

The following general categories of uses shall be permitted:

Commercial recreation and entertainment facilities, including but not limited to:

- Health clubs, gyms, spas
- Racquetball clubs
- Dance studios
- Karate or yoga studios
- Swimming pools
- Tennis courts
- Batting cages (CUP)
- Trampolines (CUP)
- Miniature golf (CUP)
- Video arcades (CUP)
- Bowling alleys

Retail and service businesses oriented to active recreation, including but not limited to:

- Bicycle rental, repair, and sales
- Skate rental
- Kite shop

Sports equipment rental
Sportswear sales
Sporting goods store
Pro shop

Eating and drinking establishments, including but not limited to:

Sidewalk cafes
Coffee houses
Frozen yogurt or ice cream parlors
Snack bars or kiosks

Specialty retail and service businesses, including but not limited to:

Art galleries and studios, indoor or outdoor
Crafts and hobby shops
Flower shops
Health food stores
Gift shops

Public or private facilities for the performing arts, indoor or outdoor, including but not limited to:

Little theater
Amphitheater
Outdoor assembly or performing arts area

Grocery stores, general, convenience, and/or specialty (CUP)

Community facilities as specified above

Shopping centers subject to provisions as specified above (CUP)

Accessory structures and uses necessary or customarily incidental to the above uses

Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification

Uses Permitted in Areas Designated "NC"

The following general categories of uses shall be permitted:

Retail businesses, including but not limited to:

Grocery stores
Delicatessens
Bakeries and other specialty food stores
Wine and liquor stores (CUP)
Drug stores
Clothing stores
Variety stores
Hardware stores
Plant stores
Gift shops

Service businesses, including but not limited to:

Beauty parlors and barbers
Printers
Dry cleaners and laundries
Travel agencies
Locksmiths
Pharmacies

Administrative and professional offices

Restaurants (other than fast food), including serving of beer and wine but without a cocktail lounge, bar, entertainment or dancing

Restaurants with entertainment and/or serving of alcoholic beverages (CUP)

Fast-food restaurants (CUP)

Banks and other financial institutions (including drive-thru)
Commercial recreation facilities, including but not limited to health clubs and studios (CUP)
Automobile service stations (CUP)
Convenience markets (CUP)
Wine and liquor stores (CUP)
Community facilities as specified above, including institutional and governmental uses
Shopping centers subject to provisions as specified above (CUP)
Accessory structures and uses necessary or customarily incidental to the above uses
Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification

Uses Permitted in Areas Designated "MFC"

The following general categories of uses shall be permitted:

Residential uses as permitted in High Density and Medium High Density residential areas
Retail businesses
Restaurants
Administrative and professional offices
Restaurants with incidental serving of beer and wine but without a cocktail lounge, bar, entertainment or dancing.

Banks and other financial institutions (including drive-thru)
Service businesses
Institutional and governmental uses
Automobile service stations
Commercial recreation and entertainment facilities
Community facilities as specified above
Accessory structures and uses necessary or customarily incidental to the above
Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification

Uses Permitted in Areas Designated "MHO"

The following general categories of uses shall be permitted:

Residential uses as permitted in High Density and Medium High Density residential areas
Hospitals, clinics, and other medical uses
Medical offices
Other uses accessory or related to the above, including but not limited to:
Retail businesses
Administrative and professional offices

Service businesses

Pharmacies

Restaurants with incidental serving of beer and wine but without a cocktail lounge, bar, entertainment or dancing.

Commercial recreation and entertainment facilities

Community facilities as specified above

Accessory structures and uses necessary or customarily incidental to the above

Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification

Uses Permitted in Business Park Overlay Zone

In addition to the uses permitted by the base zone, the following general categories of uses shall be permitted in the Business Park Overlay Zone:

Administrative offices

Professional offices, including but not limited to such professions as:

Accounting

Law

Income tax

Insurance

Architecture

Engineering

Medicine

Optometry

Podiatry

Chiropractic

Osteopathy

Dentistry

Real estate

Escrow

Financial brokerage

Securities brokerage

Interior design

Institutional and governmental uses

Retail and service businesses serving the needs of office users, including but not limited to:

Printers
Stationers
Secretarial services
Office supplies and equipment
Office machine sales and service
Messenger and postal services

Commercial recreation facilities compatible with office use, including but not limited to:

Health clubs or gyms (CUP)
Racquetball courts (CUP)

Banks and other financial institutions

Eating and drinking establishments

Automobile service stations (CUP)

Retail studios, stores, and associated workshops for the purpose of sales, office, showroom, and design with workshop for custom-made samples of products for display only in the showroom for the following types of art and craft items: jewelry, picture frames, quilts, ceramics, potteries, and specialty gift items. Custom-made consumer goods within studios, stores, and associated workshops may be allowed, provided that the workshop areas for custom-made consumer goods are ancillary to the studios or stores and do not exceed 35 percent of the leased floor area, and subject to City Planner approval.

Retail and service businesses serving the needs of residential users, including but not limited to the following, and subject to the approval of the City Planner: Interior design office with showroom, catering establishments, contractors (excluding contractors' yards), beauty supply stores, fabric stores, shoe repair shops, antique dealers, security device sales and

service, telephone sales, luggage sales, pet grooming and supplies, opticians, medical supply sales and rentals, electrical, electronic, data processing, and telecommunications sales and service, audio/video sales and service, furniture sales, apparel shops, and similar businesses.

Convenience retail and service businesses such as food and beverage sales, newsstands, bookstores, barber and beauty shops, cleaners, travel bureaus, photo developing, tuxedo rental, bicycle sales and repair, locksmiths, ticket sales, etc.

Community facilities as specified above

Accessory structures and uses necessary or customarily incidental to the above uses

Other uses which are found by the Planning Commission to be consistent with the spirit and intent of this land use classification

Parking requirements for Business Parks shall be the same as for office parks, unless otherwise approved by the Planning Commission.

Site Development Standards for Office and Commercial Uses

Building site area, site width, and site depth: No minimum subject to Development Review Process.

Building site coverage: No maximum.

Building setbacks: As indicated in Table V-5.

Parking setbacks: As indicated in Table V-5.

Wall setbacks: As indicated in Table V-5.

Building height:

For areas designated "NC": 40 feet maximum.

For other areas: No maximum.

Building separation: As permitted by the Uniform Building Code.

TABLE V-5
OFFICE AND COMMERCIAL BUILDING REQUIREMENTS

Adjoining Condition	Building Setback	Parking Setback	Wall Setback
Major Arterial ¹	38' min, 43' avg ²	28' min, 43' avg	28' min ³
Secondary Arterial ¹ or Collector	25' avg	17' min ⁴	property line
Trail or Major Path	0' with 25' building separation	0'	0' with 15' clear area
Other Conditions	0' ⁵	0'	0'

¹ Setback measured from face of curb.

² Techniques to minimize height (berming, below-grade construction, etc.) may lessen this requirement, subject to design review.

³ Planters and associated walls for gateways excepted.

⁴ Must be screened if within 20 feet of curb face by landscaping, berm, or structural element with minimum height of 30 inches.

⁵ Subject to Uniform Building Code.

Definitions

Building: A structure having a solid roof supported by columns or walls, and suitable for human occupancy or use.

Building Height: The vertical distance, excluding foundations or understructures, between the average finished ground surface adjacent to the structure and the highest point of the structure, provided that a roof shall be measured to the average height of the roof but that no part of the roof shall extend more than five (5) feet above the permitted height. However, architectural features and appurtenances such as, but not limited to, chimneys, antennas, elevator and similar mechanical equipment, penthouses, screen walls for roof-mounted equipment, and widow's walks shall not be considered part of the building for purposes of this definition. Clock towers, identification monuments, fountains, and similar features may exceed the building heights permitted for adjoining buildings, subject to the approval of the City Planner.

Building Site: A lot, or contiguous lots of land in single, multiple, or joint ownership, which provides (exclusive of all rights-of-way and all easements, except open space easements, that prohibit the surface use of the property by its owner) the area and open spaces required by this ordinance for construction of a building or buildings, and which abuts a public or private street or alley, or easement determined by the City Planner to be adequate for the purpose of access.

Building Site Area: The gross area contained within the boundaries of a building site, but excluding any street rights of way.

Building Site Boundary: A boundary of a building site as defined herein.

Front boundary: The boundary abutting a street and constituting the edge of the street right-of-way, or the boundary designated as the front boundary on a subdivision or parcel map or site plan.

Rear boundary: A boundary, not intersecting the front boundary, which is most distant from and most closely parallel to the front boundary. In the case of an irregularly shaped or three-sided site, a line within the site ten (10) feet in length, parallel to and most distant from the front boundary, shall be interpreted as the rear boundary for purposes of determining required setbacks under this ordinance.

Side boundary: A boundary that is not a front or rear boundary.

Street boundary: Any boundary abutting a street.

Interior boundary: A boundary not abutting a street.

Building Site Coverage: The ratio between the ground floor area of the building or buildings and the building site area.

Unenclosed unsupported or post-supported solid roofs over patios and walkways, unenclosed unsupported or post-supported eave overhangs, and open trellis and beam construction shall not constitute buildings for purposes of this definition.

Building Site Depth: The distance between the midpoint of the front boundary and the midpoint of the rear boundary of a building site.

Building Site Width: The distance between side boundary lines of a building site, as measured at the front setback line, or the average of the front and rear boundary lengths, whichever is greater.

Cluster Development: A single family or multi-family residential development where dwellings and accessory or related buildings are situated in accordance with a comprehensive site plan into a more desirable arrangement than would be possible in a conventional development.

The entire cluster development or a portion of the development is considered a building site (each residential lot need not comply with the requirements for a building site), and accessory structures including garages may be separated from the dwelling unit. Cluster developments often include areas held in common by all residents, though they need not in every case.

Collector Road: As used in setback regulations, any road designated as a Collector on the Circulation Plan, including roads designated as Commercial Collectors unless otherwise specified.

Conventional Development: A single family residential development where each dwelling unit is situated on a lot that constitutes a building site as defined herein. A conventional development may, though generally it does not, include other lots held in common by all residents.

Density: The total number of dwelling units in a project site, divided by the gross area of the project site. The dwelling unit density range permitted under the applicable land use classification shall apply to the overall project site and shall not be literal to any division thereof.

Dwelling Unit: One or more rooms and a single kitchen, designed for occupancy by one family or household for living and sleeping purposes.

Gross Area: The entire area within the boundaries of a project site, building site, or lot, measured to the centerline of adjoining street rights of way.

Lot: Any parcel of real property delineated by a property line on an approved record of survey, plat, parcel map, subdivision map, or certificate of compliance as filed in the office of the City Community Development Department, County Recorder, or County Planning Department, or any parcel legally created or established pursuant to the applicable zoning or subdivision regulations in effect prior to the effective date of application of this ordinance to such parcel.

Mobile Home: As used herein, includes mobile homes as defined in Section 18008 or Section 18211 of the California Health and Safety Code, factory-built housing as defined in Section 19971 of the California Health and Safety Code, and manufactured homes as defined in Section 18007 of the California Health and Safety Code.

Multifamily Residential Development: A development where the number of dwelling units on one residential lot of record is three or more.

Multifamily dwellings, such as condominiums, rental apartments, and community or cooperative ownership apartments, are usually built in cluster patterns. However, dwellings such as rental or condominium-owned triplexes or fourplexes, which are technically multifamily, can have the appearance of traditional single family homes and may be incorporated into conventional developments subject to Development Review approval.

Project Site: The entire gross area of land for which comprehensive site planning is done at one time. A project site will generally include multiple building sites and may include a number of subdivision maps and/or site plans.

Property Line: The boundary of a lot or building site.

Secondary Arterial: As used in setback regulations, any road designated as a Secondary Arterial on the Circulation Plan, including roads designated as Special Secondary Arterials unless otherwise specified.

Setback Area: The area between the setback line and the building site boundary or street right of way line. The setback area shall be unobstructed by any building or portion of a building from the finished grade upward, except that:

- a. Eaves, balconies, cornices, chimneys, unsupported roofs or sunscreens, unroofed landings, porches and stairs, walkways, planters, air conditioning equipment, and similar architectural appurtenances may project into any required setback area as permitted by the Uniform Building Code;
- b. Fences, walls, poles, posts, and other customary yard ornaments, accessories, and furniture area permitted in any required setback area subject to the limitations in this ordinance;
- c. Patios, unroofed driveways or parking areas, swimming pools and equipment, tennis courts, and other unenclosed outdoor uses may extend into any required setback area if such uses are permitted by the land use regulations of this plan governing the building site; and

- d. Open trellis and beam construction shall be permitted to attach a carport to a dwelling or other building and may extend from a building to the building site boundary in the side or rear setback area, as permitted by the Uniform Building Code.

Easements along local streets for street trees, street maintenance, and/or utilities may contain provisions more restrictive than the above, affecting setbacks from local streets.

Setback Line: An imaginary line on a building site specifying the closest point from a building site boundary or an ultimate street right of way line (or, in certain cases, the back of the curb in an adjoining street) where a building may be located. The setback line is defined by the applicable setback distance provided in this ordinance; where no setback distance is given, or where the setback distance given is zero, the setback line is the property line or street right of way line. All setback distances are from the building site boundary unless specifically noted otherwise.

Single Family Residential Development: A development where each dwelling unit is situated on a residential lot of record and no lot contains more than one or two dwelling units. Single family development is deemed to include duplexes (two dwelling units on one residential lot).

Single family development may be detached (as in a traditional subdivision or zero lot line development) or attached (as in a duplex divided into two different lots, or a townhome). Single family homes may be arranged as either conventional developments or cluster developments; in either case, there may be some areas of the subdivision held in common by all residents.

Uniform Building Code: The Uniform Building Code as adopted by the City of Rancho Cucamonga.

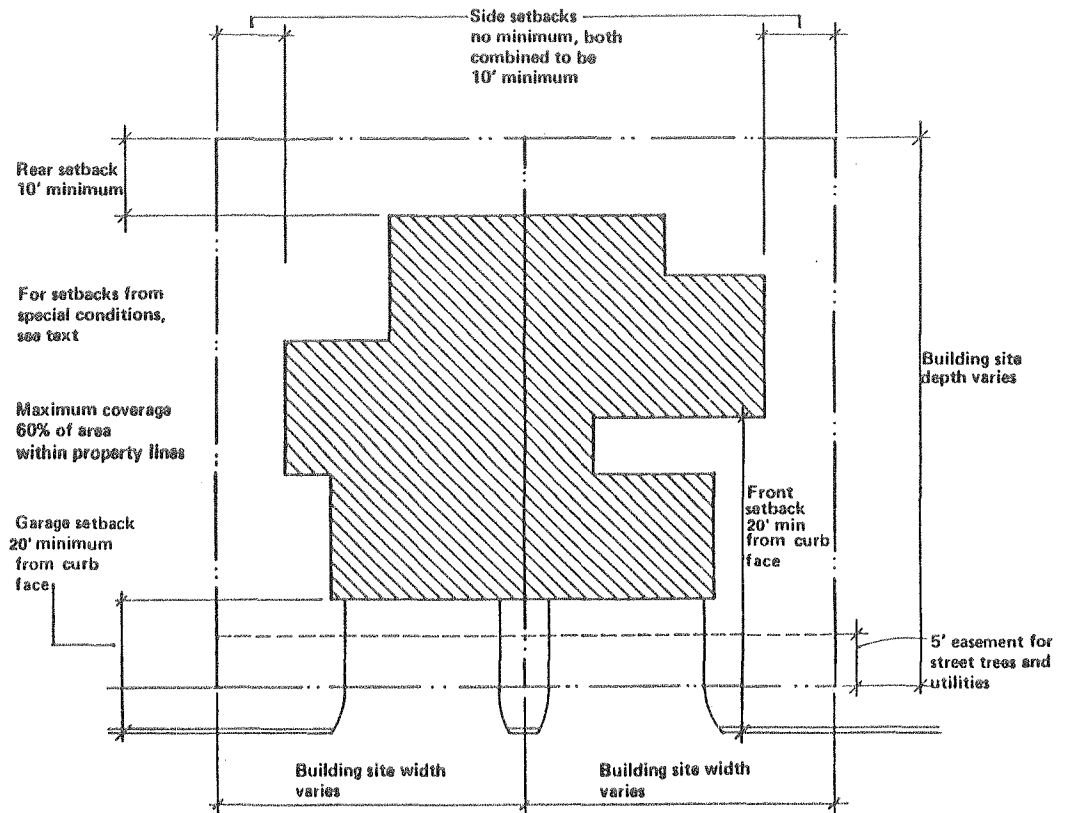


FIGURE V-3
Deleted from the 1990 Revised Community Plan.
No Longer Applicable

FIGURE V-4
Single Family Attached

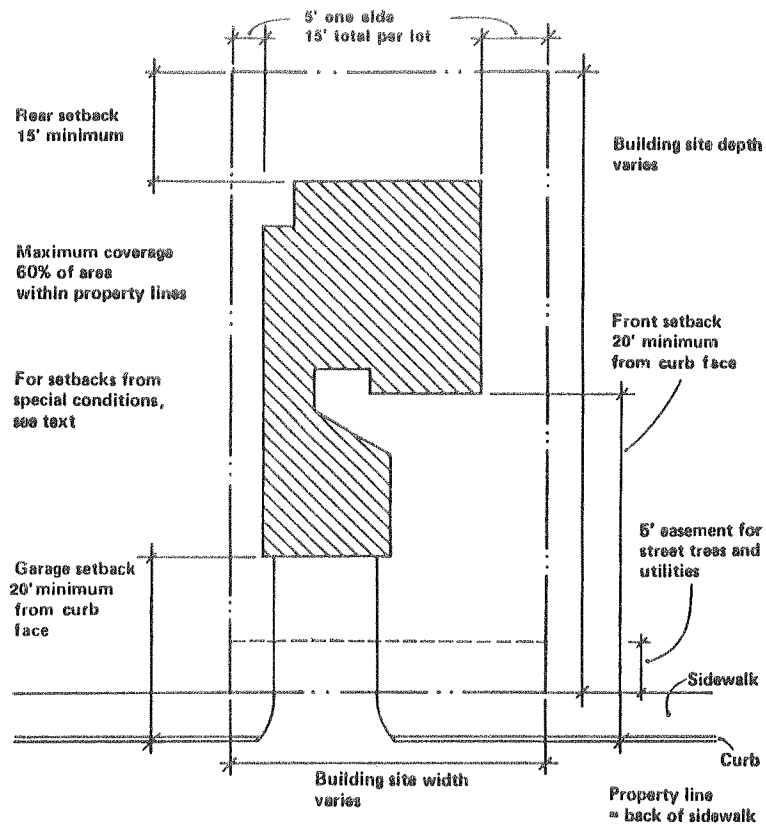


FIGURE V-1
Single Family Detached Center Plot

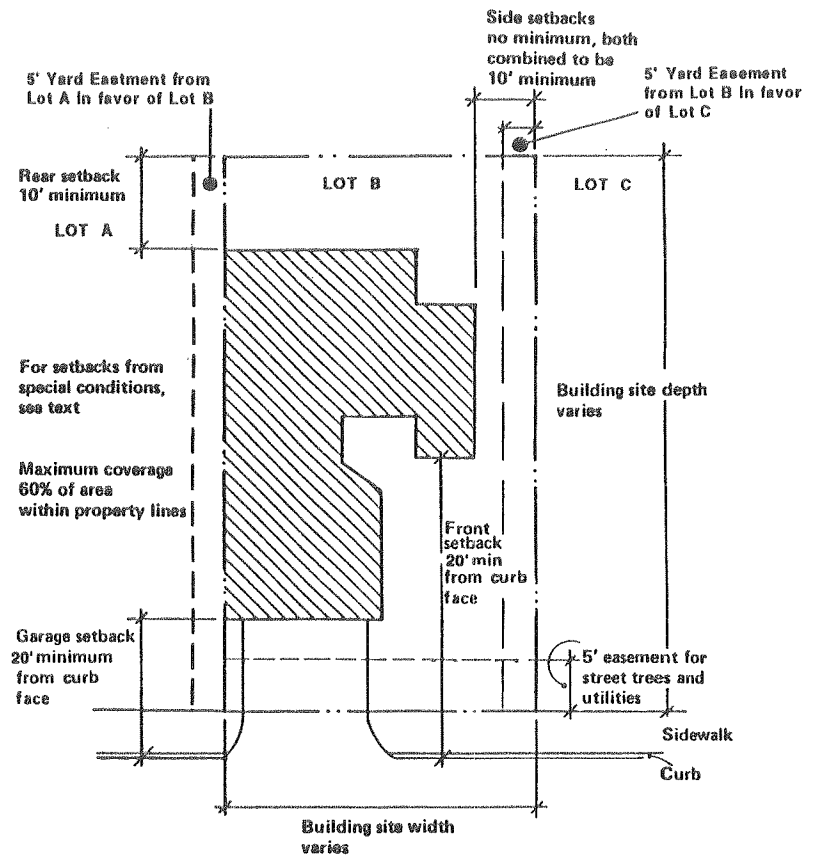


FIGURE V-2
Single Family Detached — Zero Lot Line

Where possible, driveway aprons should discourage the storing of automobiles in such a way that they block the public roadway or interfere with pedestrians

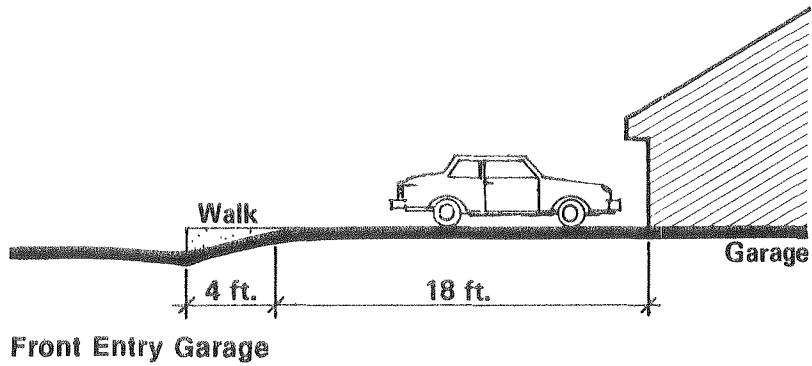
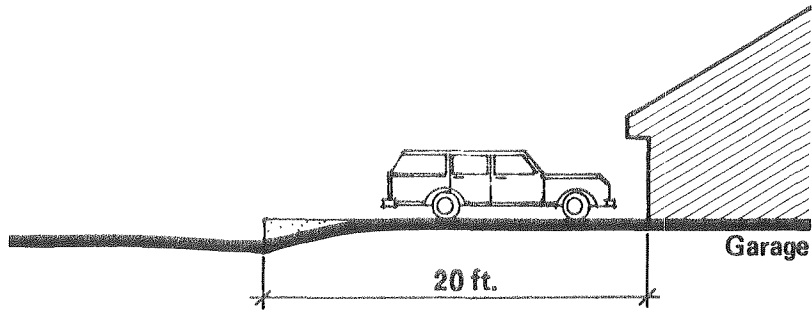
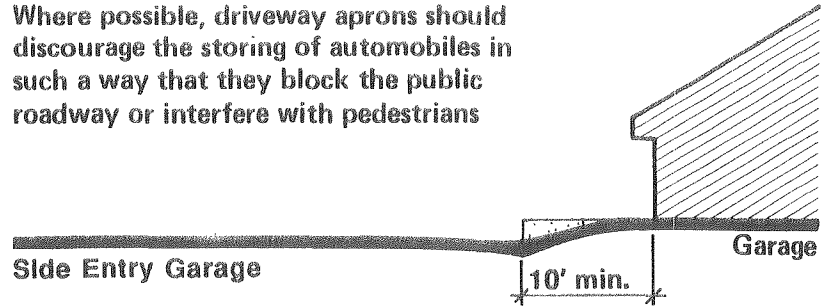


FIGURE V-5
Garage Setbacks from Local Public Streets

TABLE V-2
GARAGE AND CARPORT SETBACKS FOR LM AND M USES

Adjoining Condition	Front Setback	Side and Rear Setback
Local Public Street ¹	20' from curb face	6' from curb face
-Standard ²	(10' for side entry)	
-Nonstandard ²	0' from property line	0' from property line
Private Street ^{1,2,3}	0' from property line	0' from property line
Alley ³	0' from property line	0' from property line
Other Streets, Trails, Open Space ⁴	Same as for main buildings (see Tables V-1 and V-3)	Same as for main buildings (see Tables V-1 and V-3)
Adjoining Residential Lots and Other Conditions	Same as for main buildings (see Tables V-1 and V-3)	0' from property line ⁵

- ¹ Automatic garage door opener required if driveway length from curb face to garage door is less than 18 feet (mobile homes excepted).
- ² On standard local public streets, street trees and utilities are located within easements outside the right-of-way. These easements and/or other maintenance easements may require slightly greater building setbacks to avoid encroachment of structures into the easement area. See Figure IV-15 for various typical conditions on local public streets; similar easements may occur along private street frontages. Nonstandard local streets may also occur, where the right-of-way is enlarged to include trees, utilities, etc. On such nonstandard streets, the property line will typically occur 6-10 feet from the curb face.
- ³ With 26' of clear area directly adjoining for back-up purposes.
- ⁴ Includes greenway, parks, trails, and other public or private open space. Openings from structures onto such open space are permitted subject to the Uniform Building Code.
- ⁵ An attached or detached garage may abut a property line, or another structure on adjoining property, provided no openings are permitted on the surface abutting another residential lot, subject to the provisions of the Uniform Building Code.

VI Implementation of the Plan

VI Implementation of the Plan

Phasing

The Terra Vista Planned Community is a large-scale planning program and will take many years to implement. Full development of Terra Vista is expected to take 10 to 20 years. The phasing of development will be determined by many factors including market conditions, engineering constraints, and service capabilities. The timing and location of development may also depend in part on the availability of government-related financial programs for specific types of developments as well as for major infrastructure. Because some major infrastructure in Terra Vista (such as major roads and storm drain facilities) serves the City at large, assistance from the Redevelopment Agency, federal agencies, or other financial programs will be needed for its construction, and the availability and timing of such programs may in turn affect where development occurs. The developer will work closely with the City, the Redevelopment Agency, service districts, and other public agencies to coordinate phasing plans with infrastructural capacities and the provision of necessary public services.

Creation of coherent living environments is an important goal of the Terra Vista phasing program. Development in the initial years will work toward the creation of a neighborhood core, including a variety of housing types, local parks and trails, a local school, and community identity elements such as portions of specially landscaped roads. Some portions of the initial neighborhood core will of

course not be developed until later years. While it is likely that development will be underway in more than one planning area at a time (different neighborhoods or different areas within a neighborhood), it is intended that development will converge toward the creation of a few coherent living environments at a time rather than scattered individual projects. Construction of the park and greenway system will be phased to coincide with residential development to the extent feasible; this is discussed in detail later in this chapter. Certain facilities, such as schools, are not under the control of the developer; however, it is intended that school sites will be developable by the time they are needed to serve Terra Vista residents.

Residential development in Terra Vista will generally start from the north and work southward. The initial residential projects are planned for the northwest neighborhood.

Parks and Open Space

Provisions for Meeting Park Requirements

The park dedication requirements of the City pursuant to Government Code Section 66477, as amended in 1982 (SB 1785, Foran), have been estimated in Table VI-1. These requirements will be met by residential developments within Terra Vista through 100 percent credit for public parks, greenways, trails, and paseos, not to total less than 55.8 acres of the total amount required.

Park requirements will be met through a combination of dedication of land, improvement of park facilities and payment of fees, as discussed below.

Park System Implementation

Objectives. The implementation of Terra Vista's park and open space system will be a long-term process supported by the incremental contributions of individual residential projects. Because of the variety of circumstances that will pertain, coordination is required to see that development of the public park and greenway system proceeds in a logical manner. This coordination will be done jointly by the project sponsor and the City as discussed in more detail below. The implementation program for Terra Vista's open space system will be developed to serve the following objectives:

- To provide a method for the park system to be phased in balance with residential development. It is not feasible to provide a park before there are sufficient residents to pay for its installation and maintenance (or, in fact, to use it). On the other hand, it is not in the interest of the developer, the new residents, or the City to have a great deal of development take place without provision of park and recreation facilities. The goal is for park development and residential development to approximately coincide.
- To assure that the major greenway and trail links serving large areas of the Planned Community can be developed by the time they are needed, so that the open space system will be coherent and usable.
- To provide a way for individual projects to meet their park requirements at the time of development, without, however, requiring that park improvements occur in minuscule increments that are infeasible to develop, maintain, and use.

**TABLE VI-1
PARK REQUIREMENT IN RELATION TO RESIDENTIAL DEVELOPMENT**

Density Category	Total No. of Units Per Category	Breakdown by Dwelling Type (Estimated) ²	Persons Per Household ³	No. of Units Per Dwelling Type	Population
LM	1,823	SFD	3.43	1,823	6,252
M	2,701	1/8 Mobile	2.01	338	679
		2/8 SFD	3.43	675	2,315
		1/8 SFA/2	2.43	338	821
		1/8 SFA/3	2.29	338	774
		2/8 SFA/4	2.75	675	1,856
		1/8 MF/5-9	1.52	337	512
MH	2,119	All MF/5-9	1.52	2,119	3,220
H	<u>1,477</u>	All MF/10+	1.53	<u>1,477</u>	<u>2,259</u>
	8,120			8,120	18,688

18,688 Population at 3 Units per 1,000 = 56.06 acres. Park Requirement = 56.06 acres.

- ¹ Number of units estimated using midpoint of density range; from Land Use Summary, Table III-2.
- ² Dwelling type categories match those used in the 1979 Special Census of Rancho Cucamonga. Definitions of various categories were verified with Vince Torres-Gio of the Population Research Unit, State Department of Finance. SFD indicates single-family detached; SFA/2 indicates single-family attached with 2 dwelling units per structure; MF/5-9 indicates multifamily with 5-9 units per structure; etc. Dwelling type breakdown within each density category has been estimated by Lewis Development Co. based on the best information currently available.
- ³ Persons per household for each dwelling type are taken from the April 1979 Special Census of Rancho Cucamonga as reported in the Community Profile, City of Rancho Cucamonga, August 1979, Table 5. This is the most recent available official census providing the average size of each class of household as required under SB 1785 (Foran, 1982).

- To time the development of park areas so that early homeowners will not be faced with an exorbitant maintenance burden.
- To apply each project's park contribution to facilities that will reasonably benefit those residents: first to the facilities of most immediate benefit to them, and secondarily to facilities serving them as well as the Planned Community as a whole.

Balance Between Parks and Housing. As discussed in detail in Chapter III, the park system in Terra Vista is generally balanced with the population distribution. In other words, the park requirement for the residential developments in a given area is about equal to the amount of public open space shown on the plan. As explained in Chapter III, the areas in the northwestern part of the community have somewhat more park than the standard, and other areas are slightly under standard.

The implementation concept for Terra Vista's open space system involves transferring small amounts of park contribution from some areas to other nearby areas so that the overall park and greenway system can be funded. These transfers are generally small; the bulk of the park facilities in each area will be funded by the residents in that immediate vicinity. To eliminate these transfers - to have every area exactly in balance - would violate the principles of park location discussed elsewhere in this plan and would essentially require the elimination of the greenway. The aim then is to provide a workable method for making small transfers of park contribution in order to implement the park and greenway system serving the entire community.

Implementation Process. Each residential project will comply with its park requirement at the time of development. Typically, the park requirement for an individual development or phase will be quite small. In general, it is not feasible to provide park improvements in very small increments. Therefore, typically the first projects in an area will meet their park requirement by providing land for the local park. Once enough land has been dedicated to make improvements feasible, the next projects will contribute to improvement of the same park.

Each project will meet its park requirement by contributing land or improvements for parks, trails, and/or the greenway. This may be done in any of the following ways:

- Dedication of raw land at the local park site, equal in value to the park contribution otherwise required (or, dedication of the amount of raw land required under the standard and separate pledging of the improvement portion).
- Once all land for the local park (or enough acreage to make improvements feasible) has been dedicated, contributing to the improvement of that park.
- If the project adjoins a trail, dedication and improvement of that trail.
- Dedication and/or improvement of a trail link extending beyond the project, if providing that trail would complete an appropriate link in the system.

- If all park facilities in the local area are completed, contribution of land and/or improvements for facilities in the next area to be developed, or a portion of the major greenway.

It is intended that each project's park contribution will first be applied to park facilities in the immediate vicinity, and used elsewhere only if those facilities are already complete. (However, in some cases the City and sponsor might agree that certain local facilities should be deferred and the park contribution applied elsewhere in Terra Vista.) The specific location and nature of offsite park contributions will be worked out jointly by the City and the sponsor at the time, considering what development is already in place and what is planned to be built next.

The application of a park contribution offsite should bear some reasonable relation to the park needs of the residents who have funded the contribution. The ultimate aim of these offsite park contributions will be to implement the primary greenway and the trail network linking the entire community. However, there is no reason to develop the primary greenway until there are a significant number of homes in place which are so located as to make use of it. For this reason, when offsite park contributions occur, they will initially "roll through" the next area being developed rather than be transferred directly to the greenway itself.

Any offsite contribution to the park and greenway system is subject to acceptance by the City. Therefore, the City will always retain final control over the phasing of the park system and can ensure that it comes together in a reasonable pattern.

The project sponsor and the City will jointly develop, in the near future, a detailed program for implementation of Terra Vista's open space system, which will become an appendix to the Community Plan.

Maintenance of Community Public Use and Open Space Facilities

Facilities designed for public use such as local parks, greenways, trails, parkways, roadside planting belts, and related improvements will be dedicated to the City and will be maintained by a City assessment district or other maintenance vehicle funded by the residents of Terra Vista. The details of this maintenance program are to be finalized by the project sponsor and the City Engineer. The assessment vehicle should be formed concurrently with recordation of the first residential developments. The allocation of maintenance costs for parks in joint use with schools will be determined by agreements with the affected school districts.

Flexibility in Density and Land Use

This Community Plan is the product of the best currently available predictions of market needs and development constraints over the lifetime of the project. Some uncertainty is unavoidable because changes in market trends, economic conditions, and community needs cannot be forecast with absolute reliability. Some elements of this plan also represent compromises between different planning objectives, which may be reevaluated at some time in the future. Revisions and improvements are therefore certain to occur.

The Terra Vista plan provides flexibility to modify elements of the plan over time without altering the basic concept of the community. This flexibility is one of the advantages of the Planned Community approach to development, allowing change while protecting the community structure as a whole. To make this possible, the following guidelines are

provided. Except as otherwise noted, modifications of this nature do not require an amendment to the Community Plan.

- Minor changes in the size and configuration of development parcels depicted on the Land Use Plan are inevitable and will be permitted as long as the overall development concept is not compromised. This is particularly true of school site locations, sizes, and configurations, which are subject to the ultimate approval of the school districts, and park areas, which will be designed in cooperation with the City. The community plan is at a large scale and all boundaries and shapes are therefore approximate. In residential areas, comprehensive or concurrent site planning may also result in some "merging" of two adjoining land use classifications with respect to the entire area being developed. Modifications of this sort may be made through the normal Development Review Process.
- To allow flexibility in responding to housing needs, any residential parcel within the Planned Community may be redesignated for another residential use, within the limits spelled out below. However, such changes should not alter the intent of the plan to maintain desirable relationships between various land uses and to provide a variety of residential housing types and densities throughout the community. Thus, a variation in the number of dwelling units in one area may (or may not) require decreases in one or more of the other areas to assure that the total number of dwellings does not exceed the number allowed within the Planned Community.

"LM" land use designations can be redesignated to "M" land use.

"M" land use designations can be redesignated to "LM" or "MH" land use.

"MH" land use designations can be redesignated to "M" or "H" land use.

"H" land use designations can be redesignated to "MH" land use.

Modifications of this nature shall be reviewed and approved by the Design Review Committee, Planning Commission and City Council.

- The provision of affordable housing is an important goal of this Community Plan. Development of affordable housing may result in density increases, as explained later in this chapter.
- An area of approximately 99 acres, bounded by Base Line Road, Deer Creek Channel, the Southern Pacific Railroad, and Milliken Avenue, has been identified in the City's General Plan as the proposed location of a City central park. It has been required that a portion of this site be provided by the Terra Vista Planned Community. The City intends to acquire and develop the remaining acreage by other means. The site has been identified in this Plan as a proposed City central park at the City's instruction in accordance with the expressed intention of the City to acquire this site. This designation is contrary to the alternative uses that the sponsor proposes. The property owners reserve the right to develop the property in the future if the City is unable or

unwilling to consummate a purchase. In the event the City does not in fact acquire this site for park development within a reasonable period of time, the site will be developed for income property uses, that is, rental properties intended to be maintained by the owner/developer for its investment portfolio, as explained in Chapters III and IV. Any dwelling units developed on this site would be in addition to the base number of dwelling units for the Planned Community shown in the Land Use Summary and the Density Distribution Plan.

The timing and method of the City's proposed acquisition are presently unknown. In view of this situation, this Plan provides for a period of 24 months after adoption of the Plan for the City to propose and the landowners to agree on the terms and conditions upon which all or part of this site shall be acquired. This does not mean that the site must actually be acquired within 24 months, but rather that a realistic plan for its acquisition that is acceptable to all parties is to be worked out. At the end of the 24-month period, if no such acquisition program has been agreed upon, the land use for the property will automatically become as depicted in Figure VI-1 of this Plan (see following page), and the property owners shall thereafter have the right to submit proposals for development. Should an acquisition program have been agreed upon for only a portion of the property, the land use for the balance of the property will be as shown in figure VI-1. This change in land use designation does not imply that development will occur immediately thereafter, nor does it preclude the City from undertaking future negotiations with the property owners for the acquisition of additional land within the site for City park purposes. Development of

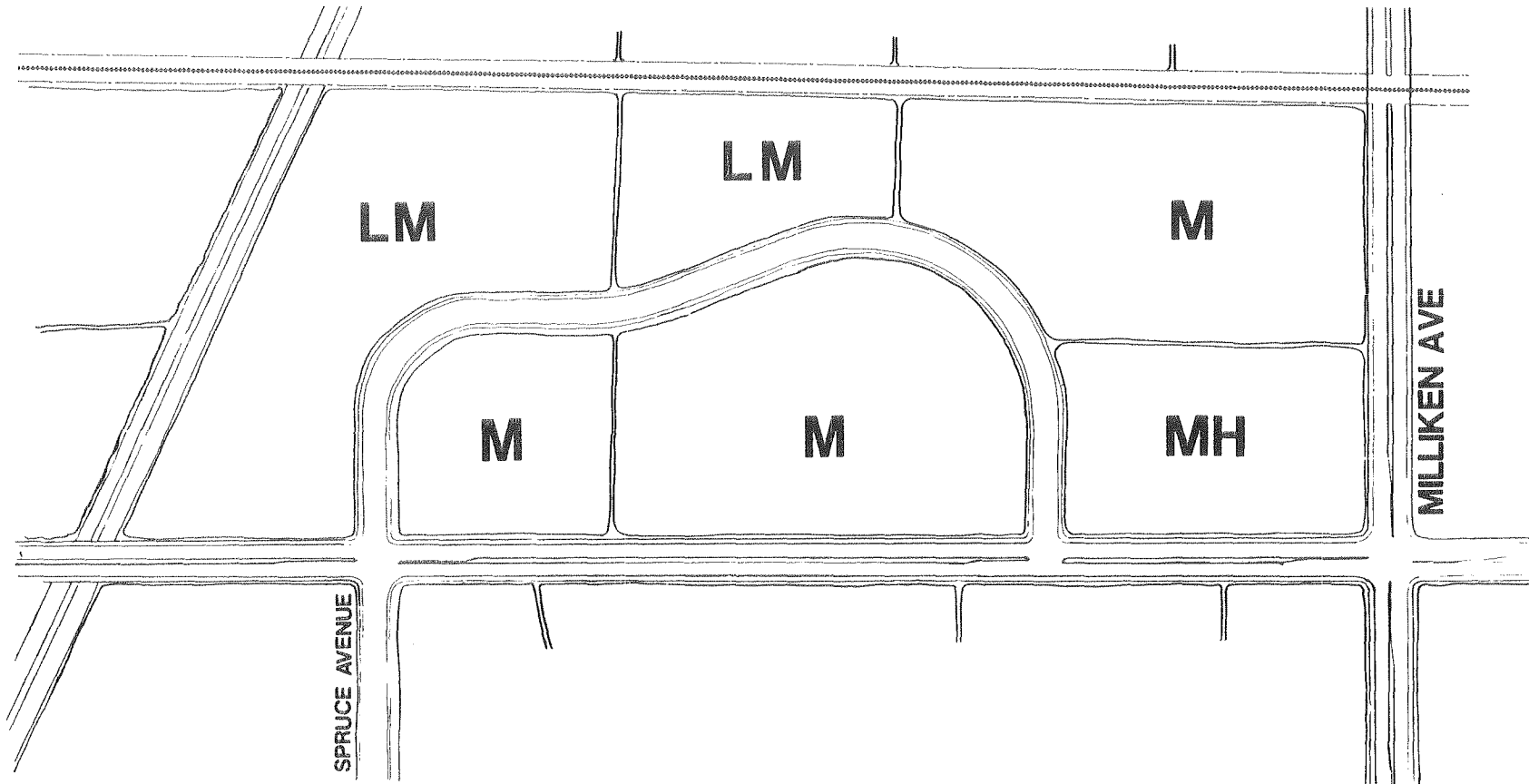


FIGURE VI-1
Alternative Land Use for Area North of Base Line Road

approximately 800 dwelling units on this site have already been evaluated in the Community Plan EIR.

Note to 1990 printing:

The City has acquired the 99 acre Central Park site and is currently planning its development.

Density Distribution Plan

Figure VI-2 indicates the midrange densities for each residential development area which have been assumed for planning purposes during the preparation of this plan. The base density of the Planned Community at midrange density is 8,120 dwelling units. Absent an amendment to the Community Plan, total residential development in Terra Vista is not to exceed 8,120 units except in the cases of density bonuses for affordable housing development or rental residential development on the proposed City park site, as provided elsewhere in this chapter.

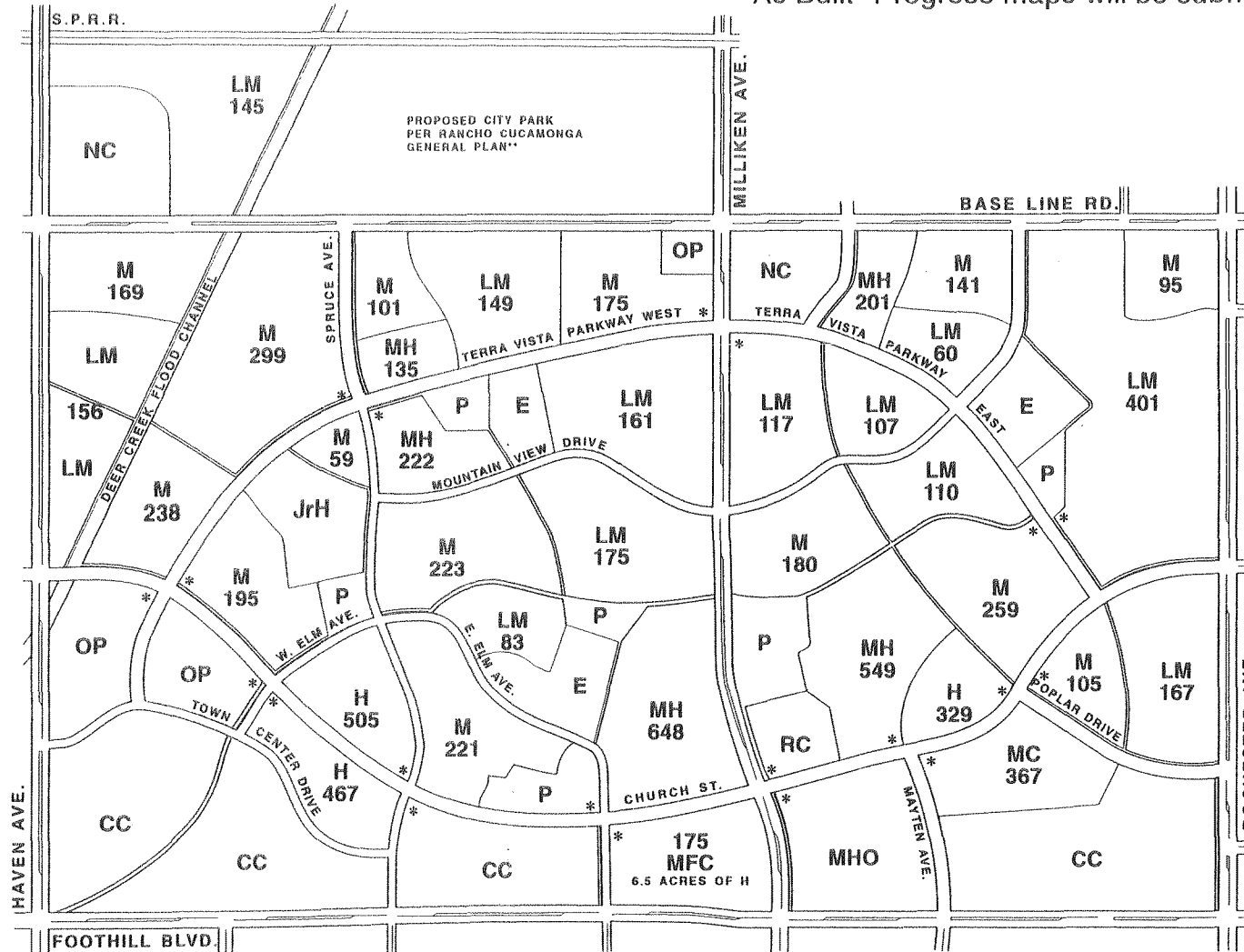
Development in any individual area may vary from the minimum to the maximum permitted by the land use density range, which may be further adjusted as discussed in the preceding and following sections. Therefore, the ultimate density of development in an area may vary from that shown in Figure VI-2. A variation of up to 25% in the density of any neighborhood is within the parameters of the plan. Periodically, the actual number of units developed will be compared against initial planning assumptions to determine if any adjustments are warranted.

Affordable Housing

Providing housing that is affordable to the average family is an important goal of the Terra Vista Community Plan. Terra Vista is meant to provide housing opportunities for a wide variety of people, income groups, and life styles.

The following definitions apply to the affordable housing program for Terra Vista:

For Actual Land Use and Density Distribution of Projects approved through February, 1990 - See Figures VI-3 and VI-4. Updated "As Built" Progress maps will be submitted to the City, periodically.



RESIDENTIAL		
LM	LOW MEDIUM DENSITY	(4-8 DU/AC)
M	MEDIUM DENSITY	(8-14 DU/AC)
MH	MEDIUM HIGH DENSITY	(14-24 DU/AC)
H	HIGH DENSITY	(24-30 DU/AC)
COMMERCIAL		
CC	COMMUNITY COMMERCIAL	
NC	NEIGHBORHOOD COMMERCIAL	
RC	RECREATIONAL COMMERCIAL	
OP	OFFICE PARK	
MIXED USE		
MFC	FINANCIAL, RESTAURANTS, RESIDENTIAL HOSPITAL & RELATED FACILITIES, OFFICE	
MHO	OFFICE	
PUBLIC & QUASI-PUBLIC		
JrH	JUNIOR HIGH SCHOOL (PROPOSED)	
E	ELEMENTARY SCHOOL (PROPOSED)	
P	PARKS	
*	BUS STOP	

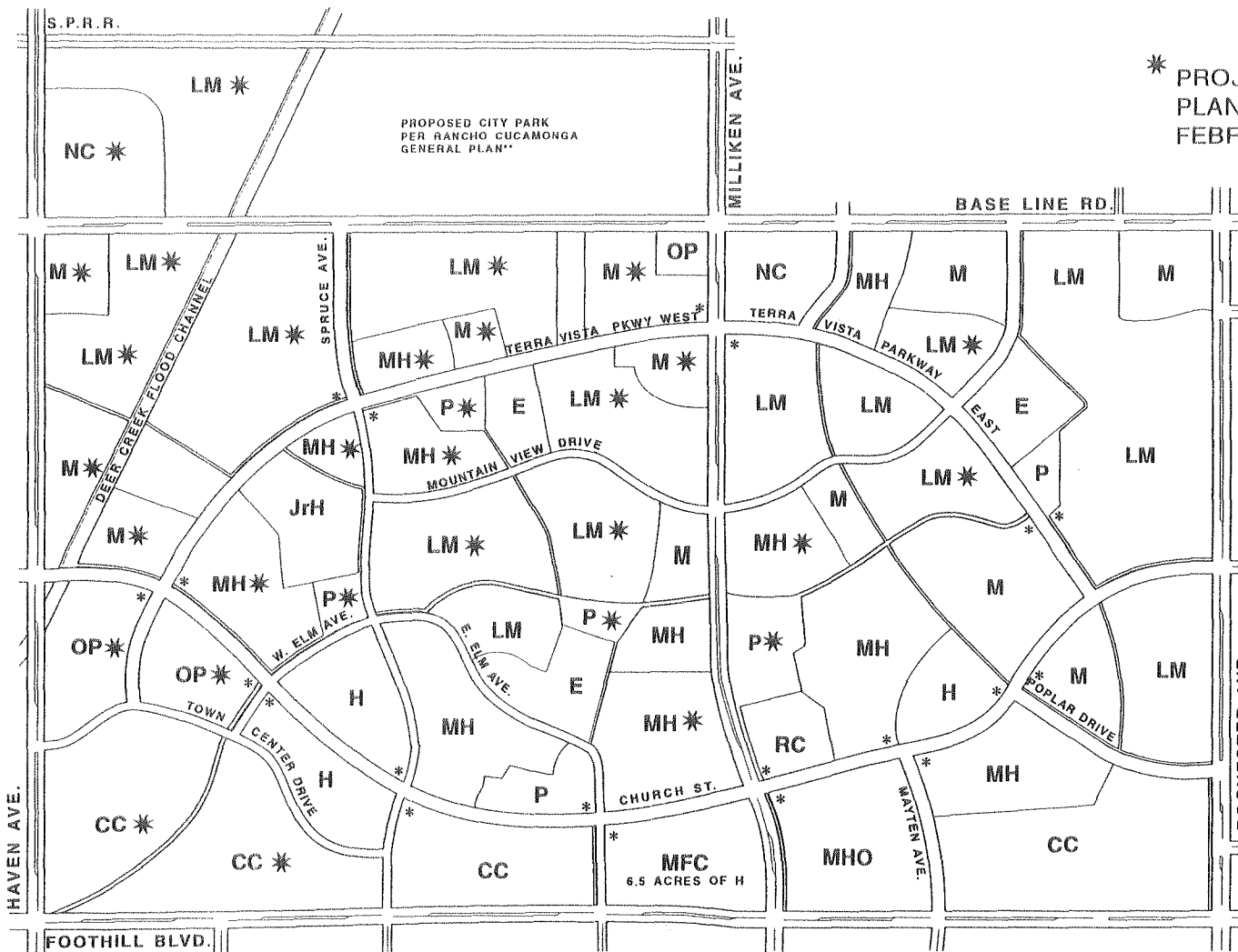
**PARTIAL DEDICATION OF UNIMPROVED LAND



FIGURE VI-2

Density Distribution Plan

REVISED Amendment Nos. 1, 2, 5, 6 & 7
VI-10



* PROJECTS BUILT OR APPROVED BY
PLANNING COMMISSION THROUGH
FEBRUARY, 1990.

RESIDENTIAL	
LM	LOW MEDIUM DENSITY (4-8 DU/AC)
M	MEDIUM DENSITY (8-14 DU/AC)
MH	MEDIUM HIGH DENSITY (14-24 DU/AC)
H	HIGH DENSITY (24-30 DU/AC)
COMMERCIAL	
CC	COMMUNITY COMMERCIAL
NC	NEIGHBORHOOD COMMERCIAL
RC	RECREATIONAL COMMERCIAL
OP	OFFICE PARK
MIXED USE	
MFC	FINANCIAL, RESTAURANTS, RESIDENTIAL HOSPITAL & RELATED FACILITIES, OFFICE
MHO	MIXED USE OFFICE
PUBLIC & QUASI-PUBLIC	
JrH	JUNIOR HIGH SCHOOL (PROPOSED)
E	ELEMENTARY SCHOOL (PROPOSED)
P	PARKS
*	BUS STOP

**PARTIAL DEDICATION OF UNIMPROVED LAND

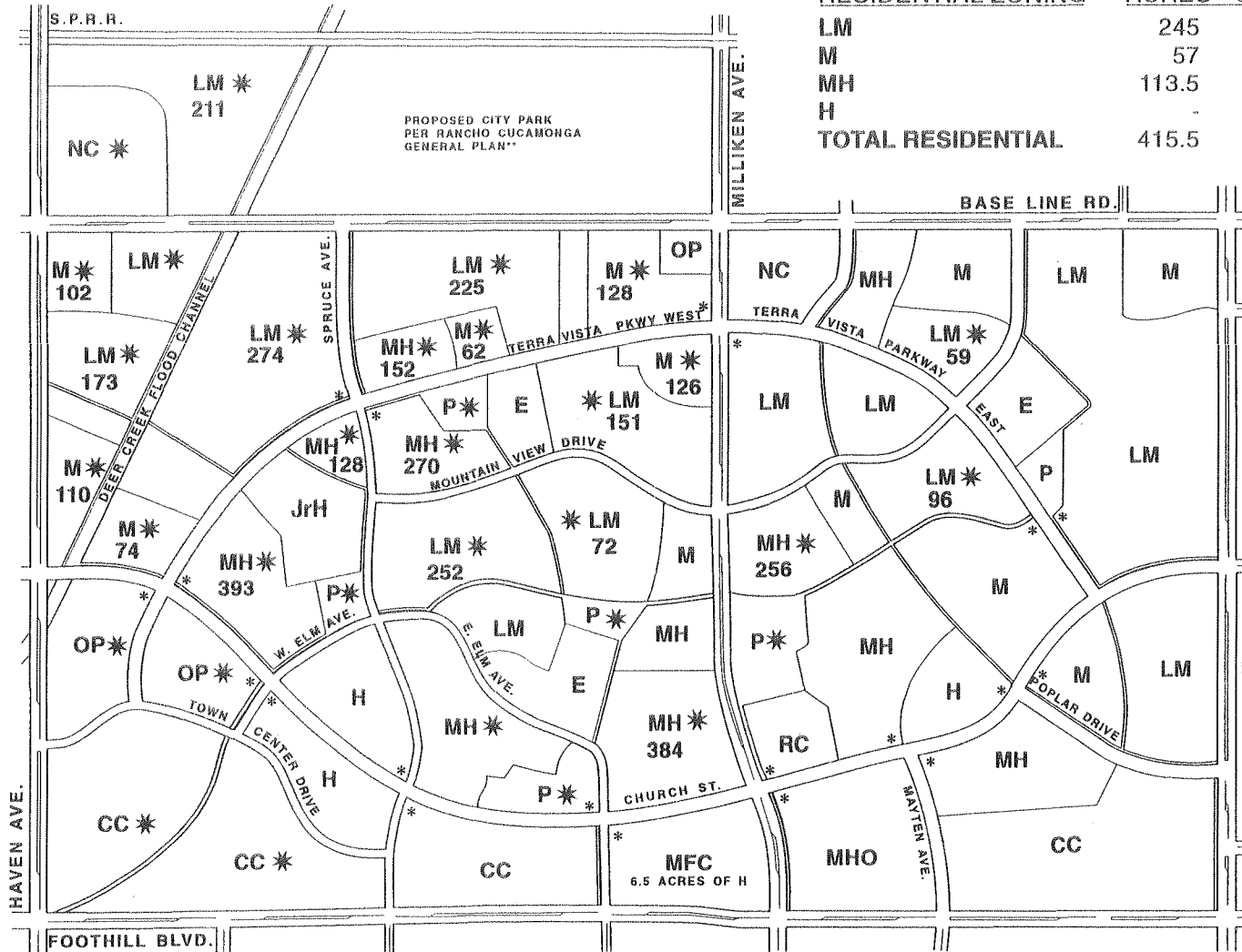


FIGURE VI-3
As Built Land Use Progress Plan

* PROJECTS BUILT OR APPROVED BY PLANNING COMMISSION THROUGH FEBRUARY, 1990.

STATUS AT FEBRUARY 1990:

RESIDENTIAL ZONING	ACRES	UNITS	DU/AC
LM	245	1,441	5.9
M	57	602	10.6
MH	113.5	1,977	17.4
H	-	-	-
TOTAL RESIDENTIAL	415.5	4,020	



RESIDENTIAL	
LM	LOW MEDIUM DENSITY (4-8 DU/AC)
M	MEDIUM DENSITY (8-14 DU/AC)
MH	MEDIUM HIGH DENSITY (14-24 DU/AC)
H	HIGH DENSITY (24-30 DU/AC)
COMMERCIAL	
CC	COMMUNITY COMMERCIAL
NC	NEIGHBORHOOD COMMERCIAL
RC	RECREATIONAL COMMERCIAL
OP	OFFICE PARK
MIXED USE	
MFC	FINANCIAL, RESTAURANTS, RESIDENTIAL HOSPITAL & RELATED FACILITIES, OFFICE
MHO	OFFICE
PUBLIC & QUASI-PUBLIC	
JrH	JUNIOR HIGH SCHOOL (PROPOSED)
E	ELEMENTARY SCHOOL (PROPOSED)
P	PARKS
*	BUS STOP

**PARTIAL DEDICATION OF UNIMPROVED LAND



FIGURE VI-4

As Built Density Distribution Progress Plan

REVISED Amendment Nos. 1, 2, 5, 6 & 7
VI-12

- An affordable for-sale dwelling unit is one which is purchased by a low or moderate income family, or which a low or moderate income family can qualify to purchase under criteria then being applied by lending institutions.
- An affordable rental dwelling unit is one which is rented by a low or moderate income family, or for which the monthly rent does not exceed 30% of the monthly gross income of a moderate income family.
- A moderate income family is one whose income is from 80% to 120% of the median family income in the area. The index used for median family income should be representative of the location where the housing is being built.
- A low income family is one whose income is below 80% of the median family income.

It is a goal of this plan that 15% or more of the dwellings within Terra Vista will be affordable to low or moderate income families when initially offered for sale or rent. It is intended that approximately one-third of these dwellings will be affordable to families with 100% to 120% of median income, one-third to those with 80% to 100% of median income, and one-third to those with 50% to 80% of median income. It is also intended that some of these units will be specifically targeted to meet the needs of the elderly and handicapped as well as mobile home dwellers. However, to make affordable housing possible under market conditions prevailing today and expected to prevail in the future, flexibility and cooperation between the public and private sectors is needed. It is not the intent of this plan that affordable housing

be provided under conditions which are economically infeasible for the owner/developer or which require a direct or indirect subsidy of the affordable units by the occupants of other homes. Experience has shown that these inequities can be avoided, and affordable housing promoted, through a close partnership between the public and private sectors in support of this specific goal. Such a partnership often produces results such as the following:

- Mortgage financing can be made available at reasonable rates, if not in the open market, then through federal or state programs, or through locally initiated programs such as mortgage revenue bonds and redevelopment funds.
- The City can grant economically significant concessions on items that affect costs, such as parking requirements, amenities, development standards, density, processing time, and fees.
- If necessary, direct financial assistance can be provided by the Redevelopment Agency or other agencies through land writedowns, mortgage buydowns, installation of improvements, or project financing.

The specific measures necessary to make any project affordable can only be determined on a case by case basis. Therefore, good-faith efforts by both the public and private sectors in support of affordable projects will be essential, and this is an integral part of the affordable housing program for Terra Vista.

Existing state law (Government Code Section 65915) requires that when at least 25% of the dwelling

units in a residential project are affordable to low or moderate households, or 10% of the units are affordable to lower income households, the local agency must either grant a density bonus of at least 25% above the maximum density otherwise allowable, or must grant maximum other incentives or concessions of equal financial value to the density bonus. Development of affordable housing in Terra Vista will be governed by these provisions.

In furtherance of this affordable housing law and to help make it economically feasible, a density bonus of one additional unit will be granted for every affordable unit provided in Terra Vista. This bonus may be applied in the affordable project itself or, at the developer's option, may be transferred to another location within the community. However, absent an amendment to the Community Plan, the density bonuses granted for affordable projects will not, in total, exceed the number of affordable units provided or 1,218 units, whichever is less.

It is a goal that 15 percent of the homes in each of the four neighborhoods of Terra Vista will be affordable as defined above. While affordable housing will be thus dispersed throughout the community, it is not the intent of this plan that any particular project or area must have 15% affordable units, or that 15% of the units being developed at any given time must be affordable. The provision of affordable housing is expected to vary from one area to another and also to change over time, depending on market conditions, the availability of government financing, and overall progress of the project. As the community evolves over time, the

city and the developer will work together to plan where and when affordable housing will be built.

It is intended that affordable housing will be delivered to the original occupants but will not thereafter be controlled as to price or sale, unless required as a condition of a special program or by law applicable to the particular project. It is the intent of this plan that inequitable subsidies of affordable units by other Terra Vista residents or by the owner/developer will be avoided, and that any subsidies employed will be fair and proper as provided under federal and state law and policy. In other words, rather than sell a dwelling worth \$70,000 at an arbitrary price of \$50,000, which would confer a "windfall" of \$20,000 on the low or moderate income purchaser at the expense of the developer or of the purchasers of other homes, it is intended to use a variety of measures to bring costs down so that the unit can be sold for \$50,000 as a fair price. This eliminates any windfall which would warrant resale controls. It also eliminates the possibility that other buyers within the project will pay higher prices to underwrite the affordable price of the affordable unit. Any subsidies employed will not come from neighboring families but from government programs which are supported by small tax contributions from all citizens in accordance with national, state, and local policies in support of affordable housing.

To summarize, it is the philosophy of this plan that involvement of local government in the sale and resale of affordable units should be avoided, and that such involvement should not be needed under the type of affordable housing program planned for Terra Vista.

Development Review Process

All development in the Terra Vista Planned Community shall be subject to the provisions of this section. The Development Review Process is established to assure that development within Terra Vista carries out the intent of the Terra Vista Community Plan and complies with the provisions of the Growth Management Ordinance (City Ordinance No. 86).

An application for approval of a development project shall be made in two stages to the Community Development Department of the City. Information should be included in the submittal for each stage as follows:

- **Stage 1: Tentative Tract Map Filing.** Concurrently with any application for a Tentative Tract Map or Parcel Map for development purposes, the proposed project developer shall also submit the following for review and approval by the Planning Commission:
 - a. Site Utilization Map including:
 - (1) Vicinity Map to show the relationship of the proposed development to adjacent development and surrounding area. This may be a small inset map.
 - (2) Conceptual Use Layout Map showing the location and types of proposed uses, the nature and extent of open space, the extent of any other uses proposed, and an indication of all adjacent uses.
 - b. Conceptual Grading and Drainage Plan and Natural Features Map showing a general indication of type and extent of grading.

- c. Anticipated development schedule of proposed project, including phasing.
- d. Such other information as may be specified on forms provided by the Community Development Department.

The Planning Commission shall approve or deny the application after a properly noticed public hearing in accordance with the procedures set forth by City Ordinance.

- **Stage 2: Design Review Approval.** Design review for nonresidential development and for multifamily residential development shall be concurrent with tentative tract approval. Design review of single family subdivisions may be concurrent with tentative tract approval at the option of the developer. The developer of the proposed project shall submit the following Design Review Package for review and recommendation by the Design Review Committee:
 - a. Detailed Site Plan to include information specified in forms provided by the Community Development Department.
 - b. Illustrative Site Plan to include proposed and existing improvements, landscape concepts, and other elements as may be necessary to illustrate the site plan.
 - c. Preliminary Architectural Plan showing elevations or perspectives of proposed buildings and structures indicating materials

to be used, trees, and landscaping. Residential development plans may include typical elevations or perspectives only, at the option of the developer.

- d. Such other information as may be specified on forms provided by the Community Development Department.

The Planning Commission, in accordance with the procedures set forth in Ordinance 79-74, Section 2A, shall consider and approve, deny, or modify the plans submitted as part of the Design Review Package.

Uses for which a Conditional Use Permit is required under this plan shall comply with the City's normal CUP process.

Amendments to the Plan

The Terra Vista Community Plan is adopted pursuant to the Planned Community provisions of the City of Rancho Cucamonga Zoning Ordinance and implements in specific terms the broader goals of the Rancho Cucamonga General Plan. The provisions of this plan supersede those of the Zoning Ordinance and related regulations with respect to items addressed in this plan, and the land use designations specified in this plan supersede those specified in the General Plan. However, areas not specifically covered by this plan (such as construction standards, health regulations, and subdivision procedures) will be governed by existing City regulations.

No amendment is required to this plan in order to add new or modified information (textual or graphic) that does not contradict the intent of the material already included in the plan. Such changes may be made administratively by the City Planner with the concurrence of the project sponsor. This chapter has also described various types of changes to the plan which may occur under certain circumstances and will generally be made through the Development Review Process. These changes can also be incorporated into the plan administratively if deemed of sufficient importance.

An attempt has been made to provide enough flexibility for implementing the plan so that Community Plan amendments will not be necessary on a routine basis. However, major changes which would affect the concept of the plan will require a plan amendment. Examples of such changes would be a dramatic restructuring of the circulation system or a wholesale change in the general land use

pattern. Any amendments to the plan will be made as provided in the Planned Community provisions of the Zoning Ordinance.

ACKNOWLEDGEMENTS

LEWIS DEVELOPMENT CO./LEWIS HOMES
Real Estate Development
1156 North Mountain Avenue
P.O. Box 670
Upland, California 91786

Ralph M. Lewis, Chairman of the Board
Richard A. Lewis, President
Randall W. Lewis, Senior Vice President
Roger G. Lewis, Senior Vice President
Kay Matlock, Project Manager
Gerald T. Bryan, Engineering Coordinator
John R. Melcher, Director of Architecture
William K. Speer, Architectural Coordinator
John M. Goodman, Vice President
Dennis A. Alekel, General Counsel

Lewis Development Co. gratefully acknowledges the participation of all elected officials, appointed Planning Commissioners, and staff members of the City of Rancho Cucamonga.

Staff Members:

Jack Lam, City Manager
Lauren M. Wasserman, former City Manager*
Rick Gomez, Community Development Director
Brad Buller, City Planner
Michael D. Vairin, Senior Planner/Project Manager*
Tim Beedle, Senior Planner*
Nancy Fong, Senior Planner
Shintu Bose, Deputy City Engineer
Lloyd Hubbs, City Engineer*
Paul Rougeau, Senior Civil Engineer
William Holley, Community Services Director*
Edward A. Hopson, City Attorney*
Ralph Hanson, City Attorney*

Mayor and City Council:

Dennis L. Stout, Mayor +
Charles J. Buquet II, Mayor Pro Tem +
William J. Alexander, Mayor
Paul Biane, Council Member
James V. Curatalo, Council Member
Diane Williams, Council Member
Rex Gutierrez, Council Member
Jon Mikels, former Mayor +
Richard M. Dahl, Council Member +
James C. Frost, Council Member +
Phillip D. Schlosser, Council Member +

Planning Commission:

E. David Barker, Chairman
Larry McNeil, Vice Chairman
Peter Tolstoy, Commissioner
John Melcher, Commissioner
Heinz Lumpp, Commissioner
Suzanne Chithea, former Vice Chairman +
Wendy Vallette, Commissioner +
David Blakesley, Commissioner +
Bruce Emerick, Commissioner +
Jeff King, former Chairman +
Herman Rempel, Commissioner +

*Former Staff Member

+ Former Council Member or Commissioner

GRUEN ASSOCIATES
Architecture/Planning/Engineering
6330 San Vicente Boulevard
Los Angeles, California 90048

Ki Suh Park, FAIA, AICP, Partner-in-Charge
Elaine V. Carbrey, AIA, AICP, Head of
Planning Department/ Project Manager
A. Jeffrey Skorneck, AIA, Assistant
Project Manager*
Marion Sampler, Head of Graphics Department*
Anne M. Kastl, Planner*
Margaret Makowski, Urban Designer*
Guillermo Aguilar, Urban Designer*
Masoud Sodaify, Planner*
Walker Wells, Senior Urban Planner
B.J. Krivanek, Graphic Designer*
Eve Meng, Graphic Designer
Michael A. DeChellis, Illustrator*
Nessa Marion, Technical Assistant*

MADOLE & ASSOCIATES, INC.
Civil Engineering
1820 East 16th Street
Santa Ana, California 92701

Milton Madole, President
William H. Humphrey, Office Manager
Gary Heely, Project Coordinator
Roger Manalastas, Hydrology and Hydraulics

EMMET L. WEMPLE & ASSOCIATES
Landscape Architects
2503 West Seventh Street
Los Angeles, California 90057

Denis L. Kurutz, Vice President*
William P. Wheatley, Landscape Architect

LAND CONCERN LIMITED
1750 East Deere Avenue
Santa Ana, California 92705

Robert Sawyer
Michael Sweeney

*Former Staff Member

F. The Affordable Housing provisions contained in Section VI, shall be revised as follows:

1. The language discussing the use of affordable programs and entitlements, shall be partially eliminated and amended.
 2. A statement shall be included that it is the goal to provide at least fifteen percent (15%) affordable housing in each of the four major neighborhoods of the Planned Community.
 3. The discussion on control of windfall profits shall be eliminated.
11. The above required revisions shall be submitted to the City Planner in draft form for review and approval within 30 days from the adoption by the City Council. The final adopted revised text shall be printed and submitted to the City Planner within 60 days from City Council approval.

APPROVED AND ADOPTED THIS 26TH DAY OF JANUARY, 1983.

PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA

BY: _____

Jeffrey King, Chairman

ATTEST: _____

Secretary of the Planning Commission

I, JACK LAM, Secretary of the Planning Commission of the City of Rancho Cucamonga, do hereby certify that the foregoing Resolution was duly and regularly introduced, passed, and adopted by the Planning Commission of the City of Rancho Cucamonga, at a regular meeting of the Planning Commission held on the 26th day of January, 1983, by the following vote-to-wit:

AYES:	COMMISSIONERS:	REMPER, STOUT, BARKER, MCNIEL, KING
NOES:	COMMISSIONERS:	NONE
ABSENT:	COMMISSIONERS:	NONE

ORDINANCE NO. 190

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, APPROVING PLANNED COMMUNITY ZONE NO. 81-01 FOR THE DEVELOPMENT OF TERRA VISTA PLANNED COMMUNITY GENERALLY LOCATED BETWEEN BASE LINE AND FOOTHILL BOULEVARD ON THE NORTH AND SOUTH AND BETWEEN ROCHESTER AND HAVEN ON THE EAST AND WEST

The City Council of the City of Rancho Cucamonga, California, does ordain as follows:

SECTION 1: The City Council hereby finds and determines the following:

- A. That the Planning Commission of the City of Rancho Cucamonga, following a public hearing held in the time and manner prescribed by law, recommends the rezoning of the property hereinafter described, and this City Council has held a public hearing in the time and manner prescribed by law as duly heard and considered said recommendation.
- B. That this rezoning is consistent with the General Plan of the City of Rancho Cucamonga.
- C. That the conditions recommended by Planning Commission Resolution No. 83-13 (Exhibit "A") and attached hereto as reference, shall be complied with as amended by City Council action as shown on Exhibit "B".
- D. Changes and alterations have been incorporated into the project which mitigates significant environmental effects to an acceptable level.
- E. The Planned Community provides for the development of a comprehensively planned urban community within the zone that is superior to development otherwise allowed under alternate regulations.
- F. The Planned Community provides for development within the zone in a manner consistent with the General Plan and with related development and growth management policies of the City.
- G. The Planned Community provides for construction, improvements, or extension of transportation facilities, public utilities, and public services required by development within the zone.

SECTION 2: The City Council hereby certifies the adequacy of the final Environmental Impact Report based upon the following findings:

- A. The final Environmental Impact Report has been prepared in accordance with the California Environmental Quality Act, the State, and local EIR guidelines.
- B. The Planning Commission has reviewed and considered the information contained within the EIR prior to recommending approval of the project.

SECTION 3: The City Council hereby adopts the following statement of overriding consideration:

To the extent that the Planned Community allows the occurrence of significant effects identified in the final EIR without full mitigation the City Council has identified specific economic, ecological, and social reasons to support its action which make infeasible the project alternatives described in the final EIR or additional mitigation measures. The City Council finds that facts supporting this finding are contained in the final EIR and the Planned Community text. Mitigation measures have been made a condition of approval of the Planned Community and are intended to mitigate or avoid the significant environmental effects identified in the final EIR. The Planned Community itself is a mitigation measure which is intended to mitigate or avoid the significant environmental effects of development which could otherwise occur without a planned comprehensive approach such as the Planned Community standards, guidelines and regulations.

SECTION 4: The following described real property is hereby rezoned to Planned Community Zone 81-01 and the zoning map is hereby amended accordingly. Further, the development of said property shall be regulated by the adopted Planned Community Text entitled "Terra Vista" and in part by the Rancho Cucamonga Zoning Ordinance.

PC 81-01 - Approximately 1321 acres and beginning at the northeast corner of Haven Avenue and Foothill Boulevard and traversing north to the Pacific Electric Railroad; thence east to the centerline of Milliken Avenue; thence south to the centerline of Base Line; thence east to the centerline of Rochester; thence south to the centerline of Foothill Boulevard; thence west to the beginning point of Haven Avenue and Foothill Boulevard.

SECTION 5: The Mayor shall sign this Ordinance and the City Clerk shall cause the same to be published within fifteen (15) days after its passage at least once in The Daily Report, a newspaper of general circulation published in the City of Ontario, California, and circulated in the City of Rancho Cucamonga, California.

PASSED, APPROVED, and ADOPTED this 16th day of February, 1983.

AYES: Dahl, Buquet, Schlosser, Frost, Mikels

NOES: None

ABSENT: None


Jon D. Mikels, Mayor

ATTEST:


Lauren M. Wasserman, City Clerk