

THE VICTORIA COMMUNITY PLAN

Submitted to the City of Rancho Cucamonga

June 1980

by

The William Lyon Company

Adopted by the

City of Rancho Cucamonga

May 20, 1981

Prepared By

The SWA Group

Laguna Beach, California

May 1980

COPY - 2-2-09
INSERTED

GOALS AND PURPOSES

This document, with its accompanying maps and diagrams is the Planned Community Ordinance for Victoria. This ordinance, hereafter called the Victoria Community Plan, was submitted to the City of Rancho Cucamonga by The William Lyon Company, the major landowner within the planning area boundaries. The SWA Group has assisted The William Lyon Company in its preparation.

The William Lyon Company has prepared this plan in recognition of the value to the community and themselves of having an overall plan. The alternatives to a planned community is piecemeal development which would not provide the many benefits that are possible when a community is viewed as a whole.

Although The William Lyon Company does not control all of the privately held land within the planning area, they believe the benefits of a total planned community are critical to the future of Rancho Cucamonga. This plan shows how all land within the planning area can be accommodated and enhanced by the Plan.

The Plan provides the City of Rancho Cucamonga with many significant features:

1. It will provide a variety of homes at prices commensurate with local incomes and local employment growth.
2. It will create an interrelated community with an attractive aesthetic urban design.
3. It will create commercial areas to provide services necessary to a balanced community.
4. Its regional center will generate considerable revenue for the City and will be an urban focus for western San Bernardino County.
5. It will encourage alternative methods of transportation and conserve energy and natural resources through creative urban planning and design.

The Plan is divided into three sections:

Part One is a detailed description of the Planning Area and the design concepts which will guide development within the Planned Community Boundaries.

Part Two contains design guidelines that specify in a detailed way the criteria that will be applied to the various kinds of development.

Part Three contains specific regulations and guidelines that will guarantee implementation of the Plan as it is described in Parts One and Two.

Approval of the Victoria Community Plan, the Land Use Plan, and the Environmental Impact Report will permit development to begin within the Planning Area.

TABLE OF CONTENTS

PART ONE: CONCEPTUAL PLAN

I	SUMMARY OF THE EXISTING CONDITIONS	5
	The Planning Area	
	The Rancho Cucamonga General Plan	
	The Victoria Community Plan in the Planning Process	
	Existing Site Conditions: Planning Areas 1, 2, 3 and 4	
	Existing Circulation	
	Existing Community Facilities	
	Existing Land Ownership	
II	THE VICTORIA COMMUNITY PLAN: OBJECTIVES & PLANNING CONCEPTS	23
	Planning Objectives	
	Planning Concepts	
III	THE VICTORIA COMMUNITY LAND USE PLAN	37
	The Victoria Community Land Use Plan	
	Regional Shopping Center	
	Variety of Residential Types	
	Joint Use of the Southern California Edison Corridors	
IV	THE VILLAGES OF VICTORIA	43
	THE VILLAGE OF VICTORIA GROVES	47
	Village Character	
	Village Edges	
	Joint Use of Deer Creek	
	A Potential Mass Transit Corridor	
	The Village Entry and Open Space	
	The Homes of Victoria Groves	
	A Village Commercial Center at Milliken and Highland	
	THE VILLAGE OF VICTORIA VINEYARDS	55
	Village Edges	
	The Village Entry and Park	
	Trails Connect Community to Schools and Open Space	
	Schools in the Vineyards	
	Variety of Residential Housing Opportunities	
	Village Commercial Center at Baseline and Milliken	

THE VILLAGE OF VICTORIA WINDROWS

63

The Village Edges
The Village Entry
Windrows Park
Edison Corridors: Mixed Use Open Space
Schools and Churches
Community Trail System for the Windrows
A New Arterial Road to Protect Etiwanda Character
Residential Housing
Special Land Use Preserves Etiwanda Avenue Character
Move-ons Permitted in Victoria Windrows
Village Commercial

THE VILLAGE OF VICTORIA LAKES

73

A Unique Water-Related Urban Community
The Village Edges
Village Entry
Victoria Lakes Link the Residential and
Commercial Community
Public Park at North Victoria Lake
Offices on Lake Edge
Trails Follow Lake System into Heart of the Regional Center
Urban Residential Land Use Typical in Victoria Lakes
Regional Center is Urban Focus for Victoria
A Special Entry to the Regional Center Area
Typical Regional Related Land Uses
Village Commercial Center Meets Local Needs
Community Facilities

V

SAFETY AND HEALTH
TRANSPORTATION WITHIN VICTORIA
COMMUNITY FACILITIES
CONSERVATION OF RESOURCES

87

SAFETY AND HEALTH

91

Storm Drainage
Sewerage
Water
Fire Protection

TRANSPORTATION WITHIN VICTORIA

93

Alternative Transportation Methods Emphasized
Local Trails and Streets
Local Residential Roads for Village Circulation
Victoria Parkway: A Road in a Park
Public Transport and Mass Transit
Special Boulevards to Protect Residential Neighborhoods
Access to the Regional Center

	COMMUNITY FACILITIES	97
	Parks and Open Space	
	Schools	
	Churches	
	CONSERVATION OF RESOURCES	99
	Use Water for Plant Materials Where the Community Benefits Most	
	A Lake System Can Save Water	
	Extensive Trail System Encourages Use of Bicycles and Pedestrians	
	Public Transport Encouraged in Victoria	
	Passive Solar Energy Can Save Energy	
	Architectural Design Criteria to Conserve Resources	
VI	STATISTICAL SUMMARY	105
	Plan Evolves Through Time	
	Summary of Land Use Acreage	
	Approach to Residential Density	
	The Optimum Yield Concept	
	A Realistic Approach to Residential Density	
	How the Optimum Yield Method Works	
	Optimum Yield by Land Use	
	Optimum Yield as a Planning Tool	
	PART TWO: DESIGN CRITERIA	
I	INTRODUCTION: How The Design Criteria Are To Be Used	119
II	VICTORIA PARKWAY	123
	Design Concepts: Overview	
	Planting Concept	
	Grading Concept	
	Right-of-Way Width Criteria	
	Typical Intersection Conditions	
	Typical Edge Condition Cross-Sections	
III	LANDSCAPE STANDARDS	149
	Introduction	
	Drought Tolerant Plants	
	Windrow Style Planting	
	Entry Hierarchy Plan	
	Plant Palette Plan	
	Typical Landscaped Edge Condition Cross-Sections	
	Landscape Grading Standards	
IV	SOLAR ACCESS STANDARDS	175

PART THREE: REGULATIONS

I	REGULATIONS AND STANDARDS FOR DEVELOPMENT	185
	Affordable Housing	
	Project Phasing	
	Community Facilities and Open Space: Construction and Maintenance	
	Distribution of Residential Dwelling Units	
	Flexibility in Managing the Plan	
	Legal Description of the Planned Community	
	Residential Development Standards	
	Commercial Standards	
	Community Facilities	
	Road Standards	
II	IMPLEMENTATION	
	Development Approval Process	251
APPENDIX		
A.	DEFINITIONS	255

note: a large fold-out version of the Community Plan is attached to the inside of the back cover. This Plan can be opened and referred to as the plan is read.



ORDINANCE NO. 287

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, ADOPTING VICTORIA PLANNED COMMUNITY AMENDMENT 85-01, TO MODIFY THE COMMUNITY PLAN TEXT TO ALLOW A RECREATIONAL VEHICLE STORAGE LOT WITH MINI-WAREHOUSE IN THE MEDIUM-HIGH (MH) OR HIGH (H) LAND USE CATEGORY, AND TO ALLOW ITS OPERATION ON A COMMERCIAL, FOR RENT BASIS, OPEN TO THE GENERAL PUBLIC.

The City Council of the City of Rancho Cucamonga, California, does ordain as follows:

SECTION 1: The City Council hereby finds and determines the following:

- A. That the Planning Commission of the City of Rancho Cucamonga, following a public hearing held in the time and manner prescribed by law, recommends the Community Plan text amendment hereinafter described, and this City Council has held a public hearing in the time and manner prescribed by law and duly heard and considered said recommendation.
- B. That this Community Plan text amendment is consistent with the General Plan of the City of Rancho Cucamonga.
- C. That this Community Plan text amendment is consistent with the original intent of the Victoria Planned Community.
- D. This Community Plan text amendment will have no significant environmental impact as provided in the Negative Declaration filed herein.

SECTION 2: The Community Plan text of the Victoria Planned Community is hereby amended to read as follows:

- A. Condition #5 of "General" of Resolution No. 81-37 to read as follows:

"Recreational Vehicle storage shall be provided within each residential land use category of Low, Low-Medium, and Medium for 25% of the lots or units contained within those lots within the Victoria Planned Community. RV storage shall be provided within the boundaries of the Victoria Planned Community. The percent of recreational vehicle storage spaces shall be revised on an annual basis and may be modified by the Planning Commission."

- B. Condition #6 of "General" of Resolution No. 81-37 to read as follows:

"Recreational Vehicle storage" shall not be permitted in the Medium-High or High land use category unless within a designated RV storage lot, and shall be restricted by CC & R's subject to the review and approval of the City Attorney. Recreational vehicle storage lots may be permitted within the boundaries of the Victoria Planned Community subject to the granting of a Conditional use Permit by the Planning Commission. Prior to approving a CUP for a RV storage lot, the Planning Commission shall make the following findings:

1. That the size and configuration of the proposed RV storage lot is adequate for the planned development.
2. That the location of the proposed facility is adjacent to a primary circulation route to permit reasonable access to the facility without resulting in negative impacts for surrounding properties.
3. That the proposed RV storage lot is compatible with existing surrounding uses, or with those further uses permitted in the land use designations.
4. That the proposed RV storage lot would not be detrimental to and result in significant negative impacts for surrounding properties, property owners, and/or residents.
5. That the total number of RV spaces provided, whether as spaces on an individual lot basis or within a RV storage lot, does not exceed more than 25% of the total number of dwelling units in the Low, Low-Medium, and Medium land use categories.

Mini-warehousing may be permitted when in conjunction with an RV storage lot developed in conformance to the above referenced requirements.

1. A mini-warehouse shall be defined as a structure, or group of structures, providing enclosed and locked compartments for the dead storage of customer goods and wares where individual lockers or stalls are rented out to different tenants for storage. As with the primary RV storage lot, a mini-warehouse facility shall be designed to primarily cater to the needs of the residents of the Victoria Planned Community.
2. RV storage lots when developed with accessory mini-warehouse shall conform to the following development standards:
 - a. The minimum area of a lot for a mini-warehouse facility shall be 2 acres.
 - b. The maximum building height shall be 2 stories or 25 feet, except that any building or portion of a building within 25 feet of the front or street side setback shall have a maximum height of one-story or 10 feet.
 - c. Only one point of ingress and egress shall be allowed, unless another is required by the Fire Department. All driveways shall be radius type, and shall have an unobstructed length of 40 feet from the curb to prevent vehicles and trailers from interfering with traffic flow in the street.

ORDINANCE NO. 143

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, APPROVING VICTORIA PLANNED COMMUNITY P.C. 80-01 LOCATED SOUTH OF HIGHLAND AVENUE GENERALLY NORTH OF FOOTHILL BOULEVARD AND GENERALLY WEST OF ETTIWANDA AVENUE.

WHEREAS, the Planning Commission has held various public hearings on the Victoria Planned Community beginning on September 16, 1980 and culminating with April 2, 1981; and

WHEREAS, the City Council has held a duly advertised public hearing on the Victoria Planned Community; and

WHEREAS, the City Council and Planning Commission have reviewed the land use, circulation, parks and open space, infrastructure, design criteria, regulations, implementation sections of the Planned Community text for Victoria; and

WHEREAS, the City Council and Planning Commission have reviewed the land use plan map for Victoria and required subsequent changes to that map as a result of that review; and

WHEREAS, the Planning Division Staff is directed to amend the Official Zoning map for the City of Rancho Cucamonga to indicate the subject property as "PC"; and

WHEREAS, the Planning Commission has reviewed the Draft Environment Impact Report on the Victoria Planned Community relative to its impacts and recommends certification to the City Council.

WHEREAS, the City Council does hereby certify the Draft EIR as complete and final statement of the environmental effects of the proposed project.

NOW, THEREFORE, BE IT RESOLVED, that the City of Rancho Cucamonga, City Council does hereby adopt P.C. 80-01, Victoria Planned Community, subject to the conditions contained within Planning Commission Resolution No. 81-37 adopted April 2, 1981 attached hereto as reference.

The Mayor shall sign this Ordinance and the City Clerk shall cause the same to be published within fifteen (15) days after its passage at least once in The Daily Report, a newspaper of general circulation published in the City of Ontario, California, and circulated in the City of Rancho Cucamonga, California.

PASSED, APPROVED, and ADOPTED this 20th day of May, 1981.


AYES: Frost, Mikels, Palombo, Bridge, Schlosser

NOES: None

ABSENT: None


Phillip D. Schlosser, Mayor

ATTEST:


Lauren M. Wasserman, City Clerk

RESOLUTION NO. 81-37

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA RECOMMENDING TO THE CITY COUNCIL APPROVAL OF VICTORIA PLANNED COMMUNITY P.C. 80-01 TO BE LOCATED SOUTH OF HIGHLAND AVENUE GENERALLY NORTH OF FOOTHILL BOULEVARD AND GENERALLY WEST OF ETIWANDA AVENUE

WHEREAS, the Planning Commission has held various public hearings on the Victoria Planned Community beginning on September 16, 1980 and culminating with April 2, 1981; and

WHEREAS, the Planning Commission has reviewed the land use, circulation, parks and open space, infrastructure, design criteria, regulations, implementation sections of the Planned Community text for Victoria; and

WHEREAS, the Planning Commission has reviewed the land use plan map for Victoria and required subsequent changes to that map as a result of that review; and

WHEREAS, duly advertised public hearings were held relative to the Planning Commission review of the Victoria Planned Community No. 80-01; and

WHEREAS, The Planning Commission has reviewed the Draft Environmental Impact Report on the Victoria Planned Community relative to its impacts and recommends certification to the City Council.

NOW, THEREFORE, BE IT RESOLVED, that the City of Rancho Cucamonga, Planning Commission does hereby recommend to the City Council of the City of Rancho Cucamonga approval of Victoria Planned Community No. 80-01, subject to the following conditions:

GENERAL

1. The maximum number of dwelling units permitted within the boundaries of the Victoria Planned Community shall not exceed 8255 dwelling units except as allowed in Land 7.
2. All reference to Planned Community Area shall be eliminated to the satisfaction of the City Planner throughout the text of the Victoria Planned Community.

3. The Planned Community text shall be revised to incorporate all conditions of approval where appropriate and to provide revisions in organization of the text from page 171 to the end of the text to the satisfaction of the City Planner.
4. Definitions shall be added or deleted to the appendix A to coincide with the additions and/or deletions that have occurred as a result of the revisions in the regulations section to the satisfaction of the City Planner.
5. Recreation vehicle storage shall be provided within each residential land use category of low, low-medium, medium, for 25% of the lots or units contained within those units within the Victoria Planned Community. RV storage shall be provided on the site of each development or within the boundaries of the Planned Community as a private recreation vehicle storage lot. The percent shall be revised on an annual basis and may be modified by the Planning Commission.
6. Recreation vehicle storage shall not be permitted in the medium-high or high land use category and shall be restricted by CC&Rs, subject to review and approval of the City Attorney.
7. Affordable housing shall be provided in accordance with the revised regulations text, page 225.
8. All tentative tracts shall be reviewed by the Design Review Committee prior to consideration by the Planning Commission and shall comply with Ordinance 86 - Growth Management.
9. Forty-five days after City Council consideration for approval of the Victoria Planned Community, a revised text incorporating all of the changes and organizational changes necessary shall be submitted to the City Planner for review and comment prior to a final draft text being reprinted for redistribution to the City Council, the City Clerk, the Planning Commission and Staff.

AMENDED BY ORDINANCE 287

AMENDED BY ORDINANCE 287

10. On page 248, local residential streets show a minimum 10-foot setback, an additional 15-foot typical setback shall be added.
11. Page 250, section 4B4 shall be the same as section 5B3a and b on page 251, i.e. instead of a building separation of a 10-foot minimum, building separation would be (a) 35 feet in height or less, 10-foot minimum, or (b) buildings greater than 35 feet, 15-foot minimum.
12. Page 253, the word provision should be added after No. 1 General. On No. 2, the minimum lot width shall be 150 feet. No. 3, the maximum height shall be indicated at 65 feet with a conditional use permit for any heights greater than 65 feet.
13. Page 234, No. 1 after General, add provision.
14. P 233, No. 3 & 4 should be revised to read:

In order to provide flexibility in the development of the Victoria Planned Community, over the 10-15 year life of the Plan, a maximum of 20% variation in the optimum yield may be allowable.

Any and all variations shall be reviewed and approved by the Design Review Committee.

The variation in the number of dwelling units within one village may require corresponding decreases in one or more of the other villages to insure that the total number of units do not exceed 8255 within the Planned Community boundary.

LAND USE

1. The development process and regional demand for the Regional Related area shall be periodically reviewed to determine the continued viability of the amount of acreage proposed. This review shall not be more than three years apart and begin after the opening of the Regional Center.

2. Prior to consideration of specific designs for the Regional Center a focused E.I.R. shall be prepared on the issuance of traffic, aesthetics and long and short term impacts of the proposed project.
3. The Village Commercial Shopping Center proposed at the northeast corner of Milliken and Base Line Road shall be eliminated and replaced with medium-high Land Use.
4. Prior to the approval of any tract within any of the villages containing proposed school facilities the School District involved must consent in writing to the need and location for school facility. Additionally, the sites reserved for schools shall be sufficient in size for the needs of the school district. This does not preclude meeting any requirements of the currently adopted Growth Management Ordinance.

CIRCULATION

1. Rochester Avenue shall be continued from Base Line to Highland.
2. Highland Avenue shall be constructed at a 64-foot curb to curb section to accommodate 5 lanes of traffic, specific alignment, and design shall be to the satisfaction of the City Engineer.
3. At the time of development at the intersections of Highland Avenue and Milliken Avenue, and at the intersection of Highland Avenue and Day Creek Boulevard plans shall be prepared by the applicant coordinating the frontage road design to the satisfaction of the City Engineer and Caltrans.
4. Should any of the streets that cross the Pacific Electric rail tracks (i.e. Milliken, Rochester, Day Creek) require grade separation, the applicant shall be required to prepare preliminary design studies to determine configuration and right-of-way requirements to the satisfaction of the City Engineer.

5. The applicant shall prepare a detailed traffic analysis detailing the circulation elements such as street location, freeway interchange, street size, etc., for the regional shopping center and the regional related areas. Such analysis shall be prepared to the satisfaction of the City Engineer and may be included in the Focused EIR for the Regional Center.
6. Precise alignment studies and rights-of-way limits will be required to be prepared by the applicant, including but not limited to, Victoria Parkway, local collectors, etc., to be concurrent with with the consideration of approval of any tentative tract maps involving significant alignment features of any unprecised street to the satisfaction of the City Engineer.
7. Street standards shall conform to those of the City of Rancho Cucamonga or as may be amended to the satisfaction of the City Engineer (Victoria Parkway shall be exempt from standard street requirements subject to the approval of the City Planner and City Engineer.

PARKS/OPEN SPACE

1. The lakes are approved in concept only. Further detailed studies shall be submitted to the City prior to any tentative tract approvals for a village that contains a lake. The detailed studies shall indicate the construction techniques of the lake, the details of design, the details of maintenance of the lake, and cost estimates for maintenance.
2. The City shall approve the selection of a lake consultant to be hired by and paid for by the applicant for the lakes proposed in Victoria Parkway and the two proposed public lakes, exclusive of the lakes contained within the regional area.
3. Park credit for paseos, Village Parks, Victoria Parkways and the two northwest Lakes, excluding the Lake in the regional center, shall be credited 100% towards park in lieu fees at the time of building permit issuance.

4. Each park area within each village shall be dedicated to the City in a complete form including, but not limited to, installed parking areas, seeded play areas, irrigation and restrooms prior to the construction of greater than 200 dwelling units of the lots within the village. Park design shall be to the satisfaction of the Director of Community Services and consistent with the Victoria Planned Communities and reviewed and approved by the Planning Commission.
5. Concession stands shall be studied to determine the economic feasibility; the economic feasibility study shall be prepared by the applicant or their consultants and submitted to and approved by the Department of Community Services and Planning Division prior to the construction of any concession stands being located on or about the lake edge.
6. The equestrian trail continued within Victoria Parkway shall begin at Deer Creek and terminate at the Pacific Electric right-of-way. Specific design of the trail shall be subject to City review. Equestrian fences, grading, soil samples, may be required.
7. Maintenance of dedicated areas, i.e. village parks, Victoria Parkway and the two northwest lakes, excluding the lake in the regional center, shall be by city-wide maintenance district.

INFRASTRUCTURE

1. The developer shall participate through financial contributions and/or construction of storm drain facilities within the Day Creek and that drainage area in accordance with a to-be-adopted master plan for Day Creek. Development will be allowed to take place within the drainage area so long as flood protection is provided by the developer to the satisfaction of the City Engineer and/or the San Bernardino Flood Control District.
2. Prior to any substantial development in any of the four villages, a solution for sewage treatment shall be selected by the City and/or the Cucamonga County Water District.
3. Adequate fire protection through either expansion of Fire Station No. 3 or the addition of another fire station or the relocation of the existing fire station shall be provided to the satisfaction of the Foothill Fire District.

RESIDENTIAL SITE PLANNING

1. Page 184, add the word minimum in the setbacks paragraph under "garage".
2. Page 185, the paragraph setbacks under side yard, add the words "each side". The new phrase would read "side yard: 5-foot minimum each side."
3. Page 186, directly under the graphic, the words variable height fence by developer on Victoria Parkway property line should indicate that fence height shall not exceed 5 feet from the highest finished grade.
4. Under setbacks garage, add the words "requires garage door openers" after 5-8 feet and add the word "minimum" after 18 feet.
5. Under side yard indicate 5-foot for each side yard rather than 10 feet between buildings.
6. On page 187 indicate in parentheses under the graphic after the phrase developer to provide fencing on corner lots and adjacent to community trail system "(see page 189)".
7. Under setbacks, garage, add the words "requires garage door openers" after 5 to 8 feet. Also, indicate what width sidewalks and location of said sidewalks.
8. Page 188, indication of minimum parking requirements may require a change in the Rancho Cucamonga Parking Ordinance. Should the ordinance not change, the indication of minimum parking standard shall be amended to be consistent with current code requirements. Also there is no indication of the kinds of sidewalks or location of sidewalks.
9. Page 188 under setbacks garage, add "requires garage door openers" after 5 to 8 feet.

10. Page 189, the second note from the top of the page add the words "5-foot maximum" so the phrase will read, 5-foot maximum back yard fence adjacent to trail by developer.
11. Page 190 under garage setback, add "requires garage door openers" after 5 to 8 feet and add the words "or sidewalk" after curb so the phrase will read garage setback: 5 to 8 feet (requires garage door openers) from back of curb or sidewalk. The indication under garage setback of the parking requirement is in excess of what current code requirements are and shall indicate code requirements.
12. Page 193 under the graphic indicates minimum of one covered parking space per dwelling, additional spaces as specified in Part III. This requirement is different from the existing parking code and shall be consistent code requirements.
13. Under the graphic there is an indication where sound attenuation walls are required they may not be needed to be continuous but may work satisfactorily when broken or staggered. The words "however, in all cases these must be consistent with the adopted Noise Ordinance" shall be added.
14. Page 196 under the first graphic, the words "automatic garage door openers required" shall be added after 5 to 8 feet.

APPROVED AND ADOPTED THIS 2 DAY OF APRIL 1981.

PLANNING COMMISSION OF THE CITY OF RANCHO CUCAMONGA

BY: Richard M. Danl
Richard Danl, Chairman

ATTEST: Paul Hill
Secretary of the Planning Commission

Resolution No. 81-37

Page 9

I, JACK LAM, Secretary of the Planning Commission of the City of Rancho Cucamonga, do hereby certify that the foregoing Resolution was duly and regularly introduced, passed, and adopted by the Planning Commission of the City of Rancho Cucamonga, at a regular meeting of the Planning Commission held on the 2 day of April, 1981 by the following vote to-wit:

AYES: COMMISSIONERS: Rempel, Tolstoy, Dahl, King, Sceranka

NOES: COMMISSIONERS: None

ABSENT: COMMISSIONERS: None

ORDINANCE NO. 467

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RANCHO CUCAMONGA, CALIFORNIA, APPROVING VICTORIA PLANNED COMMUNITY AMENDMENT 91-02, AMENDING VARIOUS DEVELOPMENT STANDARDS AND DESIGN GUIDELINES FOR MULTI-FAMILY RESIDENTIAL DISTRICTS, AND MAKING FINDINGS IN SUPPORT THEREOF

A. Recitals.

(i) On April 24 and continued to May 8, June 12, July 10, August 14, August 22, and September 11, 1991, the Planning Commission of the City of Rancho Cucamonga conducted duly noticed public hearings with respect to the above-referenced Victoria Planned Community Amendment. Following the conclusion of said public hearing on September 11, 1991, the Planning Commission adopted Resolution No. 91-136, thereby recommending that the City Council adopt Victoria Planned Community Amendment No. 91-02.

(ii) On September 11, 1991, the City Council of the City of Rancho Cucamonga conducted a duly noticed public hearing and concluded said hearing prior to its adoption of this Ordinance.

(iii) All legal prerequisites prior to the adoption of this Ordinance have occurred.

B. Ordinance.

NOW, THEREFORE, the City Council of the City of Rancho Cucamonga does hereby ordain as follows:

SECTION 1: This Council hereby specifies and finds that all of the facts set forth in the Recitals, Part A, of the Ordinance are true and correct.

SECTION 2: This Council hereby finds and certifies that the project has been reviewed and considered in compliance with the California Environmental Quality Act of 1970, and further, this Council hereby issues a Negative Declaration.

SECTION 3: The Rancho Cucamonga City Council finds as follows:

- a) That the Planning Commission of the City of Rancho Cucamonga, following a public hearing held in the time and manner prescribed by law, recommended approval of the Community Plan text amendment hereinafter described to the City Council. This City Council has held a public hearing in the time and manner prescribed by law and duly heard and considered said recommendation.
- b) That this Community Plan text amendment is consistent with the General Plan of the City of Rancho Cucamonga.
- c) That this Community Plan text amendment is consistent with the Development Code of the City of Rancho Cucamonga.

- d) That this Community Plan text amendment will have no significant environmental impact as provided in the Negative Declaration filed herein.

SECTION 4: The City Council of the City of Rancho Cucamonga hereby approves Victoria Planned Community Amendment 91-02 as follows:

- a) The Victoria Community Plan text, Part III, Section I, Regulations and Standards for Development, subsection "Residential Design Guidelines Medium Density Residential" commencing on page 224, is hereby amended, in part, to read as attached hereto and incorporated herein by this reference.
- b) The Victoria Community Plan text, Part III, Section I, Regulations and Standards for Development, subsection "Cluster and Innovative Housing Setbacks," commencing on page 225, is hereby amended in part, to read as attached hereto and incorporated herein by this reference.
- c) The Victoria Community Plan text, Part III, Section I, Regulations and Standards for Development, subsection "Residential Development Standards Medium High Density Residential," commencing on page 229, is hereby amended, in part, to read as attached hereto and incorporated herein by this reference.
- d) The Victoria Community Plan text, Part III, Section I, Regulations and Standards for Development Standards, subsection "Cluster Housing Setbacks," commencing on page 231 is hereby amended to read as attached hereto and incorporated herein by this reference.
- e) The Victoria Community Plan text, Part III, Section I, Regulations and Standards for Development, subsection "Residential Development Standards High Density Residential," commencing on page 232, is hereby amended, in part, to read as attached hereto and incorporated herein by this reference.
- f) The Victoria Community Plan text, Part III, Section I, Regulations and Standards for Development, subsection "Cluster Housing Setbacks," commencing on page 233 is hereby amended to read as attached hereto and incorporated herein by reference.

SECTION 5: The Mayor shall sign this Ordinance and the City Clerk shall cause the same to be published within fifteen (15) days after its passage at least once in the Inland Valley Daily Bulletin, a newspaper of general circulation published in the City of Ontario, California, and circulated in the City of Rancho Cucamonga, California.

Ord. 407 adopted 1991

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

4. Medium Density Residential (" M " Land Use Plan Designation) :

Land designated as Medium Density Residential is intended for residential development that ranges from 8 to 14 dwelling units per adjusted gross acre. The following regulations are applicable for these areas:

- a Use Permitted: detached or attached residential dwellings not exceeding fourteen dwellings per adjusted gross acre, including, but not limited to :
 - 1. Single family dwellings - attached or detached, including, but not limited to townhouses, triplexes, fourplexes, and condominiums.
 - 2. Cluster housing.
 - 3. Community facilities, page 241
- b Site Development Standards :
 - 1. Cluster housing.
 - (a) Building site area : 3 acres minimum.
 - (b) Building site coverage : As permitted by required setbacks and private open space
 - (c) Building setbacks : See building setback diagrams on the following page for typical setbacks.
 - (d) Building separation : The standards from the Rancho Cucamonga Development Code shall apply.
 - (e) Building height : 40 feet maximum.
 - (f) Building site width and depth : As permitted by required setbacks.
 - (g) Transition of density : The site plan should consider compatibility with surrounding neighborhood through providing proper transition of density, particularly on infill sites adjacent to lower densities. Comparable densities, open space buffer zones, increased setbacks and architectural compatibility are encouraged along common boundaries to provide proper transition of density. Clustering of units can provide large open space areas as a buffer.
 - (h) All cluster housing development and multi-family development within the Planned Community area must comply with the Design Guidelines as outlined in the Residential Section of the Rancho Cucamonga Development Code.

Ord. 407

2. Innovative single family housing.

- (a) Building site area : 3,500 sq. ft.. minimum, 4,000 sq. ft. average.
- (b) Building site coverage : As permitted by required setbacks and private open space.
- (c) Building setbacks : See building setback diagrams on the following page for typical setbacks.
- (d) Building separation : See Building setback diagrams on the following page for typical setbacks.
- (e) Building height : 35 feet maximum.
- (f) Building site width and depth : As permitted by required setbacks.
- (g) Private open space : 300 sq. ft. minimum.

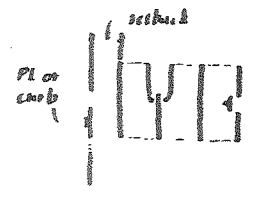
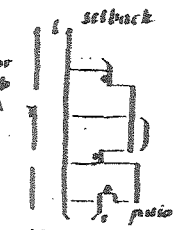
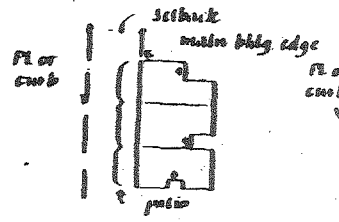
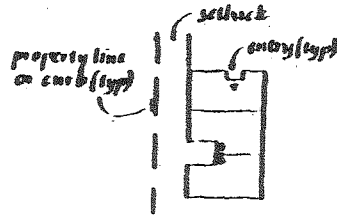
The above site development standards apply to projects which are deemed innovative. Innovation in single family development means providing creative design solutions which address the critical concerns of neighborhood compatibility, density transition, and design quality. Innovative projects are characterized by an attractive streetscape which is not monotonous, nor is the street scene dominated by asphalt/concrete, garages, and cars. Innovative design means finding creative ways to create well-designed space, particularly usable yard space.

OSTER HOUSING SETBACKS:
standards for - M, MH & H
residential land uses

FRONTYARD

REARYARD

SIDEYARD



RESIDENTIAL OR COMMERCIAL ①, ②

with fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

without fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

OPEN SPACE ③

except Victoria Parkway

10 feet min

patio fence must abut property line

5 feet min

5 feet min

NEIGHBORHOOD STREET ③, ④

with/without facing garage access
(see Typical Garage Setbacks)

22
10 feet min

27
15 feet minimum

22
10 feet min

22
5 feet min to back of curb or walk

LOCAL RESIDENTIAL STREETS ③, ④

without facing garage access
(see Typical Garage Setbacks)

22
10 feet min

27
15 feet min

27
15 feet min

22
10 feet min

ARTERIAL ROADS ③, ④

with fence or wall on property line

38
25 feet min, 20 feet typ

25 feet min

25 feet min

25 feet min

without fence or wall on property line

38
20 feet min

25 feet min

25 feet min

25 feet min

VICTORIA PARKWAY

see Typical Edge Condition Cross Sections

also see Typical Inmate Setbacks

note drawings not to scale

NOTES:

- 1 Add 10 feet if adjacent to VI or L Districts.
- 2 Measured from property line.
- 3 Measured from ultimate curb.
- 4 Building setbacks shall be measured from the exterior of the wall including stairwells, patios and other architectural elements or features except for those listed in Section 17 08.060 C1 and C2 of the Rancho Cucamonga Development Code. Landing places which do not exceed a height of 36 inches above grade may encroach into the front and corner side yards a maximum of 6 feet.

103

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

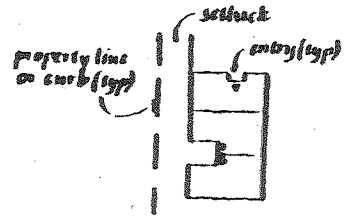
5. Medium High Density Residential (" MH" Land Use Plan Designation) :

Land designated as Medium High Density Residential housing is intended for residential development that ranges from 14 to 24 dwelling units per adjusted gross acre. The following regulations are applicable for these areas:

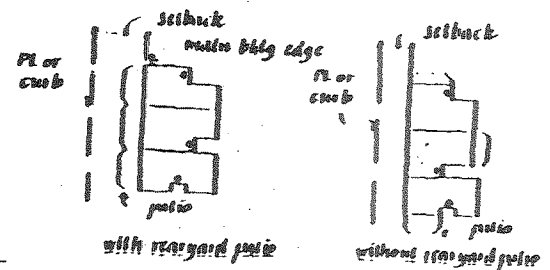
- a. Use Permitted:
 1. Multiple-family dwellings , including, but not limited to , apartment projects, condominium projects, and cooperative apartment projects.
 2. Accessory buildings, structures and uses where related and incidental to a permitted use.
 3. Community facilities, page 241
- b. Site Development Standards :
 1. Building site area : 3 acres minimum.
 2. Building setbacks : See building setback diagrams on the following page for typical setbacks.
 3. Building separation : The standards from the Rancho Cucamonga Development Code shall apply.
 4. Building height : 40 feet maximum.
- c. Transition of density : The site plan should consider compatibility with surrounding neighborhood through providing proper transition of density , particularly on infill sites adjacent to lower densities . Comparable densities, open space buffer zones , increased setbacks, and architectural compatibility are encouraged along common boundaries to provide proper transition of density . Clustering of units can provide large open space areas as a buffer.
- d. All cluster housing development and multi-family development within the Planned Community area must comply with the Design Guidelines as outlined in the Residential Section of the Rancho Cucamonga Development Code.

MINIMUM HOUSING SETBACKS:
 standards for - M, MH & H
 residential land uses

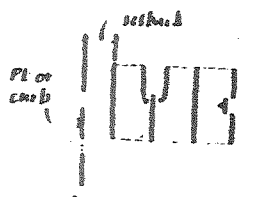
FRONTYARD



REARYARD



SIDEYARD



RESIDENTIAL OR COMMERCIAL ①, ②

with fence or wall on property line
 without fence or wall on property line

10 feet min, 15 feet typ
 10 feet min, 15 feet typ

15 feet min
 15 feet min

10 feet min
 10 feet min

5 feet min
 5 feet min

OPEN SPACE ③

except Victoria Parkway

10 feet min

patio fence only
 abt. property line

5 feet min

5 feet min

NEIGHBORHOOD STREET ③, ④

without facing garage access
 (see Typical Garage Setbacks)

22
 10 feet min

27
 15 feet minimum

22
 10 feet min

22
 5 feet min to
 back of curb or wall

LOCAL RESIDENTIAL STREETS ③, ④

without facing garage access
 (see Typical Garage Setbacks)

22
 10 feet min

27
 15 feet min

27
 10 feet min

22
 10 feet min

ARTERIAL ROADS ③, ④

with fence or wall on property line
 without fence or wall on property line

36
 15 feet min, 20 feet typ
 36 20 feet min

36
 25 feet min
 36 25 feet min

36
 25 feet min
 36 25 feet min

36
 25 feet min
 36 25 feet min

VICTORIA PARKWAY

see Typical Edge Condition Cross Sections

note: drawings not to scale

also see Typical Garage Setbacks

NOTES:

- 1 Add 10 feet if adjacent to VL or L Districts.
- 2 Measured from property line.
- 3 Measured from ultimate curb
- 4 Building setbacks shall be measured from the exterior of the wall including stairwells, patios and other architectural elements or features except for those listed in Section 17 08 060 C1 and C2 of the Rancho Cucamonga Development Code. Landscaping which do not exceed a height of 36 inches above grade may project into the front and corner side yards a maximum of 6 feet.

105

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

6. High Density Residential (" H" Land Use Plan Designation) :

Land designated as High Density Residential is intended for residential development that ranges from 24 to 30 dwelling units per adjusted gross acre. The following regulations are applicable for these areas:

- a Use Permitted:
1. Multiple-family dwellings , including, but not limited to , apartment projects, condominium projects, and cooperative apartment projects.
 2. Accessory buildings, structures and uses where related and incidental to a permitted use.
 3. Community facilities, page 241
- b Site Development Standards :
1. Building site area : 3 acres minimum.
 2. Building site coverage : 60 %
 3. Building setbacks : See building setback diagrams on the following page for typical setbacks.
 4. Building separation : The standards from the Rancho Cucamonga Development Code shall apply.
 5. Building height : 50 feet maximum.
- c. Transition of density : The site plan should consider compatibility with surrounding neighborhood through providing proper transition of density , particularly on infill sites adjacent to lower densities . Comparable densities, open space buffer zones , increased setbacks and architectural compatibility are encouraged along common boundaries to provide proper transition of density . Clustering of units can provide large open space areas as a buffer.
- d. All cluster housing development and multi-family development within the Planned Community area must comply with the Design Guidelines as outlined in the Residential Section of the Rancho Cucamonga Development Code.

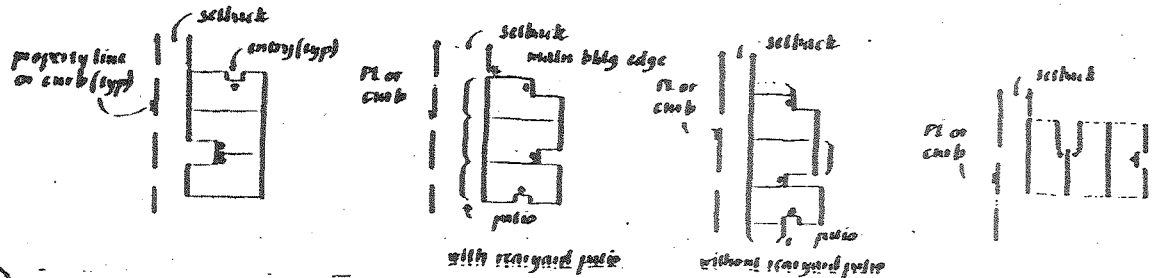
New

STER HOUSING SETBACKS:
standards for M, MII & H
residential land uses

FRONTYARD

REARYARD

SIDEYARD



RESIDENTIAL OR COMMERCIAL ①, ②

with fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

without fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

OPEN SPACE ②

except Victoria Parkway

10 feet min

patio fence may abut property line

5 feet min

5 feet min

NEIGHBORHOOD STREET ③, ④

without facing garage access
(see Typical Garage Setbacks)

27
10 feet min

27
15 feet minimum

22
10 feet min

22
5 feet min to back of curb or walk

LOCAL RESIDENTIAL STREETS ③, ④

without facing garage access
(see Typical Garage Setbacks)

22
10 feet min

22
10 feet min

27
10 feet min

22
10 feet min

ARTERIAL ROADS ③, ④

with fence or wall on property line

38
10 feet min, 10 feet typ

38
25 feet min

38
25 feet min

38
25 feet min

without fence or wall on property line

38
20 feet min

38
25 feet min

38
25 feet min

38
25 feet min

VICTORIA PARKWAY

see Typical Edge Condition Cross Sections

note dimensions not to scale

also see Typical Garage Setbacks

NOTES:

1. Add 10 feet if adjacent to VL or L Districts.
2. Measured from property line.
3. Measured from ultimate curb.
4. Building setbacks shall be measured from the exterior of the wall including stairwells, patios and other architectural elements or features except for those listed in Section 17.08.060 C1 and C2 of the Rancho Cucamonga Development Code. Landing places which do not exceed a height of 36 inches above grade may project into the front and corner side yards a maximum of 6 feet.

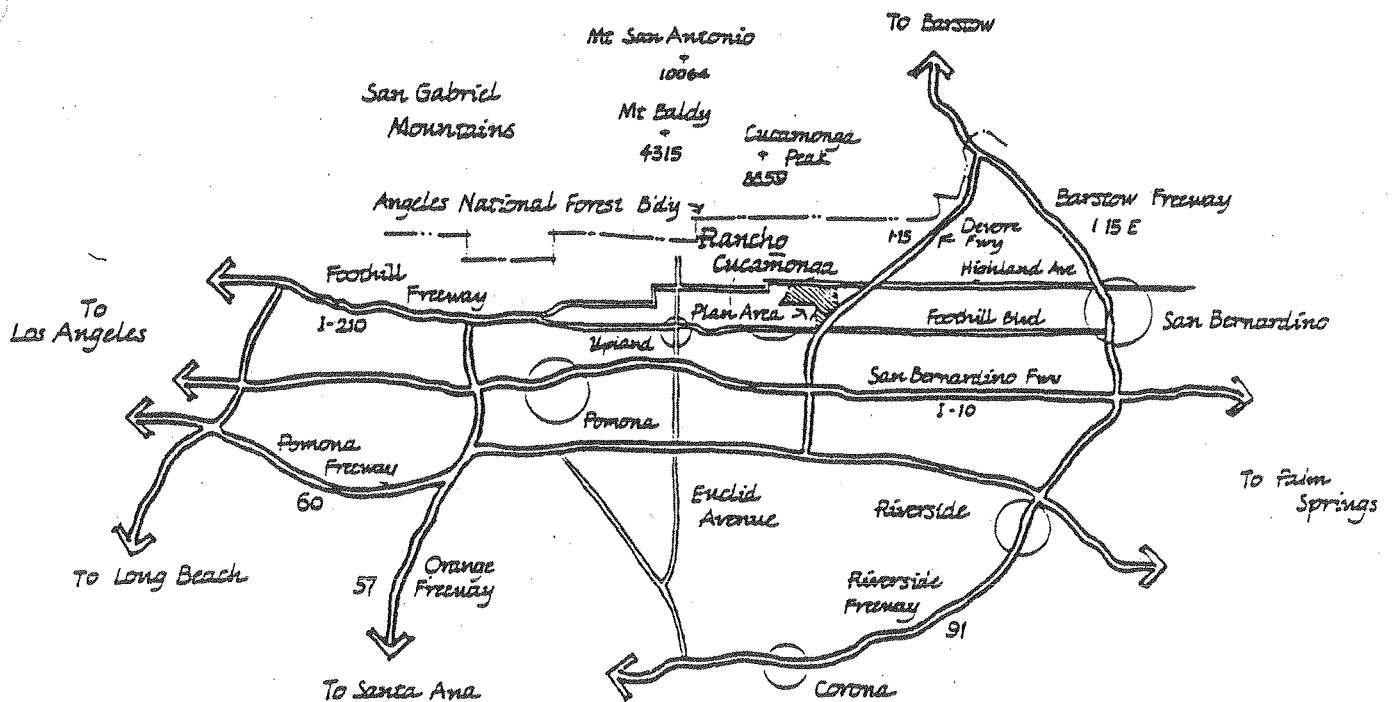
107

PART I

I
***SUMMARY OF EXISTING
CONDITIONS***

THE PLANNING AREA

The map shows the location of the planning area. It is bounded on the north by Highland Avenue, the east by Etiwanda Avenue, and the south and west by, respectively, the Devore Freeway (I-15), Day Creek, Baseline, Milliken, the Southern Pacific railroad tracks, and Deer Creek. Of the approximately 2,150 acres in the planning area, The William Lyon Company controls approximately 1,390 acres, or roughly 70% of the privately owned property. The other large land holders include Southern California Edison and San Bernardino County Flood Control (who together own 260 acres, or 12%). Other privately owned parcels in the planning area vary in size and use from single family homes on small lots, to a lumberyard, a distillery and vacant land. The Victoria Community Plan shows how all the other land can be integrated into one total community to everyone's benefit.



Vicinity Map



THE RANCHO CUCAMONGA GENERAL PLAN

The General Plan is more than a map of proposed land uses. It is a series of policies which systematically guide the total needs and desires of the community. It also translates these policies into a plan of action to create a desirable place in which to live, work and play. As part of the planning process, it is continually reviewed and refined as time progresses and development occurs.

As legally required of a newly incorporated city, Rancho Cucamonga is now in the process of completing its General Plan. The Victoria Community Plan is consistent with the goals and objectives of the General Plan, many of which served as a basis for developing the Victoria Community Plan. Briefly stated, some of these goals are:

1. Strengthen community identity and preserve the heritage of the original Tri-Communities.
2. Create open space that defines urban areas, that encourages leisure time use, and provides visual relief.
3. Improve the visual quality of Rancho Cucamonga.
4. Attract a major regional commercial center.
5. Improve the choice in residential housing.

In preparing the General Plan, approved in May of 1981, the City of Rancho Cucamonga incorporated many of the ideas from the Victoria Community Plan into their planning for the part of the City contained within the Planning Area.

THE VICTORIA COMMUNITY PLAN IN THE PLANNING PROCESS

The Victoria Community Plan is the planning link between the City's General Plan and the tract maps submitted for city review prior to applying for construction permits. The Community Plan is a method of zoning which establishes standards, guidelines and design criteria for the development of the Planned Community. It also shows how the plan can be implemented and what procedures will be followed in reviewing each step of implementation.

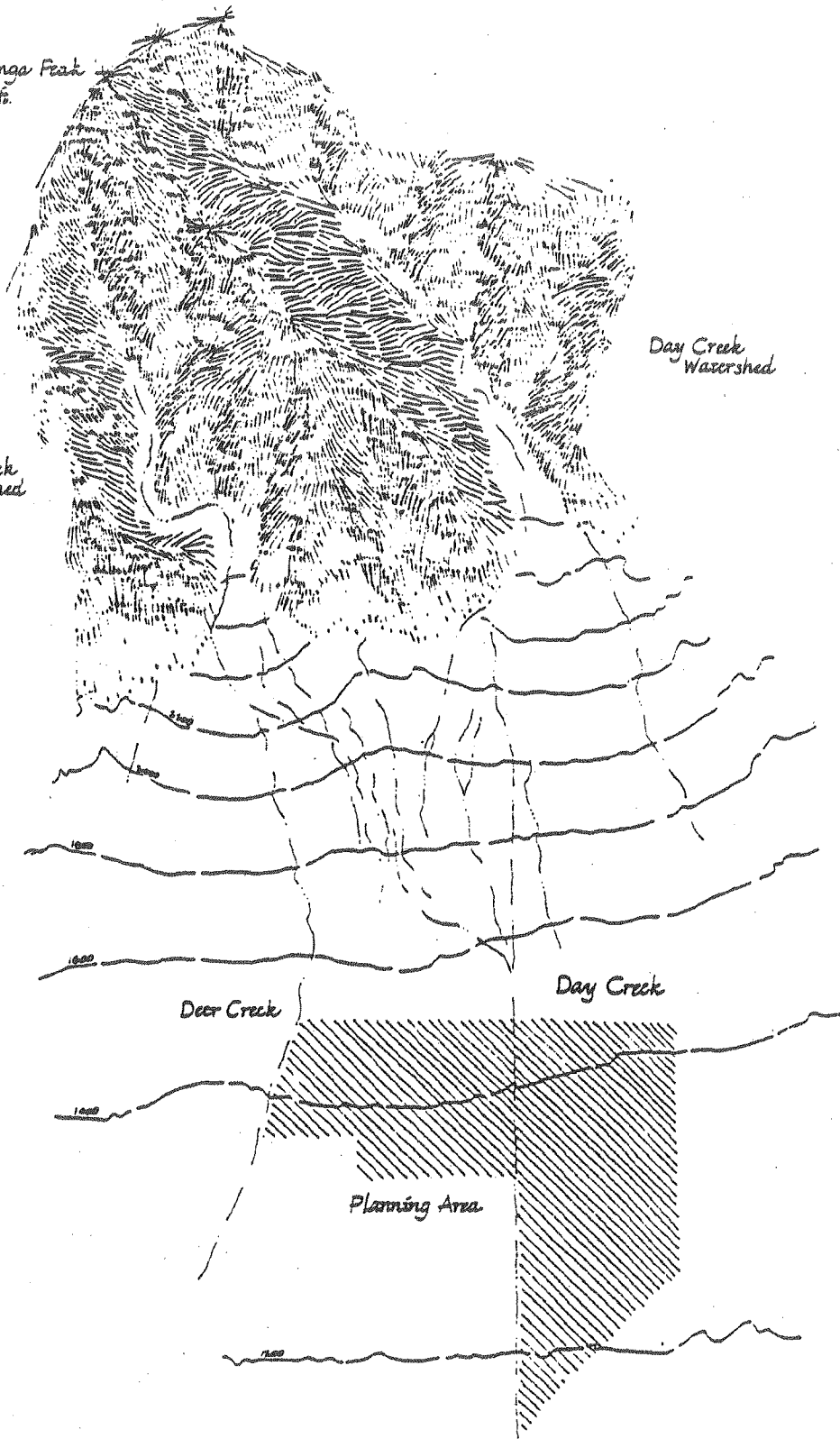
Associated with this Community Plan is a Draft Environmental Impact Report (EIR) that provides information on the impact of this plan on the natural and cultural environments. The EIR examines the proposed plan and reports the needs of the community with respect to schools, fire and police protection, flood control and other community services.

Together, the Community Plan and the Draft EIR are the basis for translating the goals of the General Plan into reality for the portion of city that they cover.

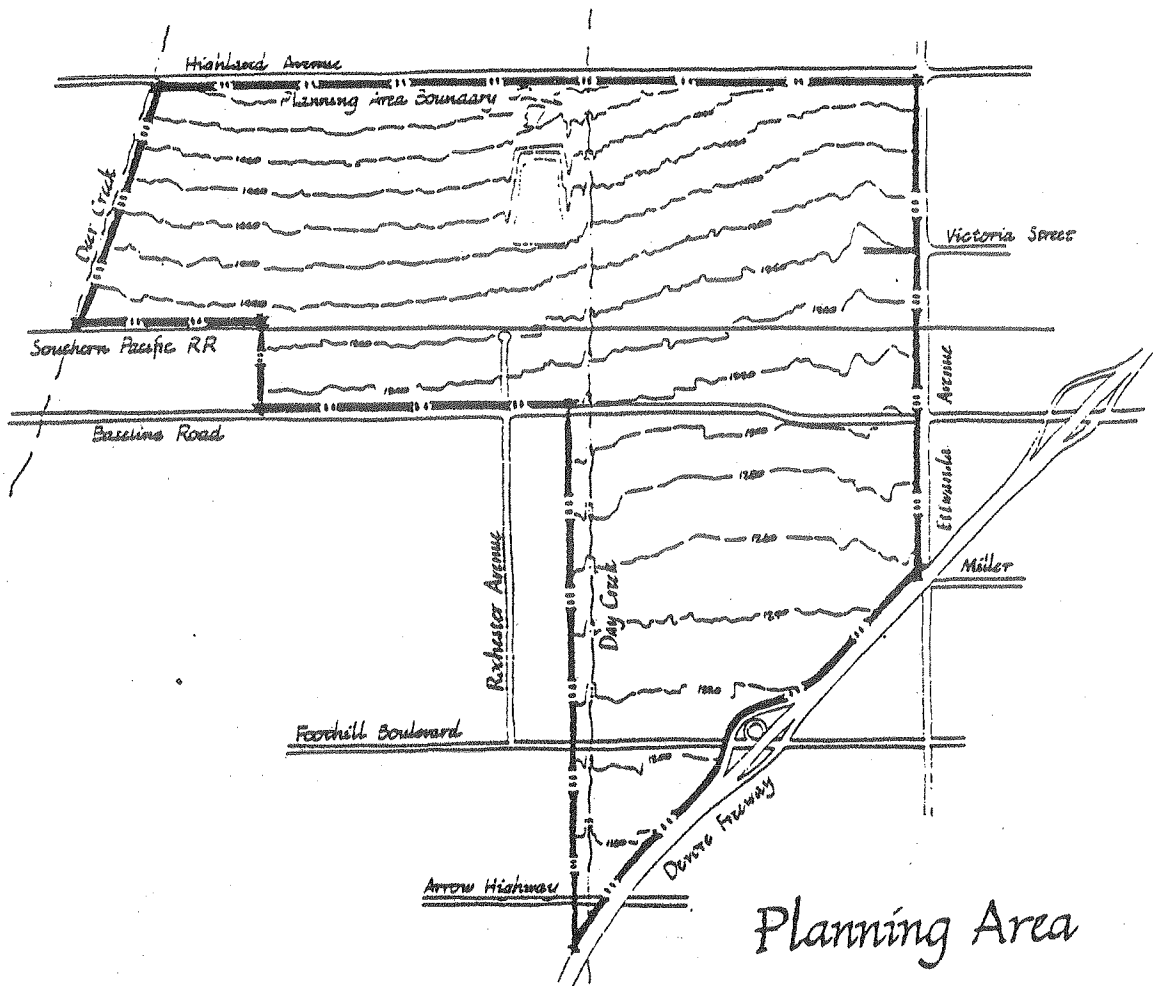
Cucamonga Peak
8859 ft.

Deer Creek
Watershed

Day Creek
Watershed



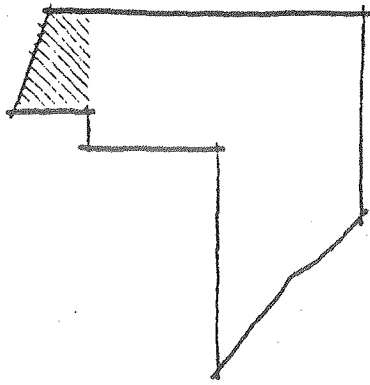
Physical Context



EXISTING SITE CONDITIONS: PLANNING AREAS 1, 2, 3 AND 4

Physically, the Planning Area is located on a gently sloping alluvial fan at the base of the San Gabriel mountains. Although portions of the site have been used, or are still in vineyards or citrus crops, these agricultural activities have become increasingly difficult to sustain economically, and much land in the planning area now lies fallow. Weedlots have replaced the citrus orchards and vineyards that have been destroyed by freezes. The drawing above shows the boundaries of the Planning Area.

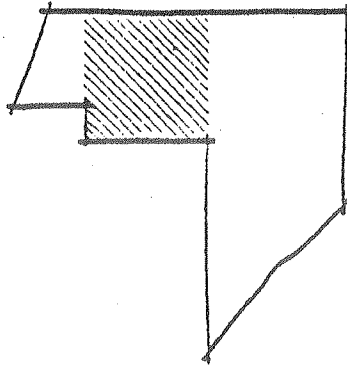
To simplify understanding of the other relevant existing conditions, the Planning Area is divided into several sub-planning areas:



Planning Area One

Sub-planning area 1 is the northwesterly portion of the site between Deer Creek on the west, Highland on the north, the proposed extension of Milliken on the east and the railroad tracks on the south. It is on the eastern edge of Alta Loma, a residential community predominately consisting of single family homes. Within the site itself, there is a mature stand of California Pepper trees. Deer Creek, the western boundary of the site, carries storm runoff from the mountains south to Prado Dam. At present it is an intermittent stream and current plans include channelization by the Army Corps of Engineers to prevent flooding which would be a constraint to future development.

All of Area 1 is within the Alta Loma Elementary School District and the Chaffey High School District.

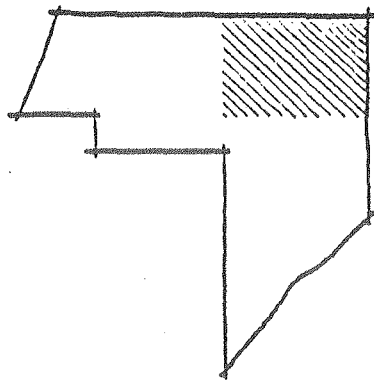


Planning Area Two

Sub-planning area 2 is bounded by Highland on the north, Day Creek on the east, Baseline on the south and the Milliken Extension on the west. The site is bisected in the southerly third by the Southern Pacific Railroad. Although these tracks are only used six times a week at present, they were once part of the Pacific Electric Railroad, or Red Car Line, as it was affectionately known. The northeast corner of the site is a large flood control retention basin operated by the San Bernardino Flood Control District. Periodically, the district releases flood water into Day Creek, and recharges the aquifer by retaining flood water.

Most of the land in Area 2 is vacant and has little defined character. There is a lumber yard at Baseline and Rochester and some scattered vineyards, but most of the area is covered with herbaceous scrub vegetation.

Area 2 is entirely within the Etiwanda Elementary School District and the Chaffey High School District.



Planning Area Three

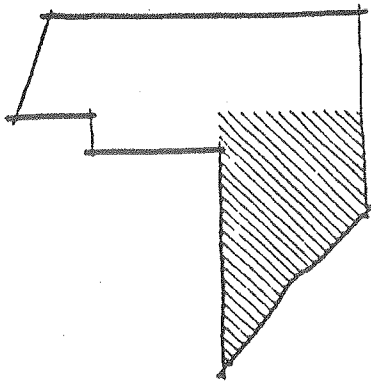
Sub-planning area 3 consists of the land between Highland on the north, Etiwanda Avenue on the east, the railroad on the south, and Day Creek on the west. Etiwanda Avenue is the central north-south road in the Etiwanda Community. It is lined with Silk Oak and Eucalyptus trees in various places, and the older single family homes along both sides of the street create a pleasing residential character. The region east and west of Etiwanda was once an important citrus raising area and the remnants include many windrows, installed many years ago to protect the citrus groves from the high winds that often sweep into the valley from the Cajon Pass. The windrow tree is Eucalyptus globulus, and most seem to be healthy specimens. Their relatively close spacing and linear alignment on a 330' x 660' grid creates a special feeling of being in a large outdoor "room" in which these trees are the walls, and the sky is the ceiling. They extend as far west as the center of sub area 3.

Although the Etiwanda area is sparsely developed, it has a strong character created by the tree plantings and the mix of land uses. Single family homes with generous setbacks and informally landscaped front yards contribute to the pleasing quality of this neighborhood. As a community with a long history, it is served by a variety of community facilities, including Etiwanda Intermediate School at Victoria and Etiwanda Avenue, Summit Elementary School at East Avenue and Highland, and a fire station on Etiwanda Avenue north of Baseline.

The western boundary of sub area 3 is Day Creek. Adjacent to Day Creek, and again approximately 1000' east, are two 300' wide corridors of land, aligned north-south, that belong to the Southern California Edison Company (SCE). These two corridors, together with a third outside the planning area east of the Devore Freeway are the principal electric utility access to the Pomona Valley from power

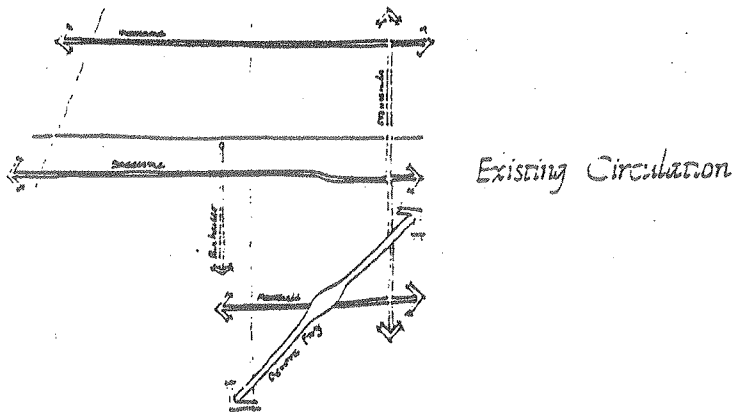
generating stations at Boulder Dam, Four Corners, and the Bonneville Dam. An additional generating station in the Lucerne Valley area has been recently approved and when it is constructed it will also serve power needs in the western valley area via these utility corridors. Presently, the western corridor has two existing power lines. Three more are eventually planned, one more in the western corridor and two in the eastern corridor. The location and importance of these corridors have a major impact on any proposed development in this area.

As in sub area 2, sub area 3 is served by the Etiwanda Elementary School District and the Chaffey High School District.



Planning Area Four

The remaining portion of the planning area is included in sub area 4. The railroad is the northern boundary. As in Area 3, Etiwanda Avenue is the eastern boundary, though there are not as many of the Eucalyptus Windrows within Area 4 as in Area 3. The southern edge is Interstate 15 (the Devore Freeway) which connects to Interstate 10 (the San Bernadino Freeway) to the south and the Cajon Pass to the northeast. The Devore Freeway roadbed is raised above the natural grade to cross important arterials like Foothill Boulevard and, consequently, views of Area 4 are particularly good from the freeway, even while traveling at high speeds. The western edge of the site is Day Creek, and the SCE utility corridors continue through to the south. Adjacent to the westerly SCE corridor on Baseline Avenue is a fire station of the Foothill Fire District. This station currently serves the entire planning area. In the center of the site along Baseline is a large distillery belonging to Heublein, and several residential homes. Land use along Etiwanda includes some single family homes, the Nichiren Temple, the Etiwanda Congregational Church, and a U.S. Post Office. Area 4 is within the Etiwanda Elementary School District and the Chaffey High School District.



Currently, north-south traffic is carried on Etiwanda Avenue and Rochester. West of the Planning Area, Haven is the major north-south arterial. Although a dedicated right-of-way exists for Milliken, and the General Plan indicates that it will be an important arterial in the future, it is presently an unimproved road. Rochester Street extends into the planning area north of Baseline, but dead ends at the railroad.

East-west traffic is carried on Highland Avenue, Baseline and Foothill Boulevard. Parallel and adjacent to Highland is a transportation corridor that may eventually contain the extension of the Foothill Freeway, though Caltrans has no plans at present to build this freeway. Proposed interchanges are currently shown at the intersections of an extended, improved Milliken and at Etiwanda Avenue. The presence of a freeway interchange at Etiwanda Avenue would change this road from a low volume road serving local residences, to a high volume, high speed arterial. Victoria Street, an important street serving residences east of Etiwanda Avenue, is unimproved west of Etiwanda, Baseline is an important road because of its location and connections, but is presently unsuited to high speed or high volume traffic. Foothill Blvd. is the old U.S. Highway 66, and is a high speed road where it bisects the southern part of the planning area and intersects the Devore Freeway. The Devore Freeway runs northeast-southwest. It is a limited access, high speed, eight-lane freeway with exits at Foothill, Baseline, and Highland. 16

EXISTING COMMUNITY FACILITIES

Community facilities in the planning area are somewhat limited because of the relatively low population. There are no existing schools or school sites in the planning area, but there is an intermediate school immediately adjacent at the southeast corner of Etiwanda Avenue and Victoria Street. On Rochester south of Baseline the Etiwanda School District owns a 20 acre site scheduled to be a future intermediate school. The proposed Rancho Cucamonga General Plan currently shows four schools within the planning area; three elementary schools and an intermediate school. As mentioned before, the area west of Milliken is in the Alta Loma Elementary School District. The small portion of the planning area south of Foothill is in the Cucamonga School District, and the remainder of the planning area is within the Etiwanda Elementary School District. Unlike the Alta Loma and Cucamonga Elementary School Districts, the Etiwanda District has only grades K through 4 in its elementary schools, and grades 5 through 8 in its intermediate school. The other districts divide their students between the 6th and 7th grades. The entire planning area is within the Chaffey High School District.

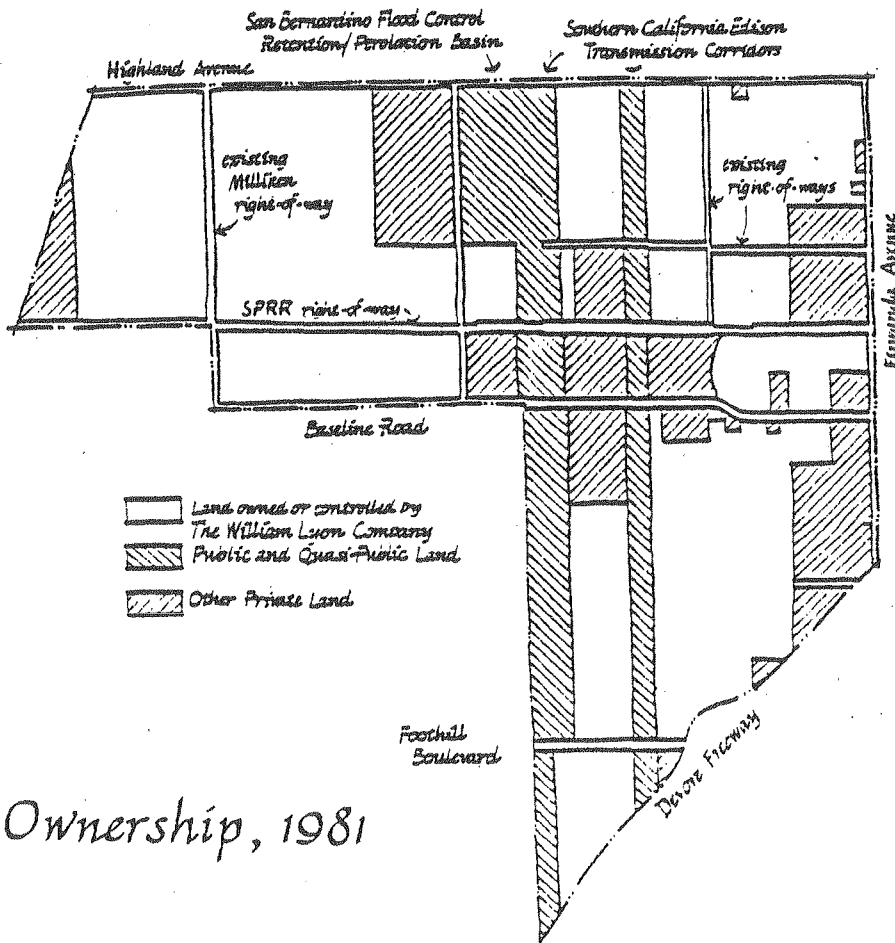
Fire protection currently is provided by the Foothill Fire District facility north of Baseline and east of Day Creek. As part of the City of Rancho Cucamonga, the entire plan area is served by the San Bernardino County Sheriff's Department. A library in Rancho Cucamonga on Baseline Road near Hellman is also provided by the county. Additional library services are provided by the public schools. The Congregational Church, Church of the Highlands, and Nichiren Temple are the existing religious facilities in the planning area.

EXISTING LAND OWNERSHIP

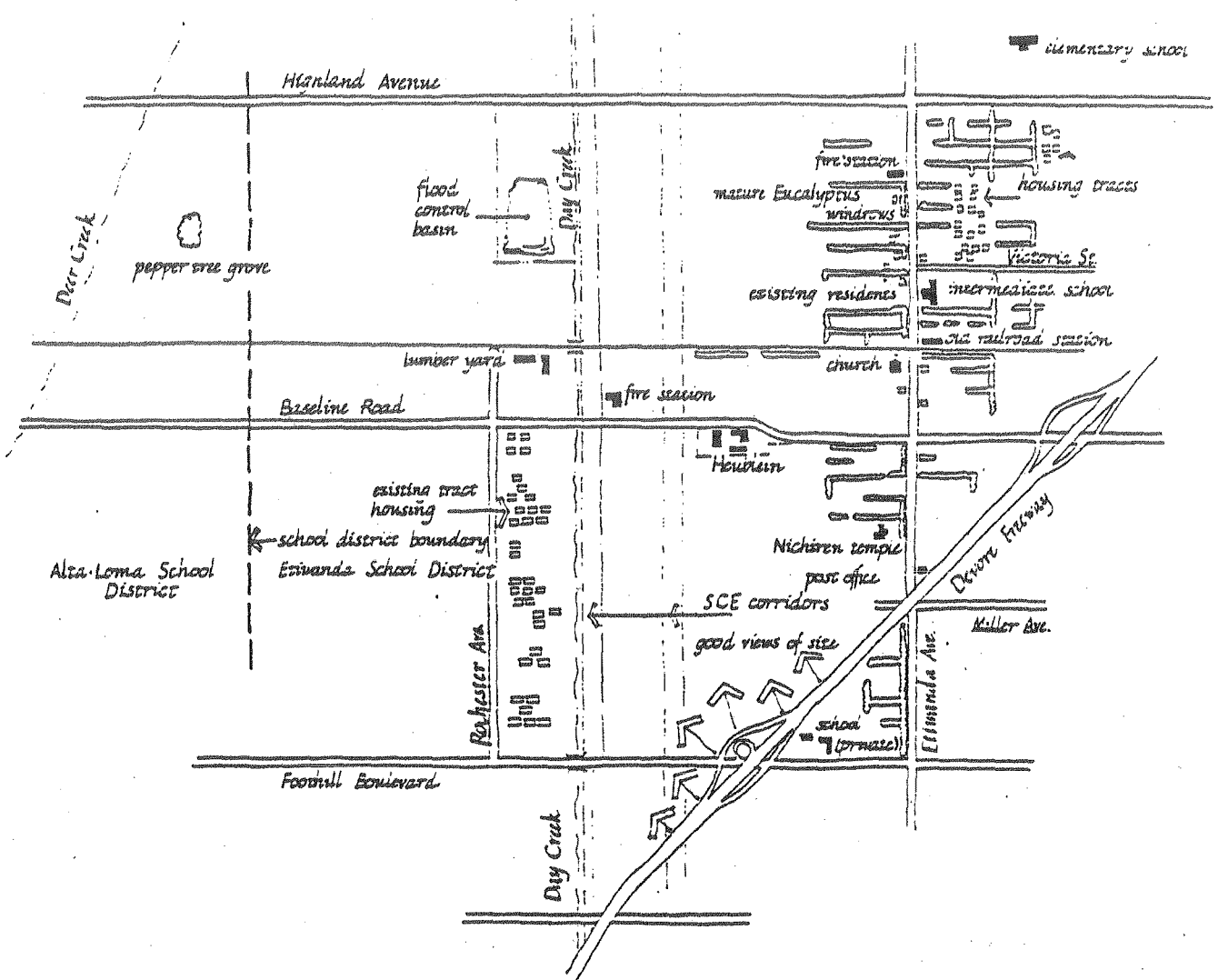
The diagram below illustrates the land ownership within the Planning Area. Although The William Lyon Company owns or controls most of the land, their land is not in one large parcel, but in many smaller ones that don't always connect to one another.

The San Bernardino County Flood Control channel and retention basin, and the two Southern California Edison corridors together contain 260 acres. This public and quasi-public land, when combined with the 1,350 acres of The William Lyon Company, comprises 75% of the total planning area of 2,150 acres.

The other land is privately held in parcels ranging from small lots for single family dwellings to holdings of 20 or 30 acres and more.



Land Ownership, 1981



Summary of Existing Conditions

II
***OBJECTIVES AND
PLANNING CONCEPTS***

PLANNING OBJECTIVES

The principal planning objective of this plan is to create a total, integrated community that responds to the goals of the General Plan, the existing environmental issues identified within the planning area itself and the unfulfilled need for housing and community services. As corollaries of this objective, the Plan will provide:

- 1) usable open space that creates an attractive environment;
- 2) residential housing that local employees, present and future, will be able to afford and will want to buy;
- 3) commercial development that serves the needs of local residents as well as the residents of the larger region;
- 4) enhancement of the existing character and provision for enrichment of that character in the future.

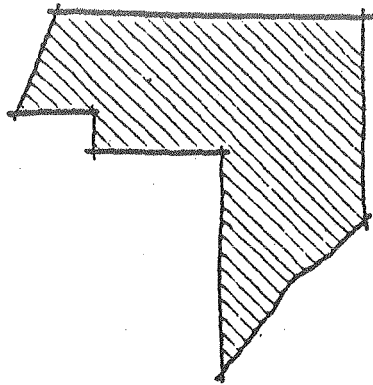
By provision of the Plan's concepts and design guidelines, the City and developers alike are encouraged and, in fact, assured that over time, the implementation of specific portions of the plan will result in a logical and compatible total community.

PLANNING CONCEPTS

The Planning Concepts are the general guidelines that will provide the continuity needed to achieve the planning goals and objectives. As development proceeds within the planning area, these concepts will serve as a measuring stick against which future planning decisions can be evaluated. The description of the plan (Part I), the design criteria (Part II), and the standards and regulations (Part III) contain more detailed information that complement and elaborate on the general Planning Concepts presented here.

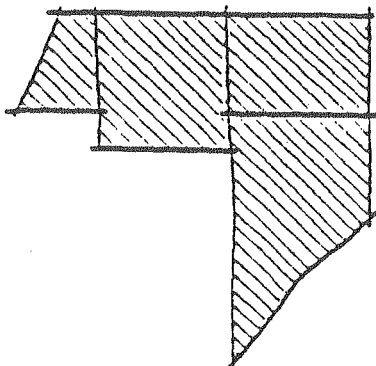
The following summarizes the basic planning Concepts for the Planning Area.

A CREATE A TOTAL INTEGRATED COMMUNITY, CALLED VICTORIA, THAT BORROWS CHARACTER FROM THE EXISTING SITE CONDITIONS, BUT BUILDS ITS OWN IDENTITY.

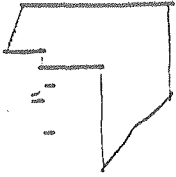


Total Integrated Community

B CREATE A SERIES OF SMALLER VILLAGES WITHIN VICTORIA THAT HAVE AS THEIR FOCUS A CENTRAL OPEN SPACE AND COMMUNITY FACILITIES. EACH OF THESE VILLAGES WILL, IN TURN, RESPOND TO THE SPECIFIC ISSUES OF ITS LOCATION, THE NEEDS OF ITS RESIDENTS, AND THE MARKETPLACE.



Separate Villages



Area 1 will be a residential village called Victoria Groves and will contain a multi-use open space which will use the existing pepper tree grove on the old farm as its theme.



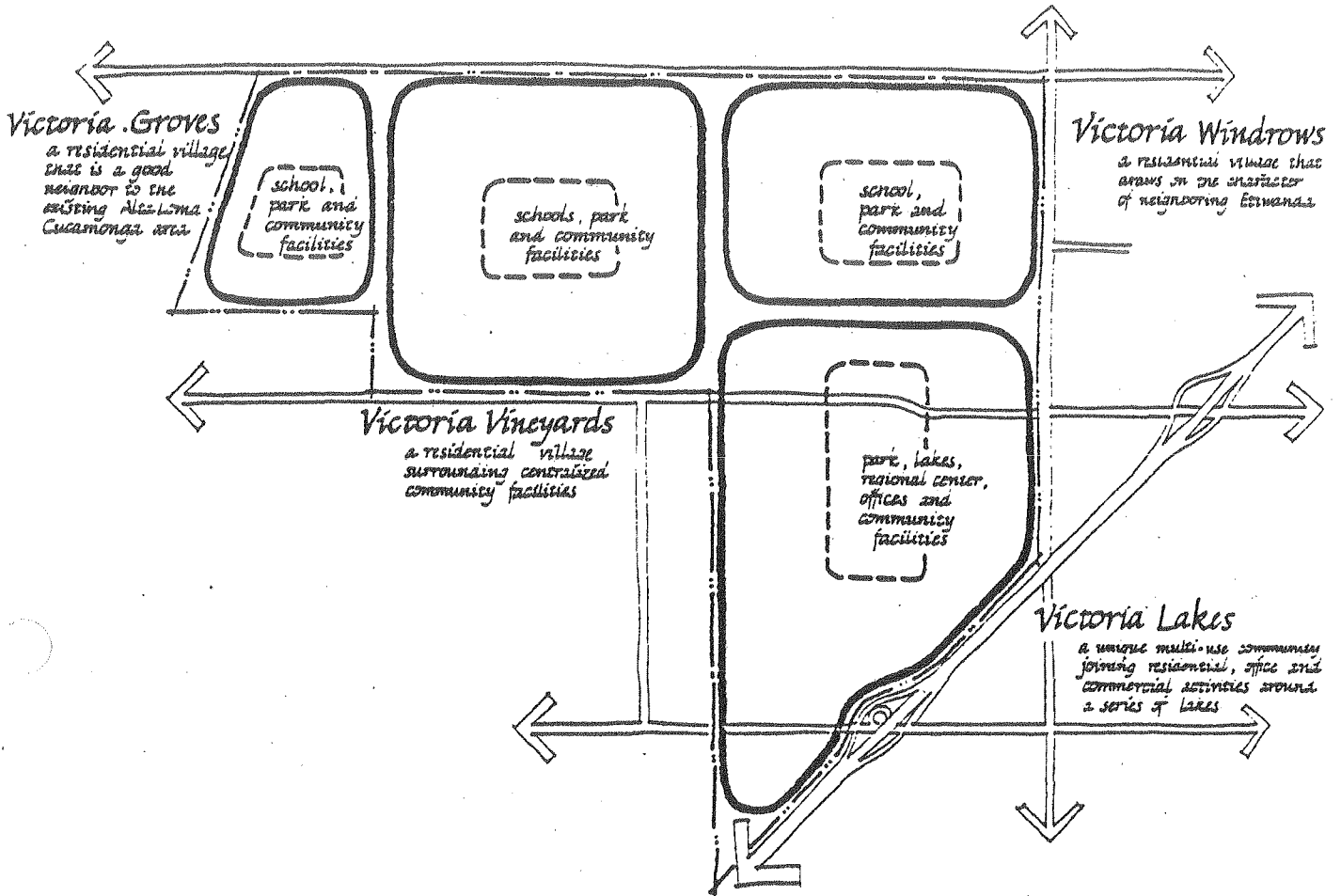
Area 2 will be a residential village called Victoria Vineyards and its thematic character will be generated by an active use park in its center with combined school and community facility.



Area 3 will become a residential village called Victoria Windrows. Its central open space will be a more passive park with a small lake as its focus. The design of this park will extend and reflect the existing Etiwanda character with a planting design that utilizes the existing windrows and palm trees.



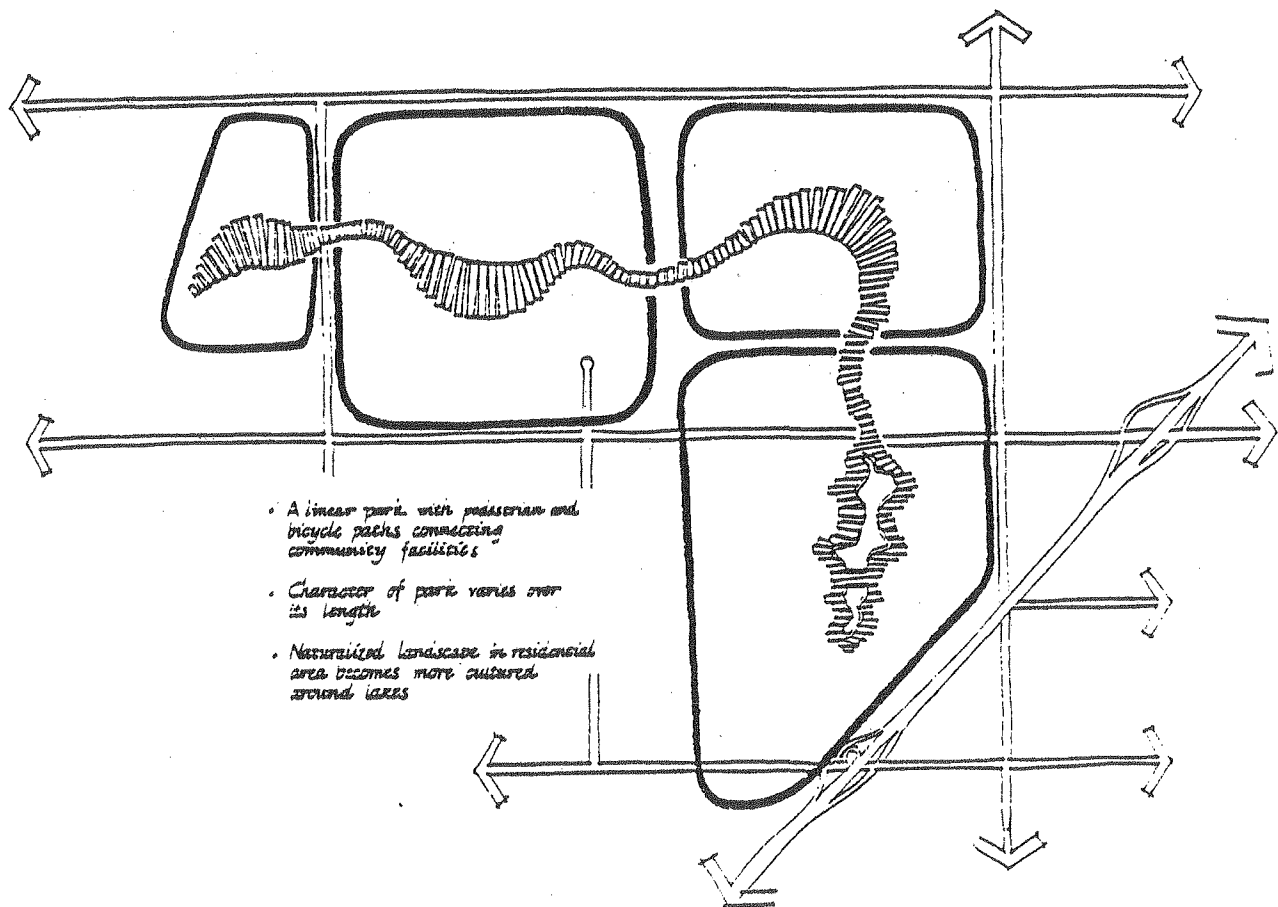
Area 4, Victoria Lakes, will be a unique multi-use community including a series of lakes, each surrounded by a succession of land uses from residential to office to commercial, culminating in a Regional Shopping Center at Foothill and the Devore Freeway.



An Interrelated Community of Villages

C. CREATE A COMMUNITY WIDE OPEN SPACE SYSTEM - A "LINEAR PARK"
- THAT TIES THE VILLAGES TOGETHER AND CONNECTS TO THE
EXISTING AND PROPOSED CITY AND COUNTY OPEN SPACE SYSTEMS.

Victoria Linear Park will connect each of the Villages' central open spaces and, in turn, connect the residential portions of the community together. The park will have a rustic, natural feeling: informal tree masses and naturalized shrubs and grasses, meandering pedestrian and bicycle trails will run the entire length, providing a framework for community activity.

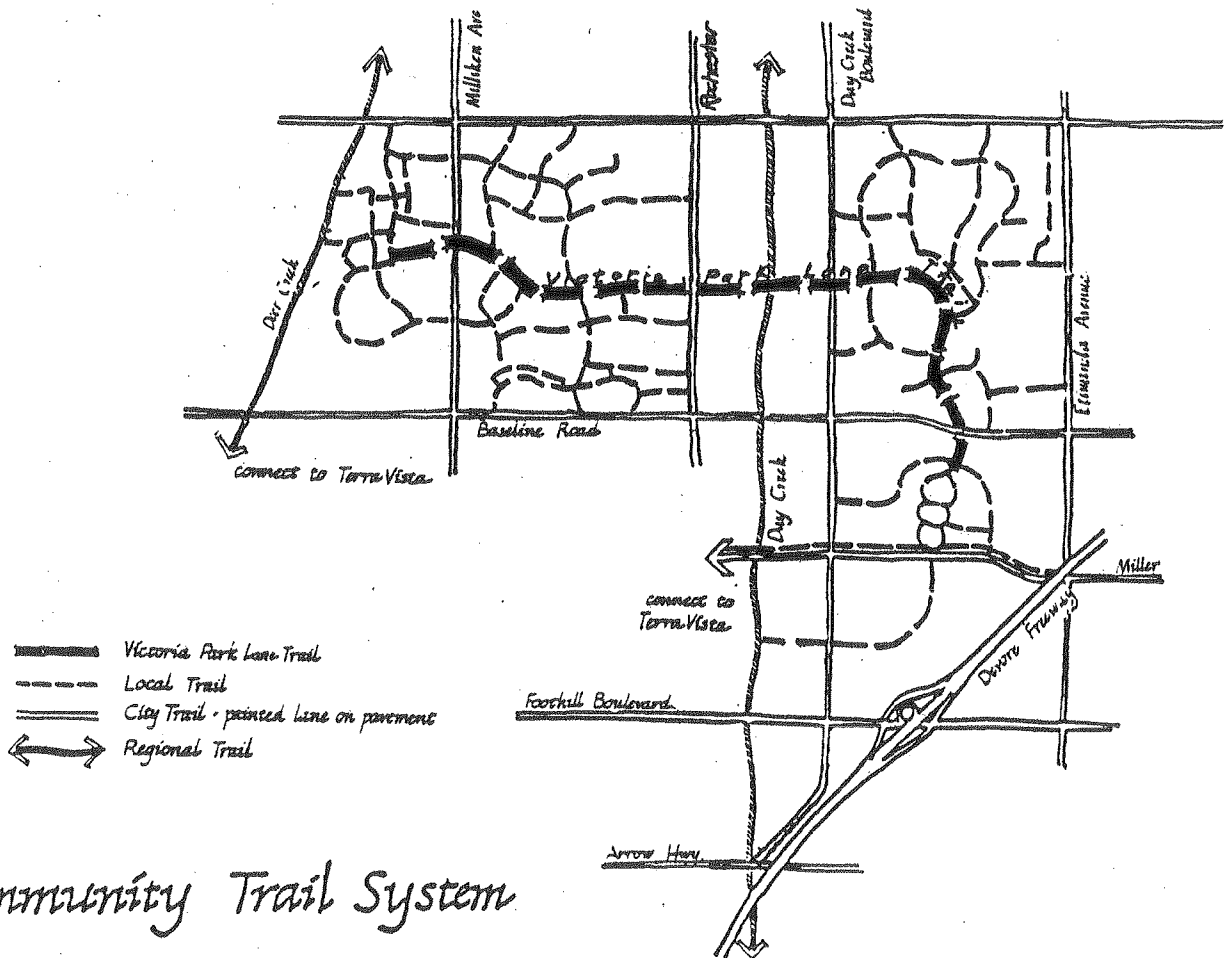


D. CREATE A RECREATIONAL TRAIL SYSTEM THAT ENCOURAGES PEDESTRIAN, BICYCLE AND EQUESTRIAN CIRCULATION

A system of trails will weave throughout the community, connecting residential areas to one another, and to the community open space system. The trails will connect the community with the regional open space and trail system along Day Creek and Deer Creek.

The trail system will take various forms: sometimes along roads, between different land uses, through residential areas, or within open space corridors. It is a system which is adapted to its particular location yet ties together as an uninterrupted connecting system.

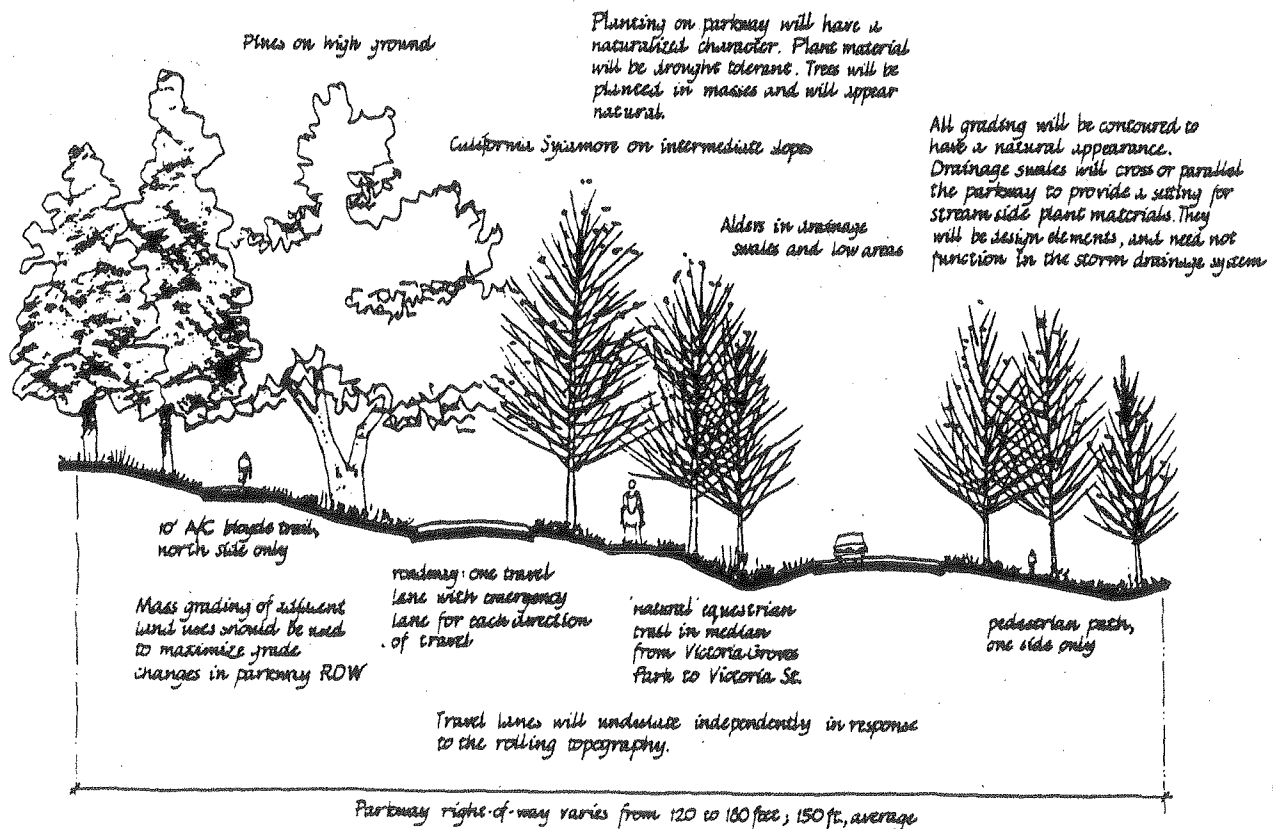
The diagram shows conceptually how the open space and trail system will work to accomplish this.



Community Trail System

E. CREATE AUTOMOBILE CIRCULATION ACCESS WHICH SERVES RESIDENTIAL NEEDS WITHOUT ENCOURAGING REGIONAL CIRCULATION INTRUSION, AND ENCOURAGES ALTERNATIVE MEANS OF MOVEMENT.

Victoria Parkway is an important part of the residential circulation system. Because of its location within Victoria Linear Park it is, literally, "a road in a park". The main visual entrances to the residential portions of each village are along the Parkway. These entries will be the most significant image forming element for the residents of the community. Although the parkway is an important road it will not be a big road: basically, one lane of traffic (with an emergency lane) in each direction separated by a generous median. The right-of-way will vary in dimension. At school and park locations, the views from the road will open on playing fields or park lawns so that as one drives along there are changing sequential experiences of enclosure and openness. The vertical and horizontal road alignment

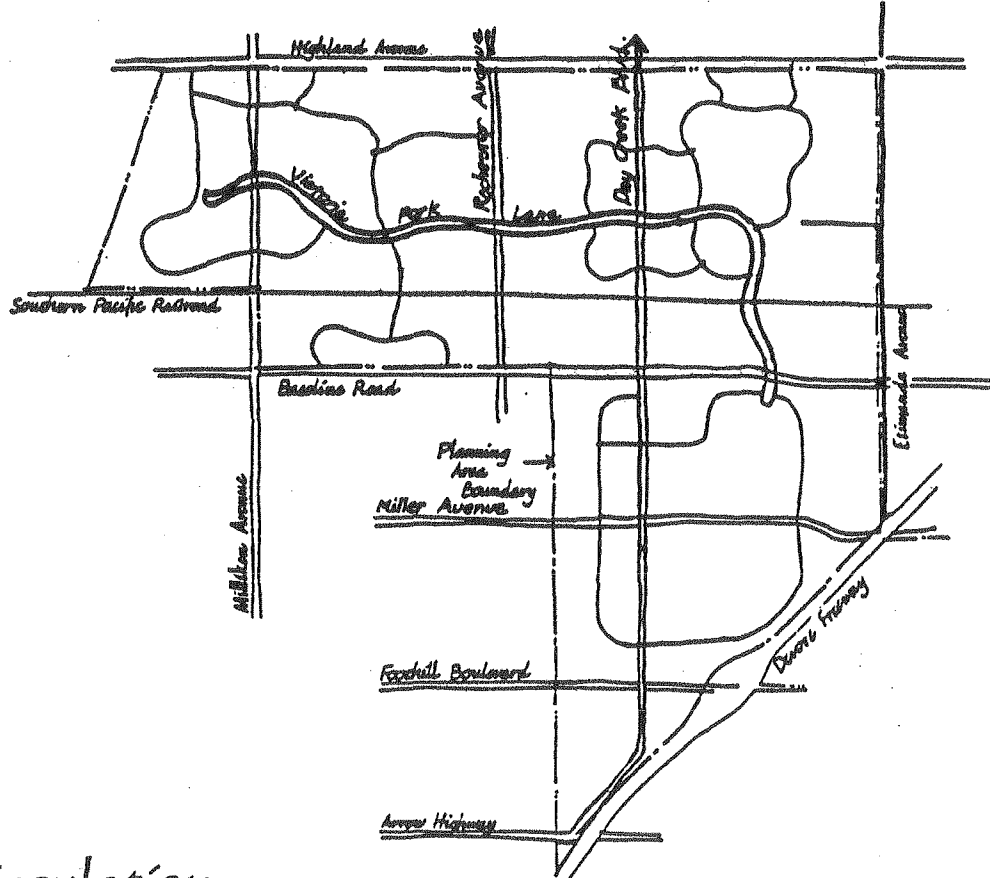


will respond to the existing rolling topography and each lane will undulate independently, enhancing the "country road" ambience of the parkway. The intent is to create a road that will be a low volume, low speed residential parkway.

Local residential roads in the community will connect to Victoria Park Lane and the peripheral arterial roads to encourage efficient traffic movement. Street tree planting will enhance the street edges.

Neighborhood streets which serve individual neighborhoods will not be through streets, but will be cul-de-sacs or loop roads to reduce through traffic and enhance the sense of neighborhood.

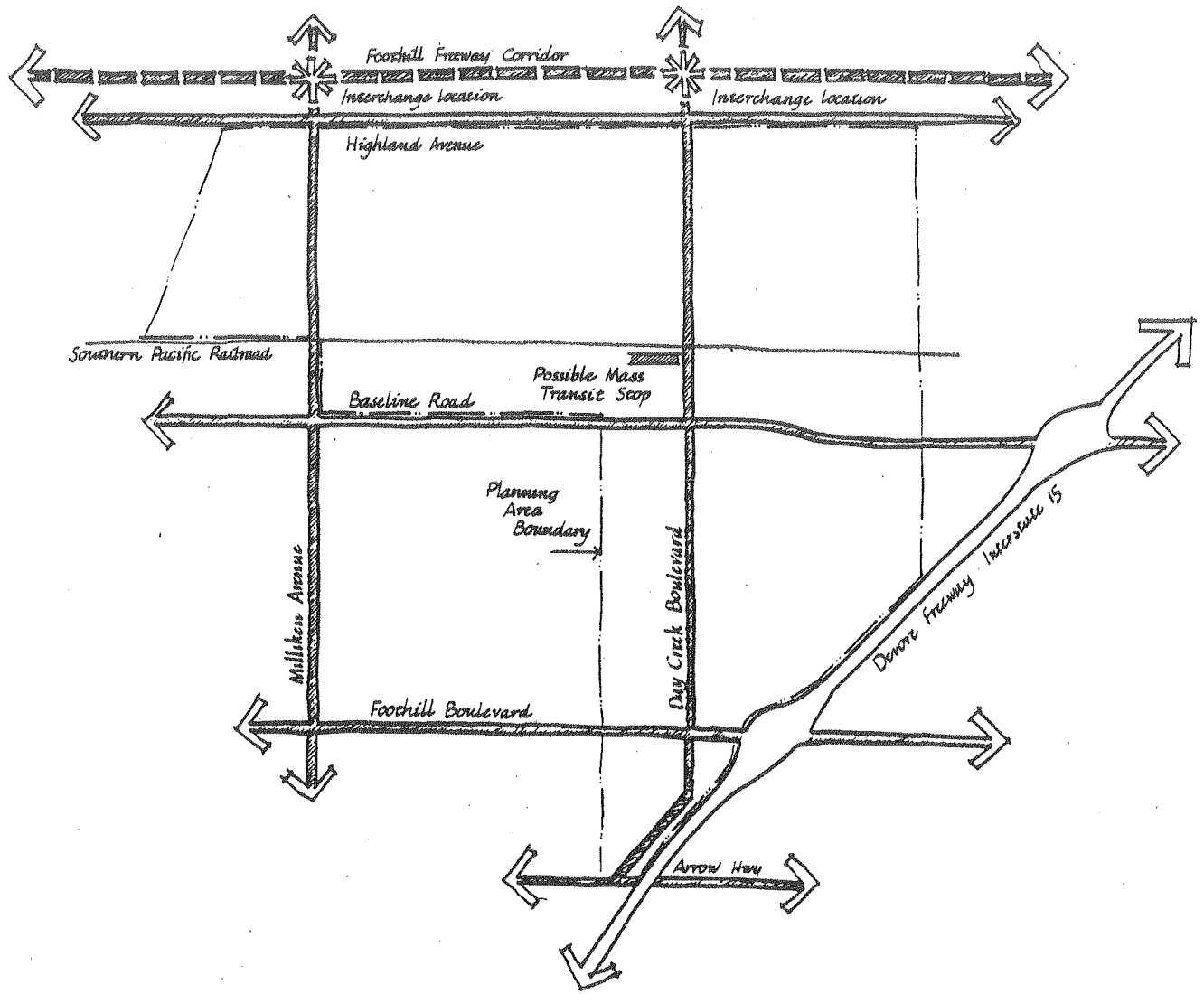
The diagram below illustrates the intent of the residential circulation.



Local Circulation

F. CREATE A REGIONAL AUTOMOBILE CIRCULATION SYSTEM WHICH ALLOWS CITYWIDE ACCESS AND MOVEMENT AND IS SEPARATE FROM RESIDENTIAL ROADS WITHIN EACH VILLAGE.

The diagram on the following page shows the proposed regional circulation system. In the case of Milliken, Highland, Baseline and Foothill, the existing system has been improved by expanding the width. To protect the special character of Etiwanda Avenue, Day Creek Boulevard, a new north-south arterial, is proposed west of Etiwanda adjacent to the SCE corridor. This road will help preserve the existing quality of the Etiwanda community by diverting regional traffic to the west. The proposed freeway exit now shown on the general plan at Etiwanda and the proposed Foothill Freeway would put an excessive number of automobiles on Etiwanda Avenue. Widening Etiwanda Avenue to accommodate this traffic would destroy the tree-lined quiet residential character that is crucial to the neighborhood's identity. The new Day Creek Boulevard will provide necessary north-south circulation without disturbing the Etiwanda character. To create an attractive streetscape, all arterial road edges will be landscaped.



Regional Circulation

G. CREATE AN OVERALL LANDSCAPED URBAN ENVIRONMENT WHICH IS PERCEIVED AND USED AS A SPECIAL PLACE BY THE RESIDENTS AND VISITORS OF VICTORIA, THE CITY OF RANCHO CUCAMONGA, AND THE REGION.

The special landscape quality of Victoria will be created by using the established local landscape vocabulary throughout the new community. The windrow style of planting - trees with a vertical growing habit that are planted in a closely spaced linear pattern - will be found throughout Victoria as Special Boulevards, adjacent to utility corridors, and weaving through residential areas as part of the trail system. The naturalized landscape character of Victoria Linear Park is an extension and enrichment of the rustic quality that pervades the planning area.

The lake system is a dramatic new element which will reinforce the connection between the residential and commercial land uses, and will provide an exciting, effective urban focus for Victoria.

III

***VICTORIA COMMUNITY
LAND USE PLAN***

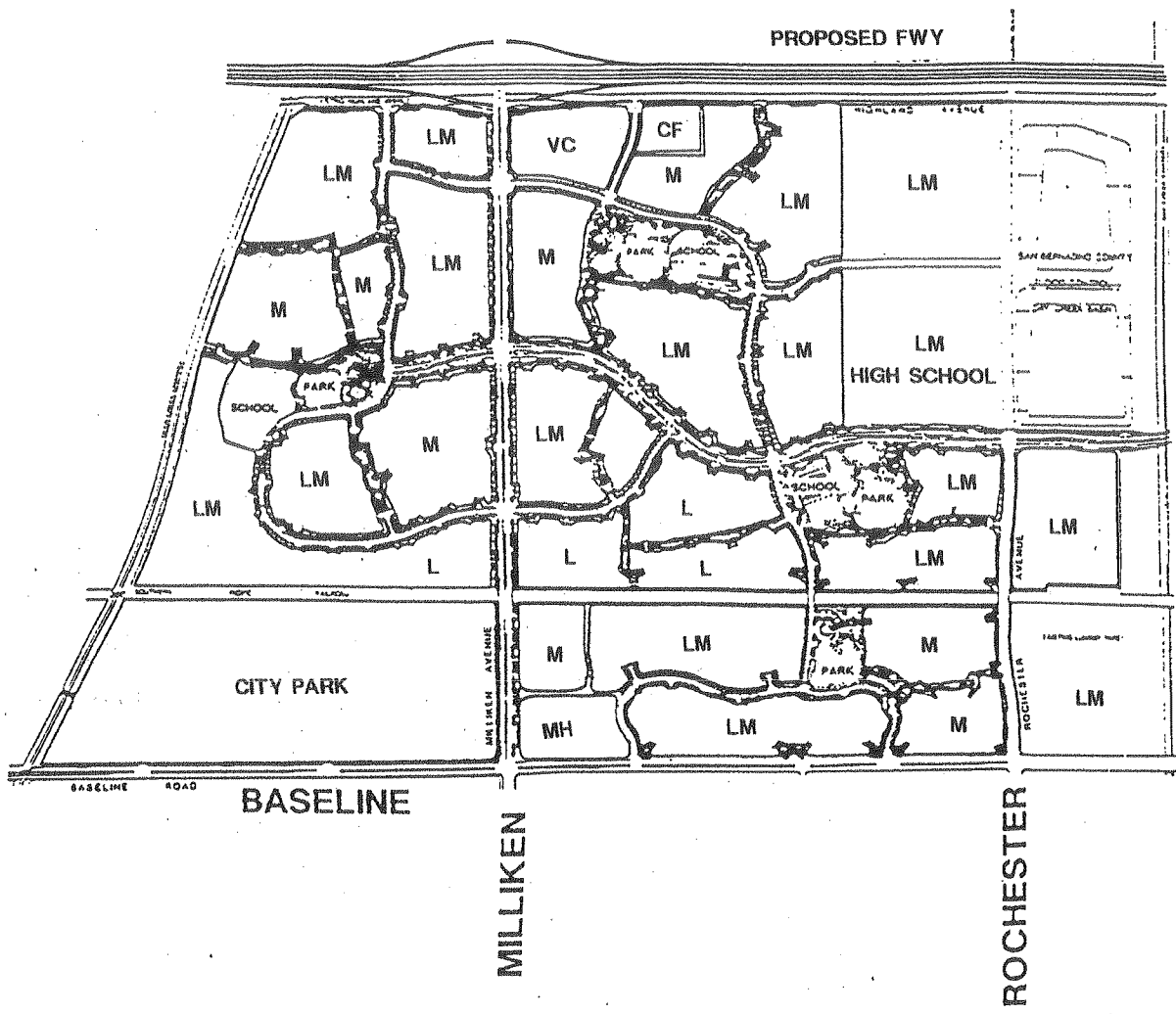
THE VICTORIA COMMUNITY LAND USE PLAN

On the following pages is the land use plan for the planning area. On this diagram, the integrative open space elements that unify the residential and commercial uses of each village have been illustrated. Victoria Groves, Victoria Vineyards and Victoria Windrows each have a multi-use community facility as the central village amenity. Victoria Linear Park links these open spaces together and provide one continuous pedestrian and bicycle trail system for the entire community. At the terminus of Victoria Linear Park, adjacent to Victoria Lakes, is a large community recreation facility. A lake overlook at this location will allow views into the heart of the regional center. The open space connection to the regional center is provided by the public pedestrian and bicycle trails of Victoria Linear Park, which follow the lake edge into the regional center. The residential areas are connected to the central community open space of their village by the trail system.

REGIONAL SHOPPING CENTER

The focus of urban activity is the regional shopping center. The lake system actually comes into the heart of the "U" shaped center, creating an active exciting place for people with a mix of civic, community and commercial activities along the lake edge. Surrounding the regional center will be related commercial businesses, offices and some residences. Within each Village, a convenient neighborhood shopping center will serve the everyday needs of Village residents.

The goal is to provide a balance of commercial activities within Victoria and Rancho Cucamonga that encourage a more interesting and diverse city and promotes the conservation of energy and natural resources by placing these facilities close to the people who will be using them.



COMMUNITY PLAN
Victoria
 A Planned Community
 in Rancho Cucamonga

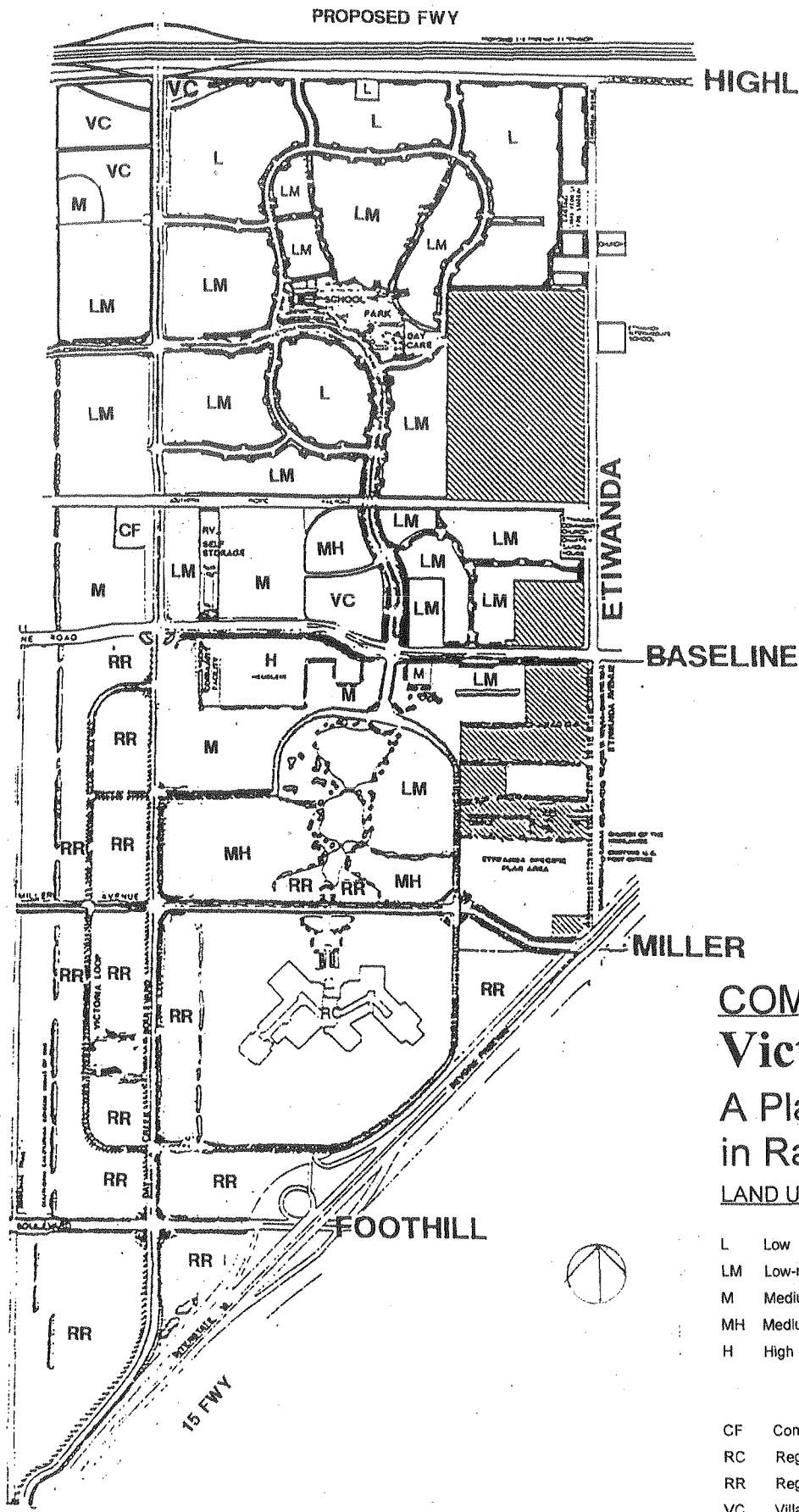
LAND USE LEGEND

RESIDENTIAL		
L	Low	2-4 DU/A
LM	Low-medium	4-8 DU/A
M	Medium	8-14 DU/A
MH	Medium-High	14-24 DU/A



COMMERCIAL	
CF	Community Facility
RC	Regional Center
RR	Regional Related Office/Commercial
VC	Village Commercial

NOTE: Hatched area represents privately held land not in the planned community.



REVISED DATE 08/01/01

COMMUNITY PLAN Victoria

A Planned Community
in Rancho Cucamonga

LAND USE LEGEND

RESIDENTIAL

L	Low	2-4 DU/A
LM	Low-medium	4-8 DU/A
M	Medium	8-14 DU/A
MH	Medium-High	14-24 DU/A
H	High	24-30 DU/A

COMMERCIAL

CF	Community Facility
RC	Regional Center
RR	Regional Related Office/Commercial
VC	Village Commercial

NOTE: Hatched area represents privately held land not in the planned community.

VARIETY OF RESIDENTIAL TYPES

There will be a wide variety of housing types at various densities within Victoria. Generally, higher residential densities will be near the regional center, or near the intersections of major arterials, and lower density residential areas will be farther from the regional center along the community edges.

Each residential land use is indicated by a letter which represents a variety of residential housing types within a specific density range. As actual construction proceeds, variations in residential housing demand or improved building technology may occur. Within each lettered residential land use, development will occur which may include several different housing products within the specified density range.

The density ranges for the residential land uses are:

- "A" - Windrow Residential: 2-3 dwelling units per acre du/A.
- "B" - Single Family - Low: 3-7 du/A.
- "C" - Single Family - Medium: 7-15 du/A.
- "D" - Garden Apartment/Condominium: 15-25 du/A.
- "E" - Town Apartment/Condominium: 25-35 du/A.
- "F" - Town Center Residential: greater than 35 du/A.

JOINT USE OF THE SOUTHERN CALIFORNIA EDISON CORRIDORS

Several uses are proposed for the Southern California Edison and county flood control corridors. Deer and Day Creek are ideal places to connect to the regional open space and trail system south of Baseline and to the regional park and

community. North of Baseline, this mixed use corridor will be a buffer to the residential areas. Here, the entire corridor can serve agricultural purposes such as raising nursery stock, sod farms, citrus orchards, vineyards, or community gardens, in addition to its function as a powerline corridor. These corridors can also serve as sanctuary for native plant and animal species.

IV
*THE VILLAGES OF
VICTORIA*

Victoria Groves

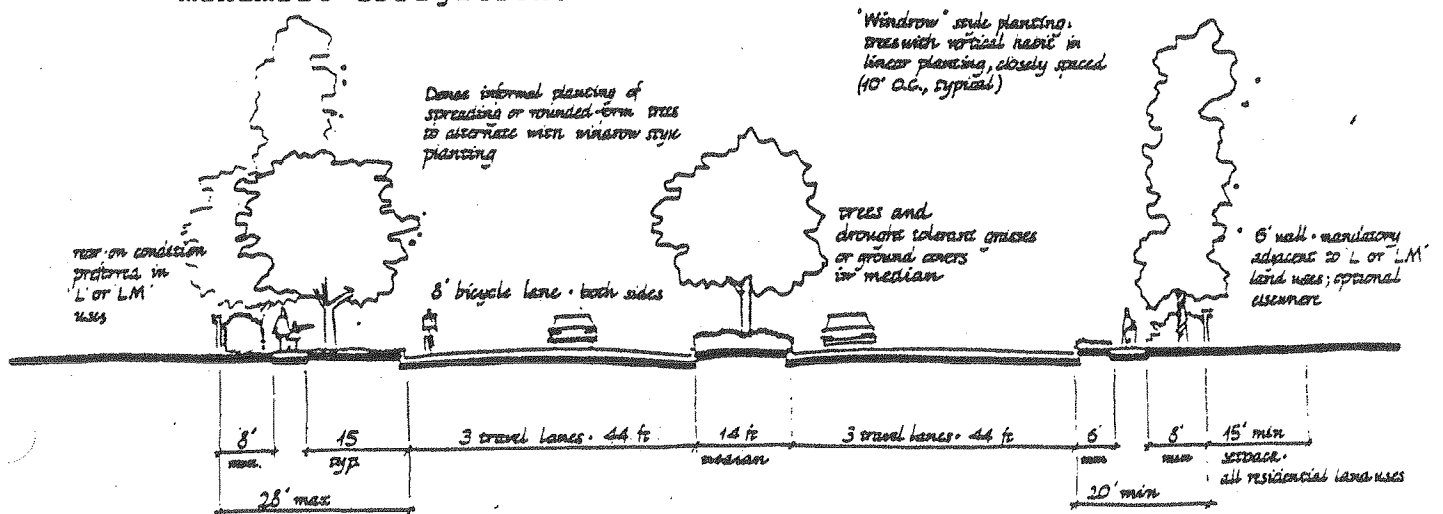
THE VILLAGE OF VICTORIA GROVES

VILLAGE CHARACTER

The character of Victoria Groves will build upon the mature grove of California Pepper Trees. The center of this Village will be a multi-use school and park community facility using the Pepper Trees in its design. The park and grove will serve as a visual and actual terminus of Victoria Parkway.

VILLAGE EDGES

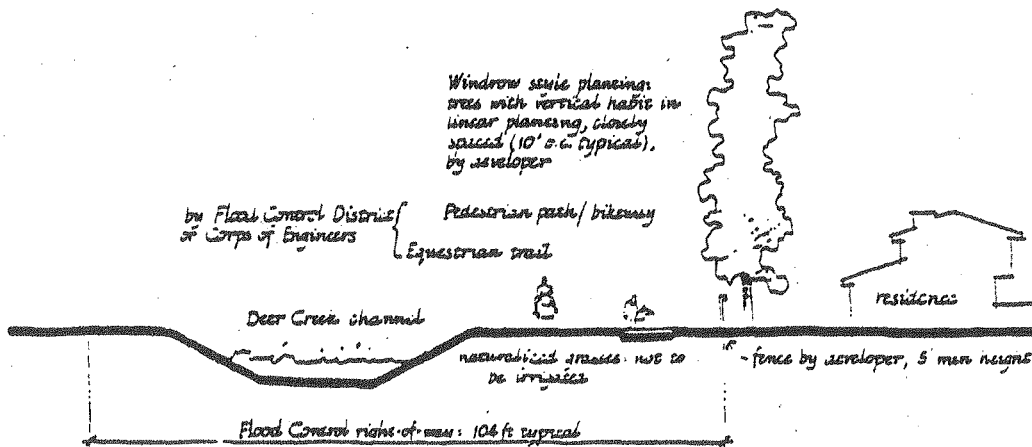
The Village is enclosed by Highland, Milliken, the Southern Pacific Railroad tracks, and Deer Creek. Highland and Milliken will be arterial roads and the Village residential areas will be buffered from the noise and traffic of these roads. The drawing below is a typical cross-section proposed for these roads showing how this could be accomplished in an attractive way. A wall that varies in setback from the road curb, with a linear "windrow" type of planting broken by smaller informal tree masses will provide the necessary acoustical and visual protection to create a pleasing and functional streetscape. Plant material selection will emphasize drought tolerant species and to minimize irrigation.



Arterial Roads

JOINT USE OF DEER CREEK

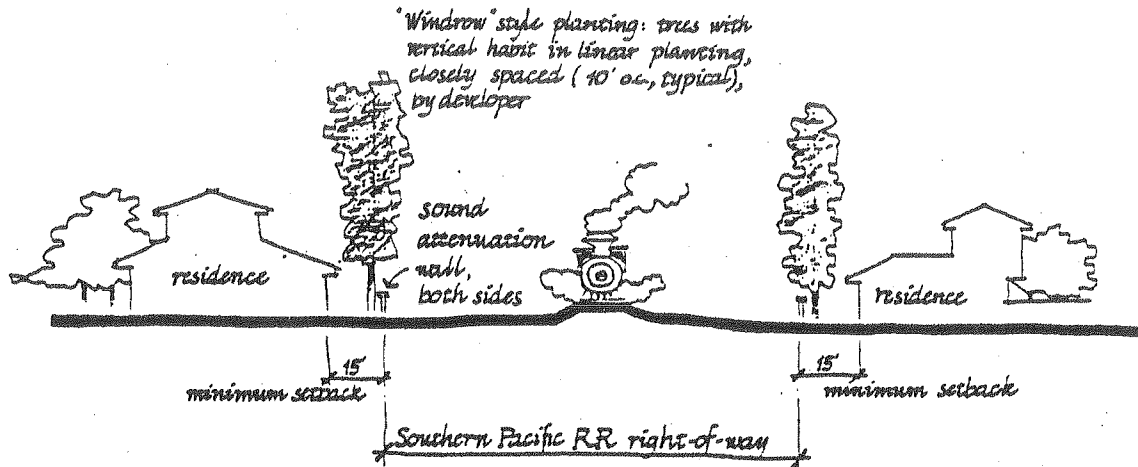
Channelization construction of Deer Creek is currently under way. If everything proceeds as planned, this project will be completed in 1982, and will provide flood control for Victoria Groves as well as the other parts of Rancho Cucamonga along its path. The drawing below is the typical cross-section that illustrates the proposed treatment of this corridor. The channel is shown offset to the western side of the 100' right-of-way. This would allow pedestrian, equestrian or bicycle trails within the right of way with little problem. These trails could eventually tie to the Regional open space lands to the north and other residential communities and open spaces to the south, such as Terra Vista. Although no Corps of Engineer funds are earmarked for these proposed uses, the right-of-way has ample space for the uses proposed.



Deer Creek

A POTENTIAL MASS TRANSIT CORRIDOR

Although the railroad tracks are currently used only six times per week, provision is made for possible future use of this corridor for mass transit purposes. The drawing below is a typical cross-section. Basically, a fence or wall at the private property line with a linear "windrow" like planting will be used on this edge.



Southern Pacific Railroad

THE VILLAGE ENTRY AND OPEN SPACE

The most interesting entry into Victoria Groves is through Victoria Linear Park on Victoria Parkway. The parkway terminates at the Groves Park where it intersects a road that connects to the residential community and, eventually, to Highland and Milliken. The park will host active uses, with facilities for the entire Village. Recreational elements that might be in the park could include a swimming pool or tot-lot. Picnic facilities might also be appropriate. Playing fields can be shared with the adjacent elementary school and there should be an area of unspecified use open space. If possible, parking for the school and park should be shared so that open space lands are not taken up by excessive parking lots. A continuation of the Victoria Linear Park trail system will connect through the Groves Park and the adjoining elementary school to the

regional trail system in the Deer Creek right-of-way, and south to Terra Vista.

THE HOMES OF VICTORIA GROVES

The residential land uses in Victoria Groves will include both detached and attached residences (see Land Use Plan). The Low Medium Density Residential ("LM") category may have single family homes on lots of 3,000 square feet (approximately 35'x90'), 4,000 square feet (45'x90'), 5,000 square feet (50'x100'), and 7,200 square feet (60'x120'). On the smaller lots, these homes may be "zero lot line" or "patio" type units to maximize effective yard space for these homes. Duplexes will also be allowed within this land use area. The density range will be from 4 to 8 dwelling units per acre (including local public streets and trails).

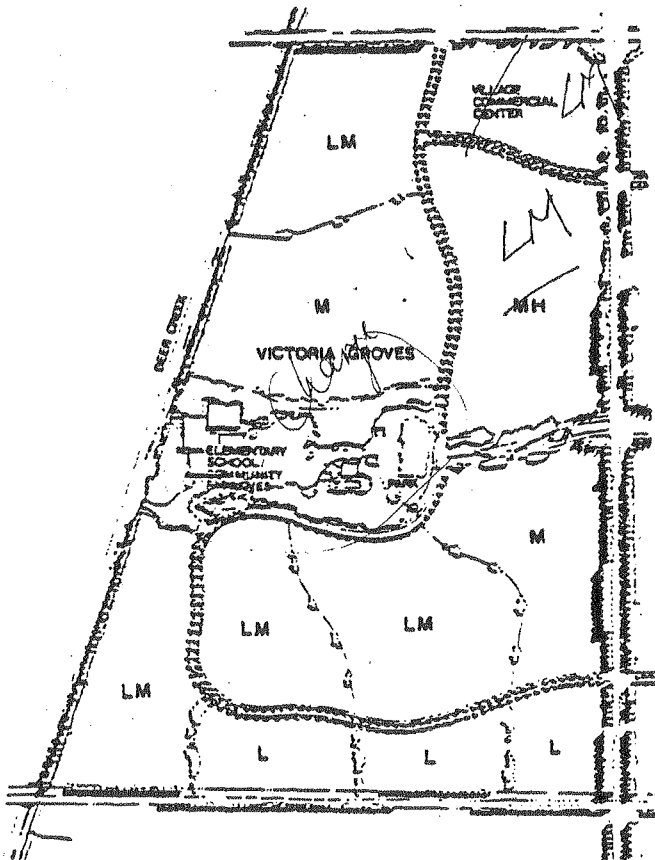
The Medium Density Residential ("M") category may include, but will not be limited to, several types of attached housing products including townhouses, condominiums, and fourplexes. The density range for this land use category is from 8 to 14 dwelling units per acre, including local public and private streets.

A VILLAGE COMMERCIAL CENTER AT MILLIKEN AND HIGHLAND

The Groves Village includes a neighborhood commercial center at Milliken and Highland. This location assures convenient auto, pedestrian or bicycle access for the Village residents. Typical uses for this kind of center would be a supermarket, a drugstore, a service station and other commercial services.

It is important that each Village Commercial Center have a strong thematic architectural treatment that is specific to that village. In Victoria Groves, a theme that uses architecture based on the traditional California Barn might be appropriate. Such a center in Carmel, California has the character that is being proposed and might serve as design inspiration. However, another theme might be more suited and an alternative treatment could be considered as specific plans are proposed.

The parking lot will be landscaped and the edges buffered to decrease the perception of large expanses of parking. Provisions will be made for safe and efficient pedestrian and automobile circulation.



see amended map on pg. 38

VICTORIA GROVES

LAND USE LEGEND		
RESIDENTIAL		
L	LOW	2-4 DU/A
LM	LOW-MEDIUM	4-6 DU/A
M	MEDIUM	8-14 DU/A
MH	MEDIUM-HIGH	14-24 DU/A
H	HIGH	24-30 DU/A
COMMERCIAL		
RC	REGIONAL CENTER	
RR	REGIONAL RELATED OFFICE / COMMERCIAL	
V	VILLAGE COMMERCIAL CENTER	
OTHER USES		
S	ALTERNATIVE USES - MOBILE HOME PARK OR CIVIC USES, RAPID TRANSIT STATION	
C	CHURCHES	
E	OTHER EXISTING USES	

COMMUNITY PLAN

Victoria
A Planned Community
in Rancho Cucamonga

the swa group
land planners

000 100 200

Victoria Vineyards

THE VILLAGE OF VICTORIA VINEYARDS

VILLAGE EDGES

Victoria Vineyards consists of the area between Highland on the north, Day Creek on the east, Baseline on the South, and Milliken on the west. The railroad tracks bisect the Village in the southern one-third. The Village edges on three sides are arterial roads. These edges will be treated in the same manner as in Victoria Groves. (see cross-section in the preceding section).

The eastern boundary of the Village is the Day Creek right-of-way and the county flood control basin. Like Deer Creek, this and the adjoining SCE right-of-way will contain a regional trail system.

THE VILLAGE ENTRY AND PARK

Entry to the Vineyards from the east or west via Victoria Parkway will be most interesting. The central open space is Vineyards Park which is centered between two schools. Like the Groves Park, Vineyards Park will be an active use public park. There will also be some free-play open space within the park. Any buildings or structures within the park and schools will be placed far away from Victoria Parkway possible to create long open space vistas for people traveling through Victoria Linear Park.

TRAILS CONNECT COMMUNITY TO SCHOOLS AND OPEN SPACE

Schools and open space are connected to the residential community by the trail system. The cross-sections on the following page illustrate the different trail conditions in residential projects. Where appropriate, the trail will be part of the residential street system or. The portion of the

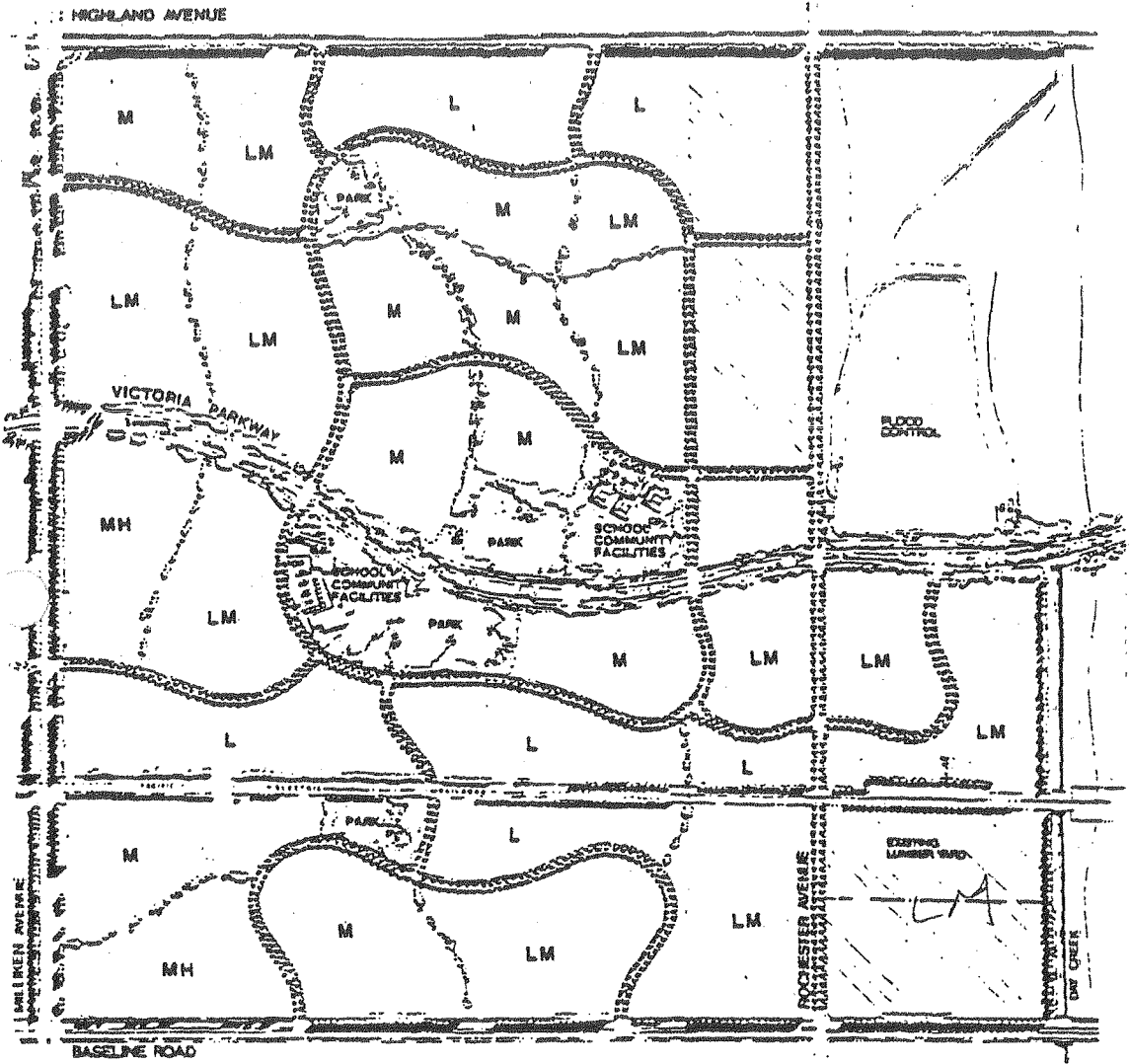
Vineyards south of the railroad tracks could have trail crossings at key locations along the tracks to allow pedestrian and bicycle access to the central open space system.

SCHOOLS IN THE VINEYARD

The schools in the Vineyards are elementary schools. Adjacent to the eastern school is a church site. Parking for the church and school should be shared if possible, and buildings will be placed within the site to maximize visual interest and continuity along Victoria Linear Park. All schools within Victoria are encouraged to provide facilities for community meetings and public gatherings. These facilities might include, for example, auditoriums for amateur theater, cafeterias for meetings, classrooms for continuing education programs, or day care facilities.

VARIETY OF RESIDENTIAL HOUSING OPPORTUNITIES

The residential land uses in Victoria Vineyards includes both the Low Medium ("L") and Medium ("M") Density Residential land uses discussed previously. In addition to these, another type of residential housing found in Victoria Vineyards is Medium-High ("MH") Density Residential. This land use designation would include a variety of apartment complexes and condominium units. The density will vary from 14 to 24 dwelling units per acre (including local streets). These projects may contain private recreation facilities like swimming pools, saunas, or gamerooms that are meant for their residents only.



LAND USE LEGEND

RESIDENTIAL		
L	LOW	2-4 DU/A
LM	LOW-MEDIUM	6-8 DU/A
M	MEDIUM	12-14 DU/A
MH	MEDIUM-HIGH	16-24 DU/A
H	HIGH	24-30 DU/A
COMMERCIAL		
RC	REGIONAL CENTER	
RR	REGIONAL RELATED OFFICE COMMERCIAL	
V	VILLAGE COMMERCIAL CENTER	
OTHER USES		
S	ALTERNATIVE USES - MOBILE HOME PARKS, CIVIC USES, RAPID TRANSIT STATION	
C	CHURCHES	
E	OTHER EXISTING USES	

see amended map on pg. 38

VICTORIA VINEYARDS

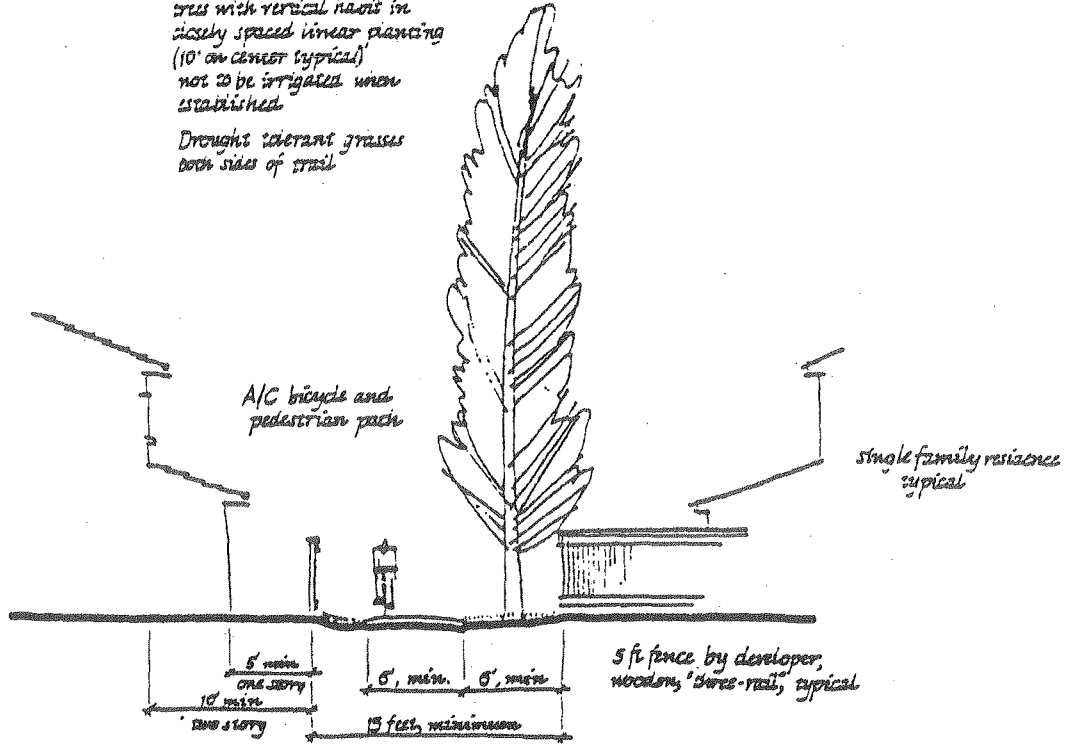
COMMUNITY PLAN

Victoria
A Planned Community
in Rancho Cucamonga

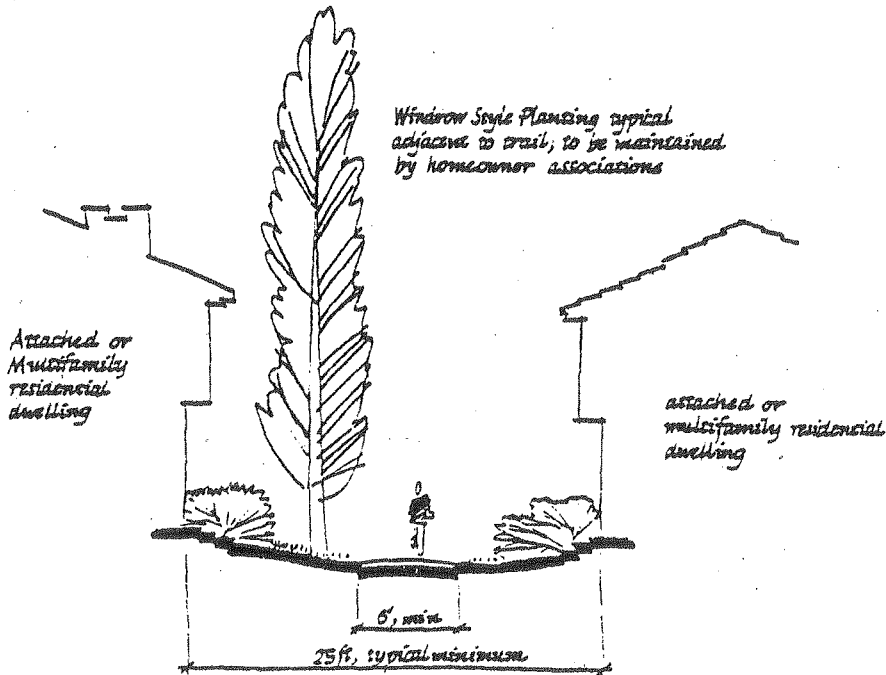
NOTE: HATCHED AREA REPRESENTS EXISTING
HOLD LAND NOT TO THE PLANNED COMMUNITY



Windrow Style planting:
 trees with vertical leaves in
 closely spaced linear planting
 (10' on center typical)
 not to be irrigated when
 established
 Drought tolerant grasses
 both sides of trail



Community Trail - through detached residences



Community Trail - through attached residences

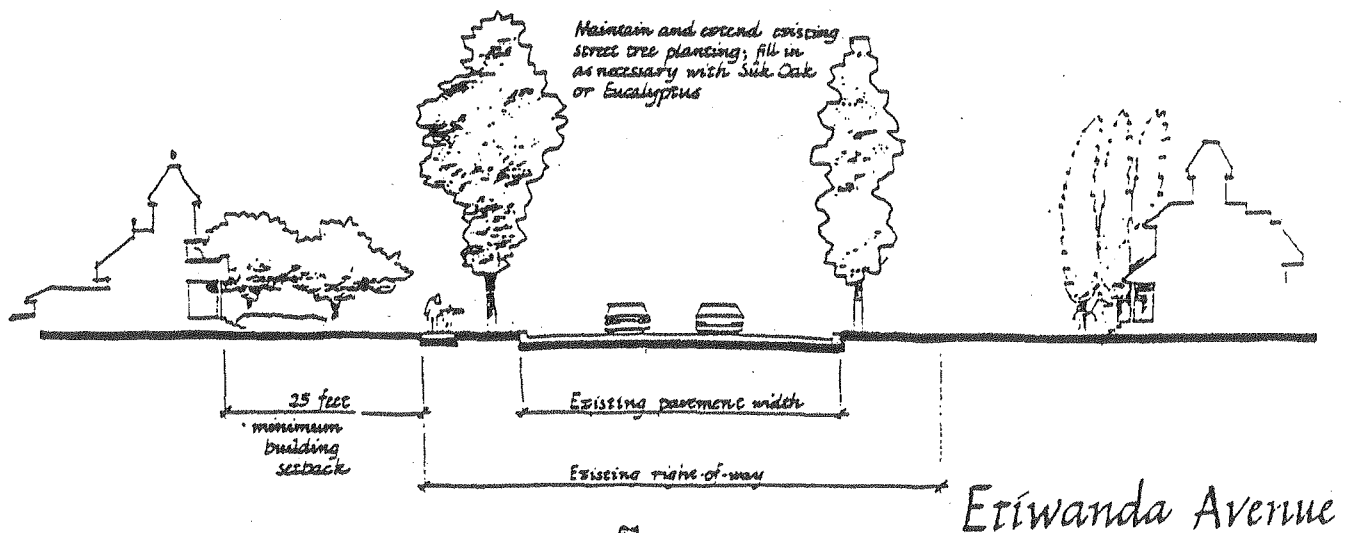
Victoria Windrows

THE VILLAGE OF VICTORIA WINDROWS

Victoria Windrows includes all the land between Highland, the railroad tracks, Etiwanda Avenue and Day Creek. This Village now contains some of the Eucalyptus windrows that were planted when this area was an important citrus raising area. The pastoral character of Etiwanda Avenue has also been identified as an important issue relating to the development of this area. Victoria Windrows will protect and enhance the quality and character of its eastern edge, and simultaneously build its own identity related to the whole planned community.

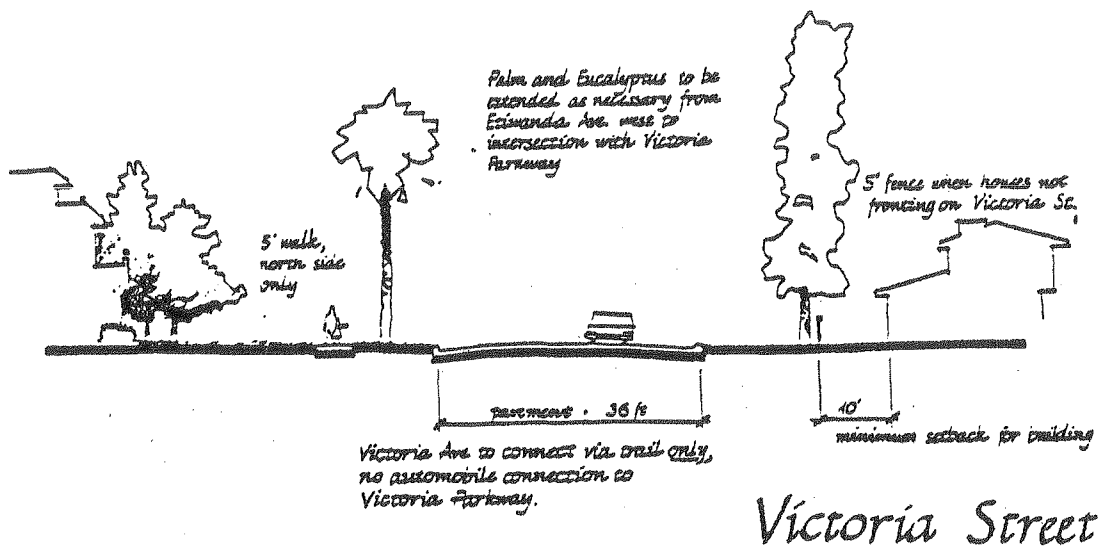
THE VILLAGE EDGES

The arterials adjacent to the Village, Baseline and Highland, will be treated as in Victoria Groves and Victoria Vineyards. The Day Creek edge is shared with Victoria Vineyards. The special character of Etiwanda Avenue calls for a special treatment that is illustrated in the cross-section below. To maintain the single family residential quality of this street, all new homes built on the street will have a single family detached, residential character, although they may contain one, two, or three residential units. The existing tree planting of Silk Oaks, Palms and Eucalyptus should be augmented to fill in the planting gaps that now exist.



THE VILLAGE ENTRY

Victoria Street ends in a cul-de-sac before entering the Planned Community. From the west and south the Village Entry is through Victoria Parkway. The central Village open space, Windrows Park, is located where the Parkway bends south and intersects the extension of Victoria Street. It has an established palm-lined character which will be continued into the Windrows Village by a community trail. A typical proposed cross-section is shown below.



WINDROWS PARK

Windrows Park is located at the western edge of the existing windrows. The central feature of the park will be a lake surrounded by informal tree plantings and lawn. A path along the lake edge will allow pedestrians to walk around and through the park. Informal picnic facilities would also be an appropriate use on the lake edge. Activities in the park will be compatible with the character of this park. Park buildings will also compliment the park's character and provide an opportunity to establish a distinctive style, such as a Victorian or period building theme.

EDISON CORRIDORS: MIXED USE OPEN SPACE

The two Southern California Edison Corridors contain a significant amount of land within Victoria Windrows and Victoria Lakes. The intent of this plan is to encourage mixed use of these corridors for the benefit of the entire community and, when possible, to shield direct views of the transmission towers.

To accomplish these objectives, several solutions are proposed. Along roadways and on property lines abutting the corridors, a "windrow style" of planting will shield long views of transmission towers. The "windrow style" of planting is a linear planting of tall growing trees with a vertical growing habit and foliage that grows down to the base of the tree. When planted close together, these kinds of trees will form an attractive visual "wall", screening the corridors with a style of planting that already exists in the planning area.

In some areas of Victoria Windrows, the corridor will act as a buffer between Day Creek Boulevard and the residential land uses to the east (see fold-out land use plan on back cover). The western corridor, adjacent to Day Creek County Flood Control land could easily accommodate equestrian, bicycle and pedestrian trails that connect to regional open space, though such a trail is not funded at this time.

Finally, these linear strips of land could accommodate any one of a number of agricultural uses that would not conflict with their use as transmission line corridors and add to the visual and actual quality of the environment. Among the uses that should be considered are: nursery stock farms, citrus orchards, vineyards, and turf farms. Provisions for individual private vegetable gardens would also be appropriate, especially next to the higher density residential areas.

SCHOOLS AND CHURCHES

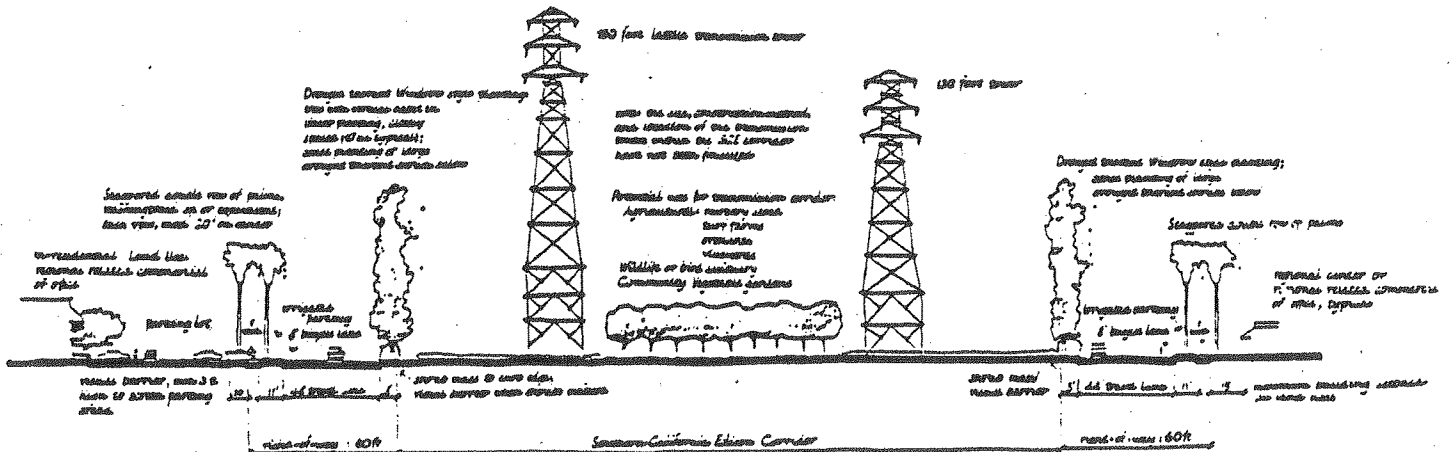
Adjacent to the park will be an elementary school. As in Victoria Groves and Victoria Vineyards, the school will share its playfields and recreation facilities with the park. A church site next to or with the school can share the school's parking. This plan encourages joint use of school facilities such as playfields, gymnasium, classrooms or the multipurpose room, or day care facilities.

COMMUNITY TRAIL SYSTEM FOR THE WINDROWS

The trail system will function in this Village as it does in the other Villages, connecting residential neighborhoods to one another and to the public open spaces. Where possible in the Windrows, the trails should be adjacent to the existing Eucalyptus Windrows. This will permit more convenient maintenance of the Windrows, and give the trails a pleasing character that will be extended throughout the Victoria trail system.

A NEW ARTERIAL ROAD TO PROTECT ETIWANDA CHARACTER

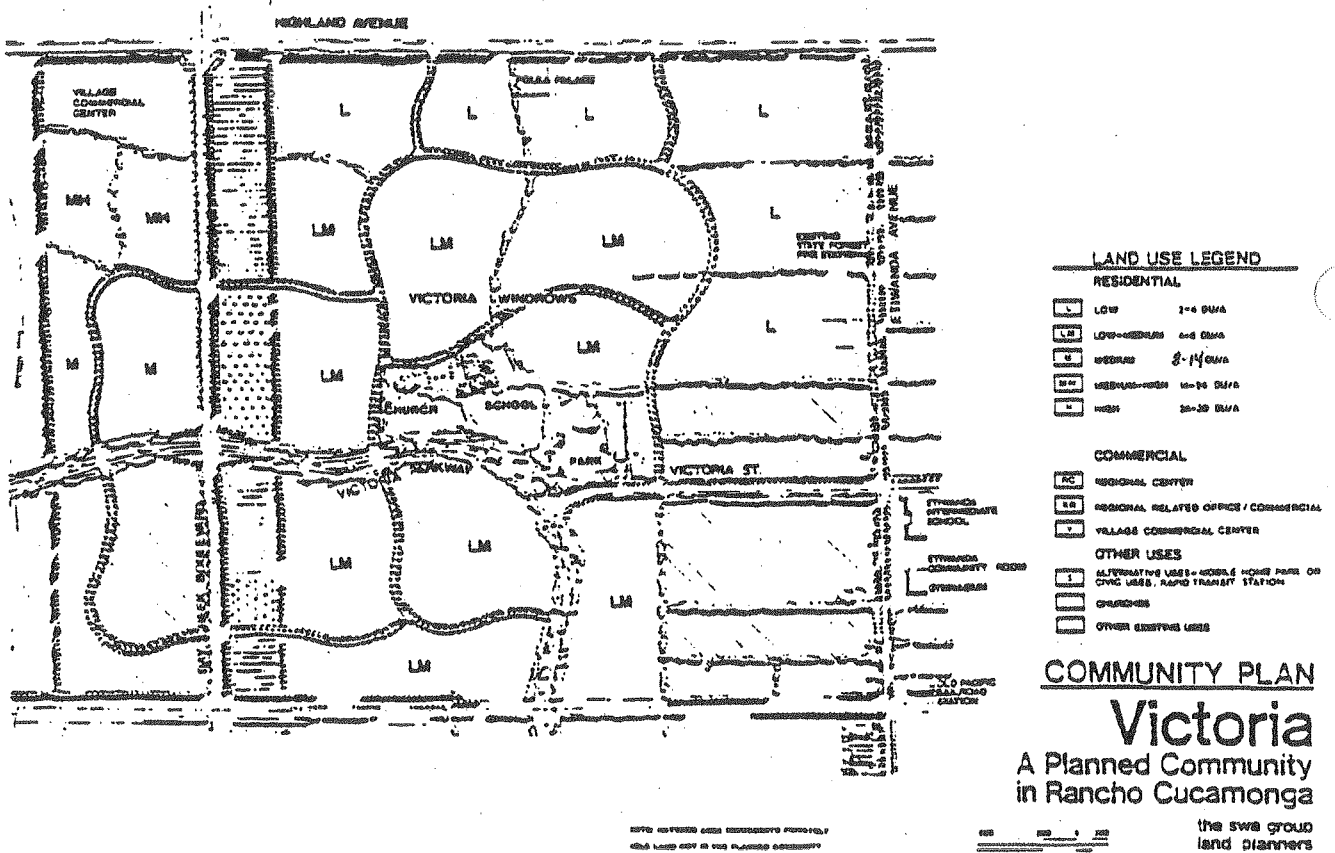
A new north-south arterial road serves Victoria Windrows on the western edge of the eastern SCE corridor. This north-south arterial will be called Day Creek Boulevard and will move regional traffic through Victoria to the regional center and/or to the freeways. A full interchange with the proposed (but unfunded) Foothill Freeway will direct regional traffic to this road, rather than to Etiwanda Avenue. Where this arterial road abuts a residential area, an edge treatment will be provided to provide visual relief. The proposed cross-section below shows a typical condition along Day Creek Boulevard in the Windrows. Also included in the section is the adjacent utility corridor and the kinds of uses that may be present there.



RESIDENTIAL HOUSING

Residential development in Victoria Windrows may include all of the types of homes found in Victoria Groves and Victoria Vineyards. Low Medium ("LM"), Medium ("M"), and Medium-High ("MH") Density residential land use categories will provide a variety of residential opportunities for residents of Victoria Windrows.

VICTORIA WINDROWS



see amended map on pg. 39

SPECIAL LAND USE PRESERVES ETIWANDA AVENUE CHARACTER

Along the Etiwanda Avenue edge is a Low Density ("L") residential land use type called "Windrow Residential". This land use may include detached and attached residences in a density range of 2-4 dwelling units per acre. Along Etiwanda Avenue itself, 1/2 to 1 Acre lots are proposed. Attached homes may be permitted on these lots, provided the buildings have the character and quality of large single family residences, with large green lawns and trees, and a generous setback from Etiwanda Avenue. Areas not fronting on Etiwanda Avenue will contain, but are not limited to, single family dwellings on large lots (8,000 square feet and larger), or clusters of detached or attached homes planned so that the density does not exceed 4 units/acre. The objective is to preserve the quality of the open space created by the windrows.

MOVE-ONS PERMITTED IN VICTORIA WINDROWS

To promote a varied and interesting community and to provide a place to locate older homes of sound construction, existing dwellings may be moved into Victoria Windrows or Victoria Lakes within the Low Density ("L") residential land use areas. It is the intention of this plan to encourage the preservation of architecturally noteworthy buildings in the community. Individual houses moved into Victoria should be significant architectural specimens of any period or style, and homes of the Victorian Era are particularly encouraged.

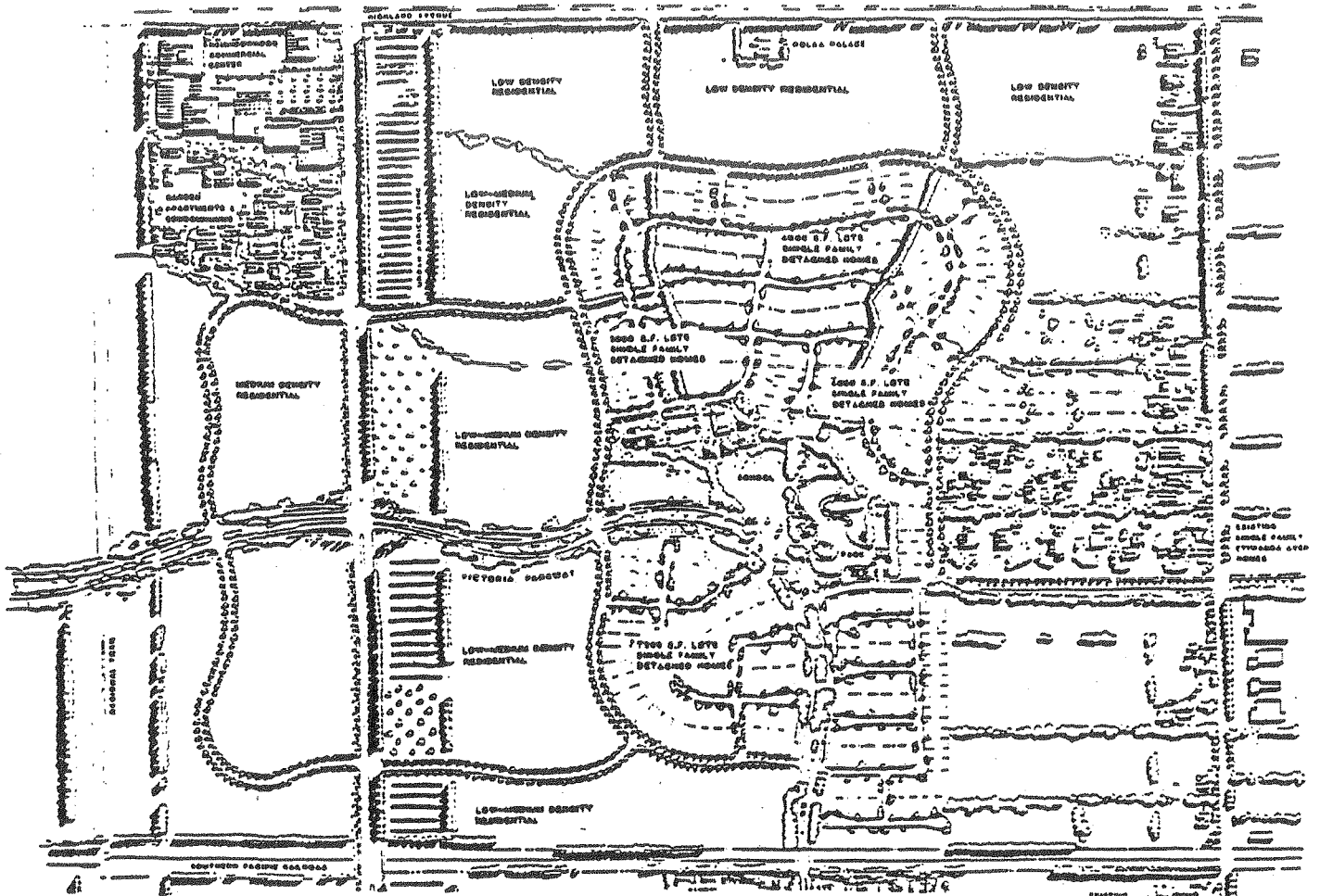
VILLAGE COMMERCIAL CENTER

Local commercial needs in the Windrows will be served by a Village commercial center at Highland and Day Creek Boulevard. Its location on two major arterial roads is

convenient to village residents as they enter or leave the community by automobile, and is also accessible to bicycles and pedestrians via the community trail system.

The architectural theme that is used for the Village Commercial Center should draw upon the character of the older Victorian homes of the Etiwanda area for inspiration.

On the following page is an illustrative diagram of the Windrows area. Using the land use diagram as a framework, many of the residential, commercial, and open space uses have been illustrated to show an example of the quality and character of each type of development proposed. This plan is an illustrative example of typical development patterns in Victoria Windrows and should not be considered as a specific site plan proposal.



VICTORIA WINDROWS

see amended map on pg. 39

Victoria Lakes

THE VILLAGE OF VICTORIA LAKES

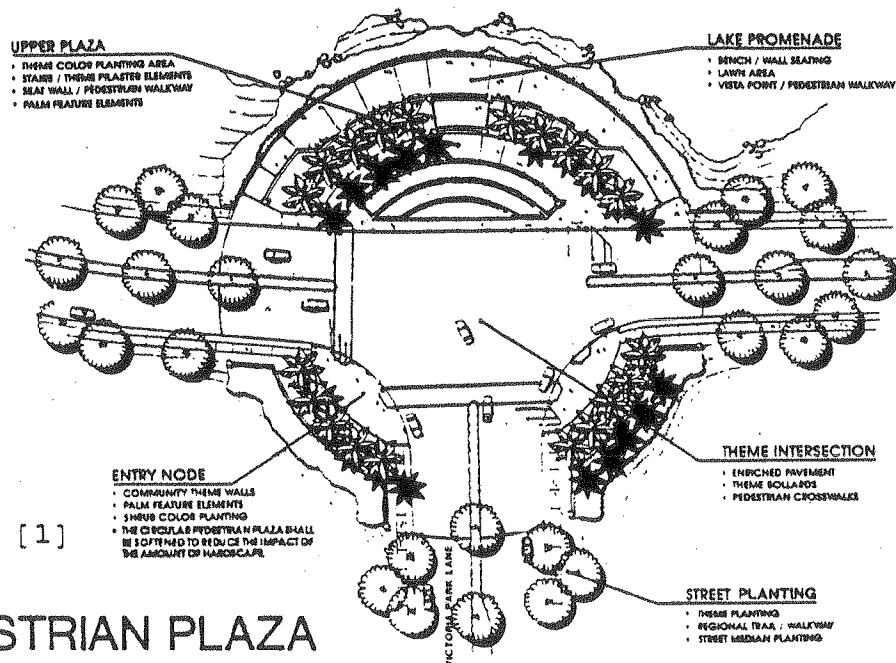
A UNIQUE WATER-RELATED URBAN COMMUNITY

The character of Victoria Lakes is formed by the unique relationship of the residential, commercial and recreational land uses surrounding the lakes which become the village's central open space. The intent is to create a high quality, water related community, with an active, people-oriented water edge that serves the residents of the Victoria Lakes Village, the entire community of Victoria, and the City of Rancho Cucamonga in general.

VILLAGE ENTRY

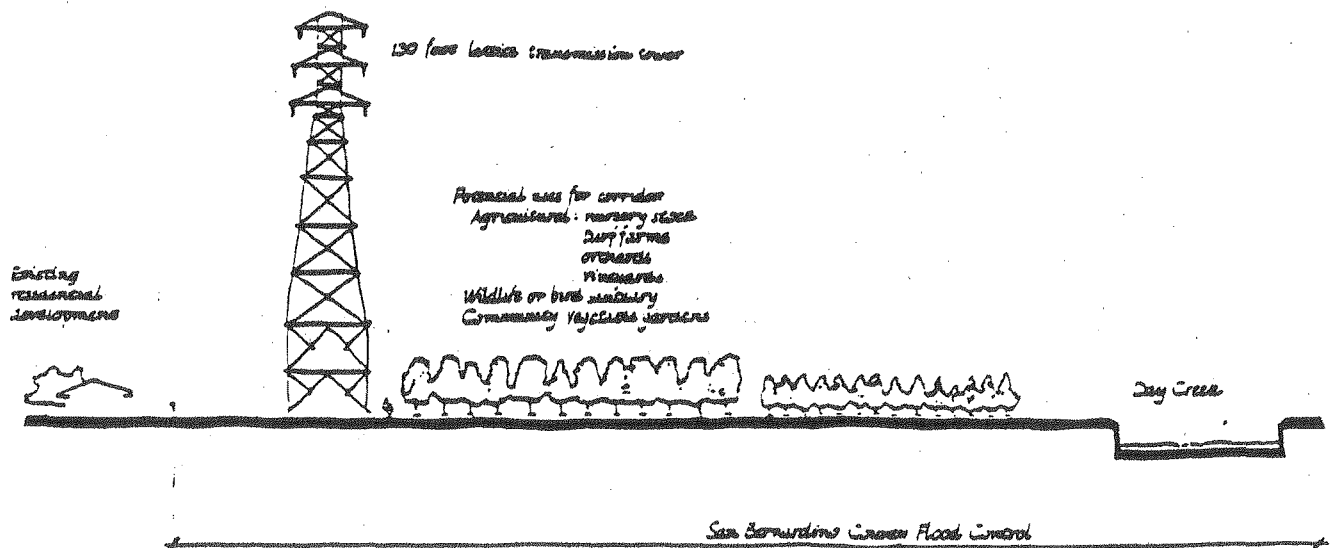
The most gracious entrance to residential portions of the Village is on Victoria Park Lane at Baseline. North of Baseline, Victoria Linear Park's landscape character and design is similar throughout its entire length. South of Baseline, however, Victoria Park Lane takes on a more formal character, with regularly spaced trees. Victoria Park Lane terminates at a large circular pedestrian plaza at the north end of North Victoria Lake. Victoria Park Lane terminates at a large circular pedestrian plaza at the north end of North Victoria Lake. There will be a lake overlook at this point, a destination where pedestrians and bicyclists can gather and look across the open vista created by the lakes and lakes' edges. A conceptual plan of the pedestrian plaza is shown below:

[1] This circular pedestrian plaza shall be softened to reduce the impact of the amount of hardscape.



THE VILLAGE EDGES

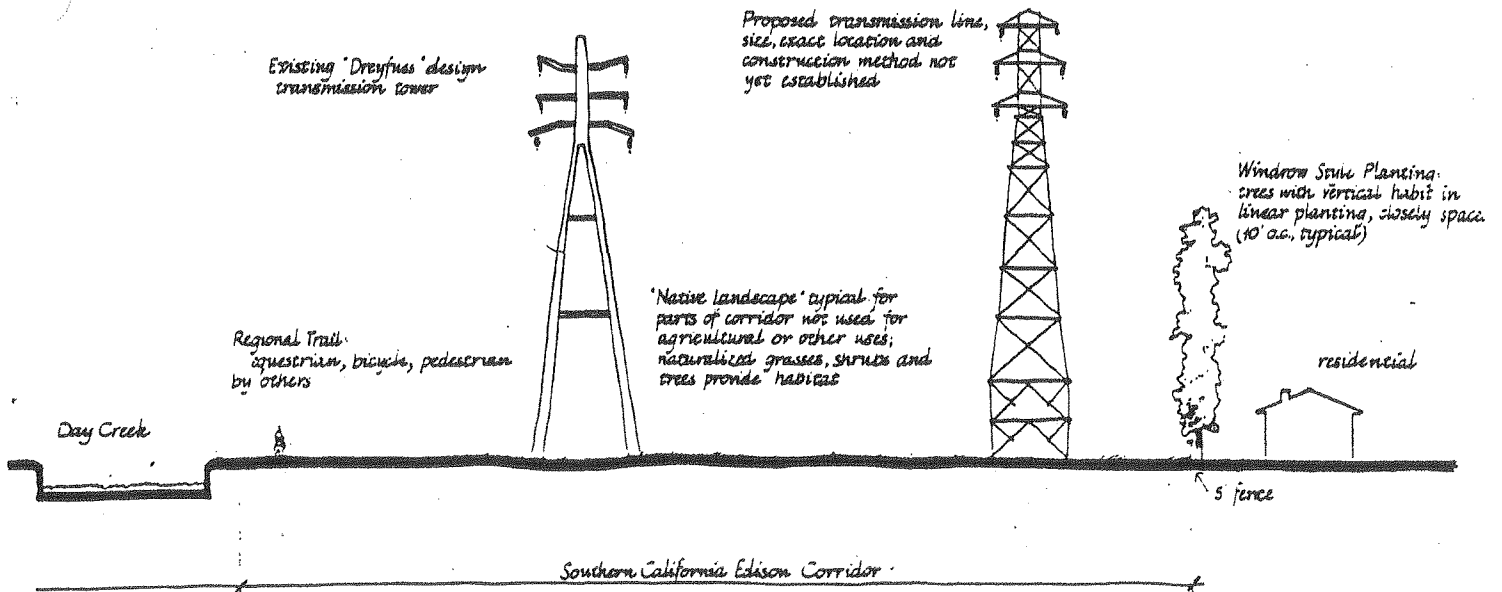
The Village edges are the railroad tracks on the North, Etiwanda Avenue on the East, the Devore Freeway on the Southeast and Day Creek on the West. The edge treatment for Etiwanda Avenue and the railroad tracks have been discussed previously. The proposed treatment of the Day Creek right-of-way and the adjoining Southern California Edison corridor is shown in below. Because of the excellent visibility of the southern half of the Village from the Devore Freeway, most of the land adjacent to the freeway will be occupied by the regional shopping center and related commercial facilities. This serves the needs of the major department stores for freeway visibility and access, and the needs of the residential community for buffering from the freeway.



Day Creek / SCE

VICTORIA LAKES LINK THE RESIDENTIAL AND COMMERCIAL COMMUNITY

The lakes proposed for this Village will provide the visual and land use connection between the residential, commercial and recreational facilities. [2] Some of the uses which may be provided at the lakes include fishing, model boat sailing and paddle boating pending Planning Commission and/or City Council approval. Because of the change in grade, there will be a series of terraced lakes, each large enough to create a dramatic visual impact. An illustrative drawing depicts how the lakes might be arranged to accomplish the intent of this plan is shown on Page 77. North Victoria Lake is bordered by single-family residential to the east and a public park to the west. Central Victoria Lake provides lake frontage to multi-family residential to the west, single-family residential to the east, the park to the northwest and multi-family to the southeast. South Victoria Lake is situated between multi-family residential land uses and regional related uses.



PUBLIC PARK AT NORTH VICTORIA LAKE

Along the western edge of North Victoria Lake, a public park facility will be located. This park will serve the entire community and provide uses compatible with the character of the lake. Lake edges will be public to provide maximum access to this special park. A conceptual plan of the park and lake is shown on page 76a.

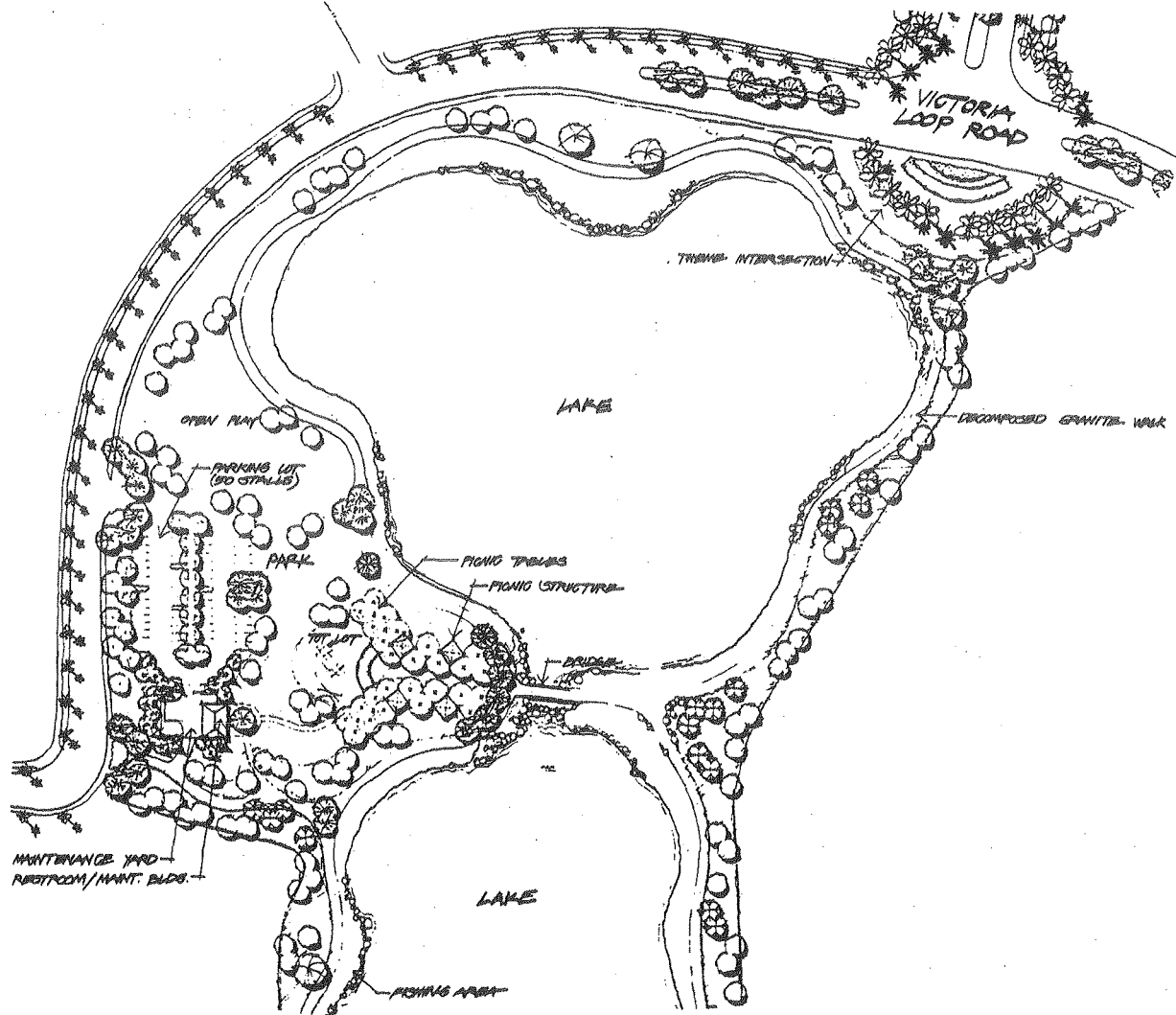
REGIONAL RELATED USES ON LAKE EDGE

The regional related uses adjacent to South Victoria Lake would be compatible with both the residential community to the north and the regional center to the south. Some commercial activity, such as restaurants, etc., would be appropriate in this area. Additional residential uses, should they be necessary, would also be appropriate on the lake edge.

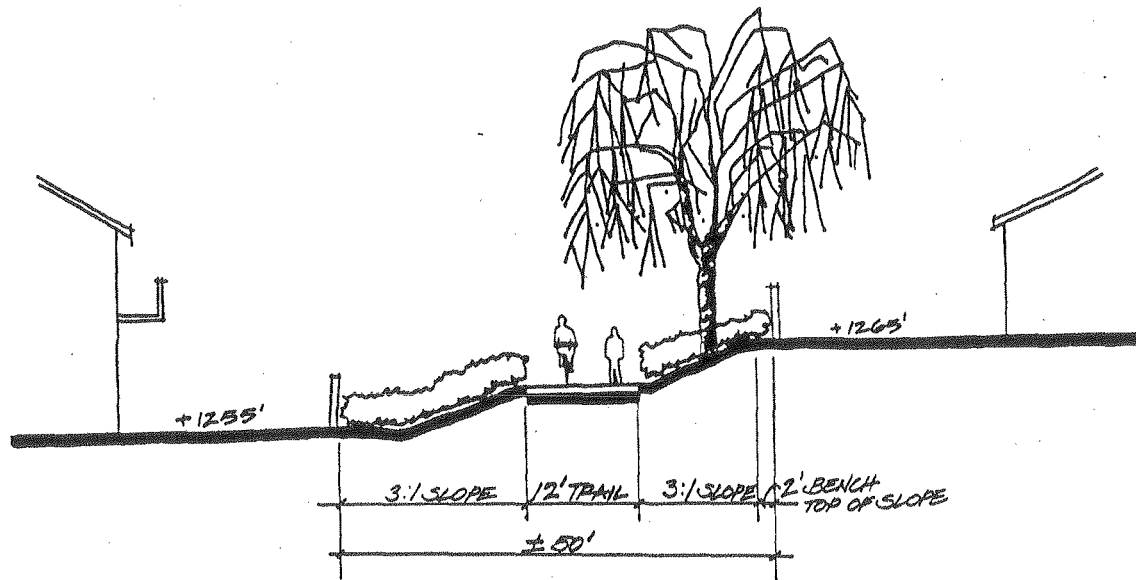
TRAILS FOLLOW LAKE SYSTEM INTO HEART OF THE REGIONAL CENTER

[3] The park and trail system of Victoria Park Lane shall continue around both sides of all three lakes down to Miller Avenue where it is expected to cross Miller and continue into the regional center. The quality of the edge, that is, whether it appears as a natural lake edge or an urban water edge may vary. [3] However, the lower lake edge shall be designed with a more urban water edge, such as bulk-heads. The Natural Lake Edge cross-section shows a typical eight-foot trail and edge condition at the lake in the residential areas along North Victoria Lake.

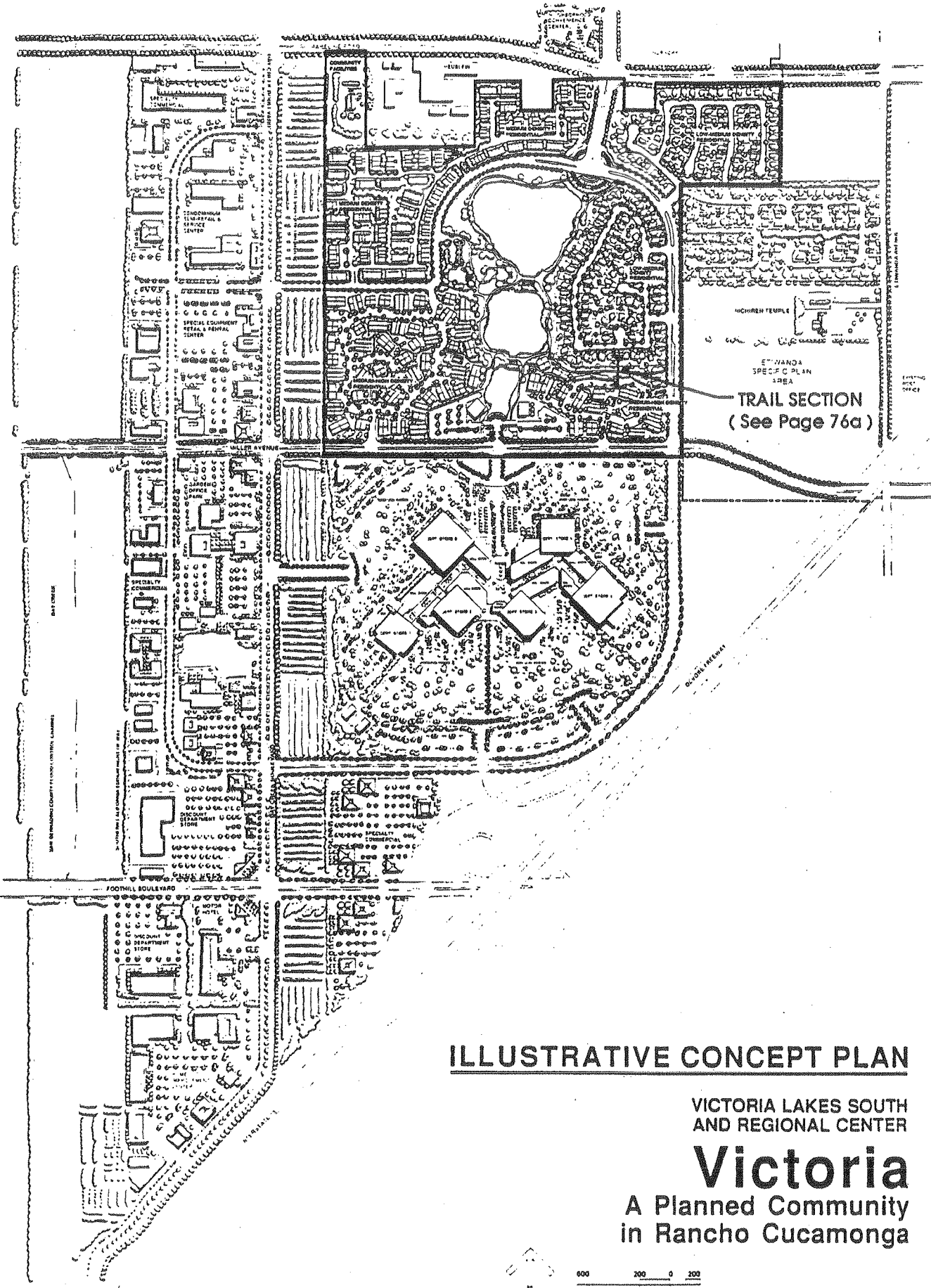
A cross section depicting a portion of the trail system that connects the lake area to Victoria Loop Road between the residential areas east of the lakes is shown on page 76a.



PARK / LAKE CONCEPT



TRAIL SECTION
(See Page 77)

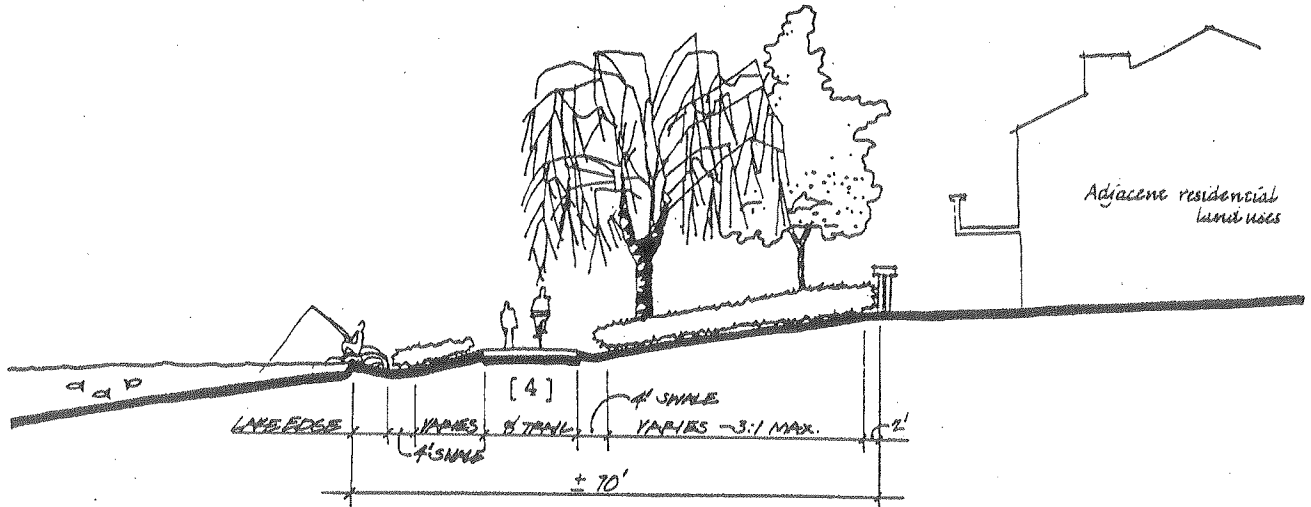


ILLUSTRATIVE CONCEPT PLAN

VICTORIA LAKES SOUTH
AND REGIONAL CENTER

Victoria
A Planned Community
in Rancho Cucamonga



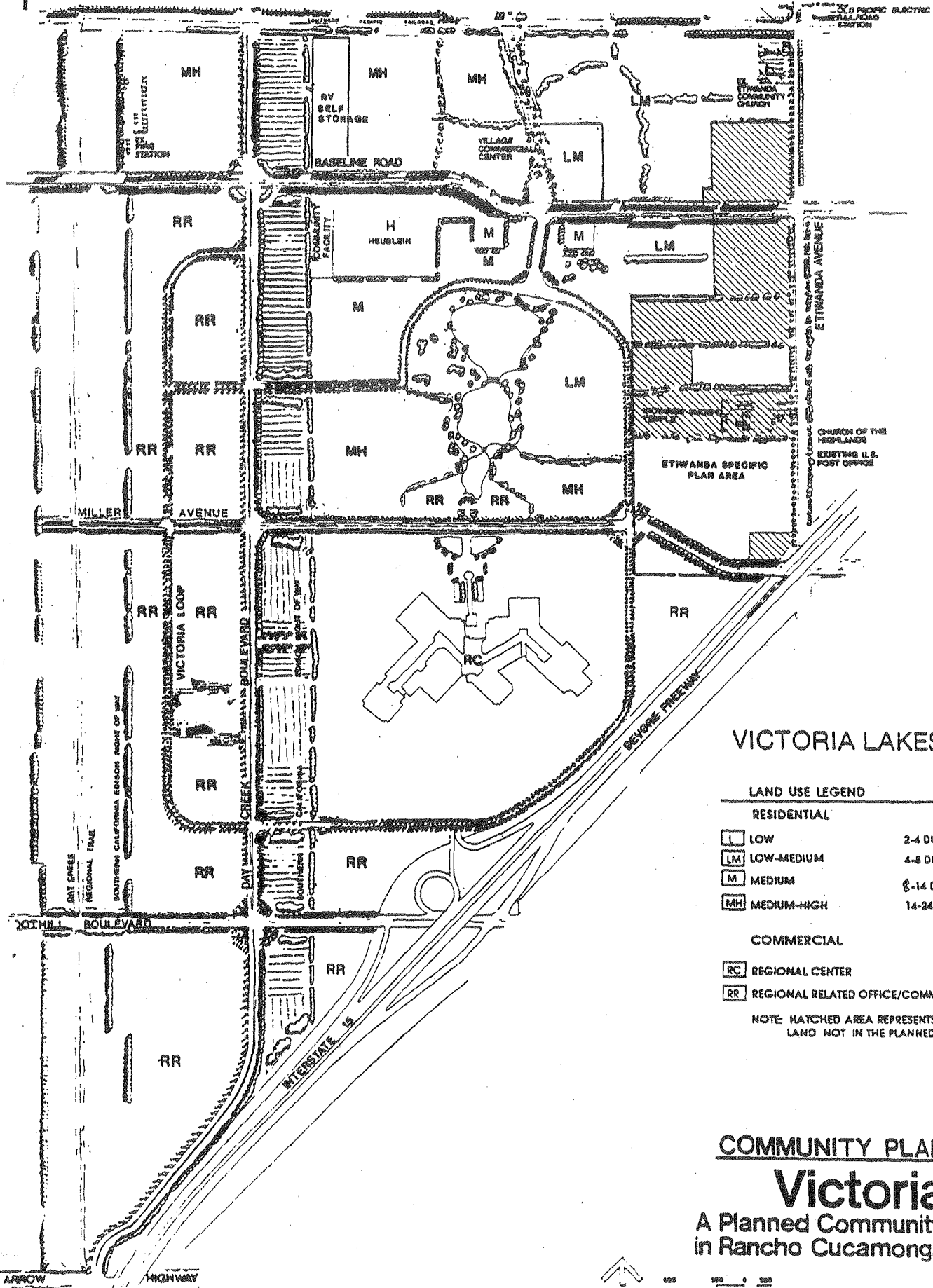


Lake edge to be open to public use, and to connect to Estuanda community and other parks via trail system.

Natural Lake Edge

RESIDENTIAL LAND USE IN VICTORIA LAKES

The residential land uses of this village will be consistent with the character of the place that is being proposed. Residential densities will range from Low-Medium to Medium-High in this village. The Low-Medium residential (LM) designation allows from 4 to 8 dwelling units per acre. The Medium-High Density residential land use category (MH) includes housing products in a range of 14 to 24 dwellings per acre. Because of their location on the lake edge these dwellings will be of quality character and highly desirable, matching their unique setting.



VICTORIA LAKES

LAND USE LEGEND

RESIDENTIAL	
L LOW	2-4 DU/A
LM LOW-MEDIUM	4-8 DU/A
M MEDIUM	8-14 DU/A
MH MEDIUM-HIGH	14-24 DU/A

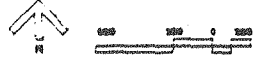
COMMERCIAL	
RC REGIONAL CENTER	
RR REGIONAL RELATED OFFICE/COMMERCIAL	

NOTE: HATCHED AREA REPRESENTS PRIVATELY HELD LAND NOT IN THE PLANNED COMMUNITY

COMMUNITY PLAN

Victoria

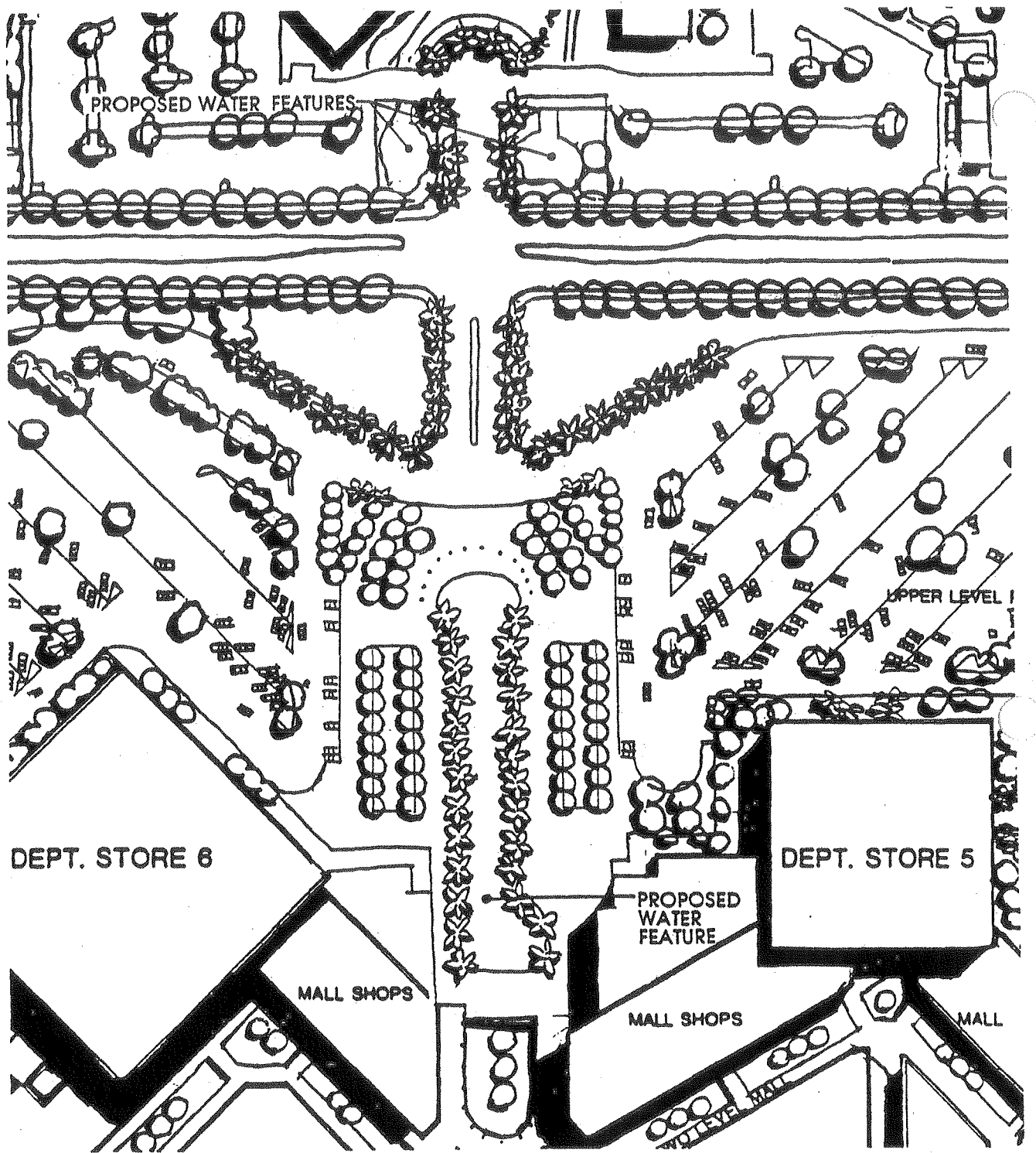
A Planned Community
in Rancho Cucamonga



REGIONAL CENTER IS URBAN FOCUS FOR VICTORIA

The regional center will be the commercial focus of Victoria and of the region (which extends beyond the City of Rancho Cucamonga to include western San Bernardino County). It is important that this center fulfill its role as the active terminus of the community-wide open space system, and as an outstanding regional commercial facility. The array of department stores and commercial activities that typically fill a regional center will be a great credit to the City of Rancho Cucamonga, both in the image it creates for the city, and in the revenue it provides through sales taxes collected by the regional center merchants. The pedestrian plaza and trail system that connects the center to the community will make this development a unique place that will function as the civic plaza for Rancho Cucamonga, and where many public functions can occur.

On the following page is an illustrative diagram showing the character of the center's north entry that will make this place exciting, people-oriented and active. This entry to the regional center is conceptually designed to function in concert with the entry to the Regional Related uses directly north across Miller Avenue. The potential for two specialty restaurants at this location would include water features flanking their entry. The trail system will have direct, convenient access to the center from the expected specialty restaurant uses.

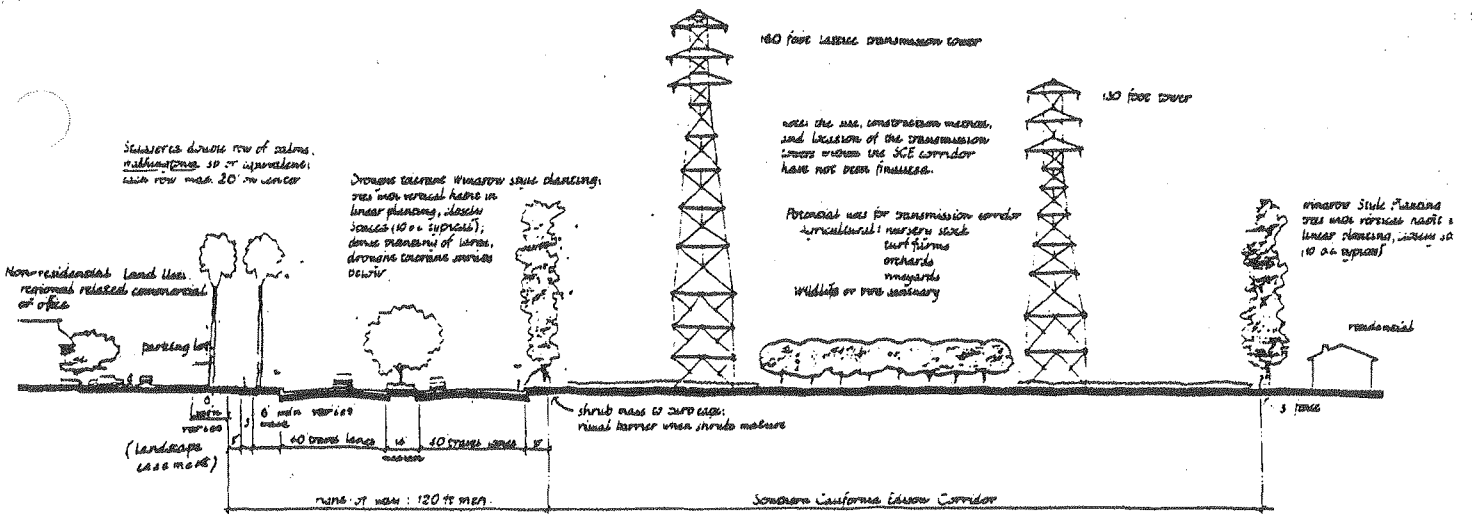


Regional Center - Character Sketch

A SPECIAL ENTRY TO THE REGIONAL CENTER AREA

The main automobile entry into the center will be from the Devore Freeway and Foothill Boulevard. There is an opportunity to create a special entry into the regional center.

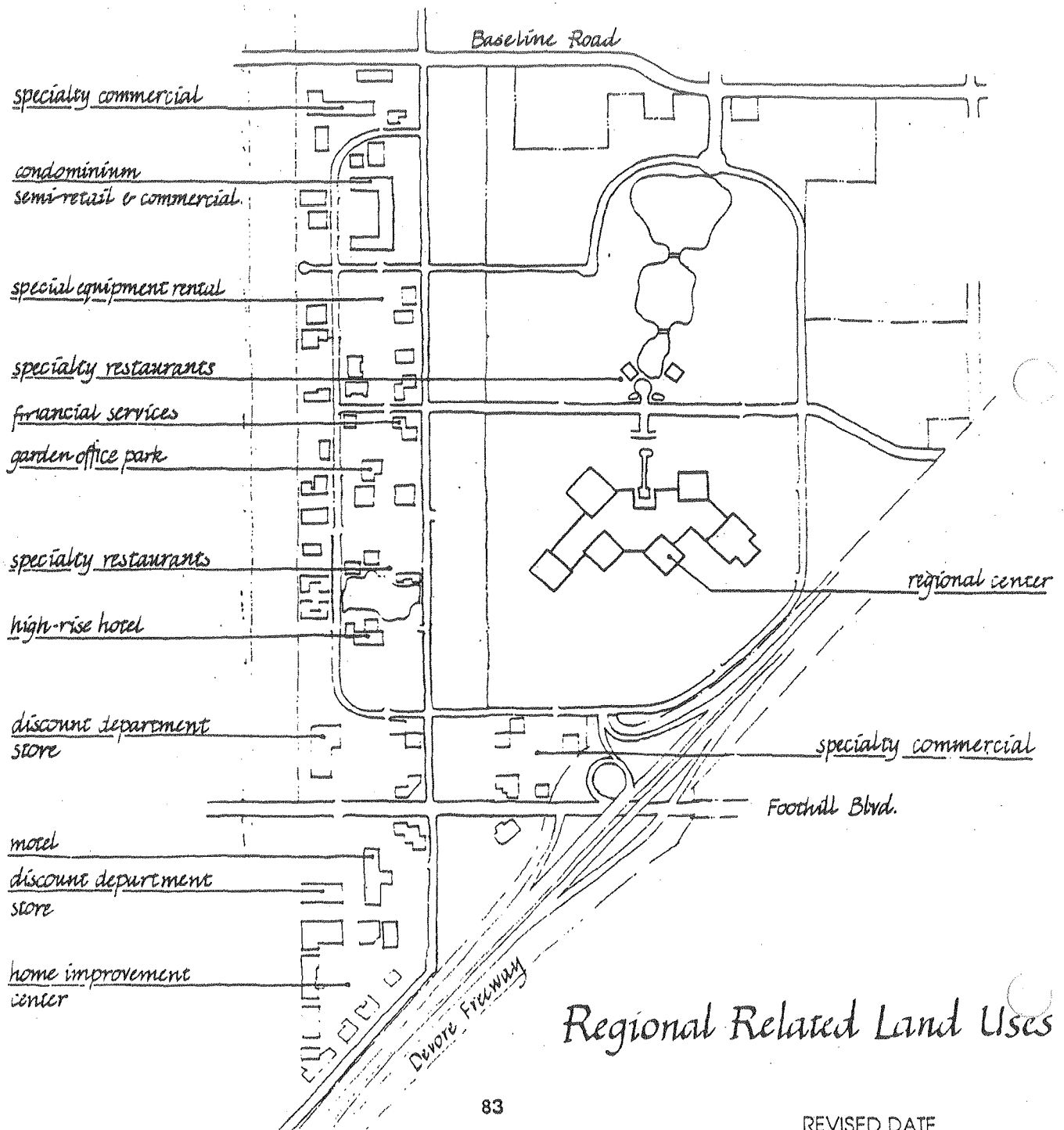
The drawing below is a typical cross-section of Day Creek Boulevard, located west of the center, showing the kind of planting that will help achieve the appropriate entry landscape.



Day Creek Boulevard at Victoria Lakes

TYPICAL REGIONAL RELATED LAND USES

The category called Regional Related Commercial and Office Use includes those activities that typically orient themselves to a regional center, but, for various reasons, do not need to be contained within the center itself. Below is a conceptual diagram that shows how these various uses may fit into a coherent pattern. The illustration is not to be construed as a specifically proposed plan at this time.



The intent of this diagram and the following description, however, is to describe what a typical scheme might be like.

South of Foothill might be a large home improvement center including a lumber yard, nursery and do-it-yourself hardware center. Facing Foothill, north of the home improvement center the diagram shows a discount department store, and next to it, a medium size motor hotel. At the north west corner of the north-south boulevard and Foothill might be another discount department store, facing the boulevard. A high rise hotel would probably be surrounded by quality restaurants, banks, savings and loans or other uses desiring prime visible locations. Behind the hotel, and paralleling the western SCE corridor, smaller offices and commercial buildings could develop that do not require high visibility, such as a veterinarian's office, a T.V. repair center, an office equipment store, or other service commercial. At Miller and Day Creek Boulevard, typical uses might include various offices. At the intersection of Baseline and Day Creek Boulevard a small convenience commercial center for the Victoria Community might be established. Facilities here might include a locksmith, dry cleaners, and a hardware store. All streets and parking lots will be landscaped and will make provision for safe and efficient pedestrian and bicycle circulation. Each project will have a landscaped character rather than a "strip development" image. This will be accomplished by minimizing driveway entrances to major arterials, and landscaping the edges to buffer objectionable views of parking and service areas.

Future market demand may indicate a need for additional residential land uses within the Regional Related land use category. Such use will be permitted if it is clear that the amount of commercial land uses will not be absorbed over a reasonable period of time, or a concept of combined commercial and residential land uses can be accomplished.

VILLAGE COMMERCIAL CENTER MEETS LOCAL NEEDS

To provide for the convenience needs of local residents within Victoria Lakes, a small village commercial center should be centrally located. This center should contain those facilities that are not normally found within a regional center or its related facilities, such as a food market or drug store. To assure convenient access by pedestrians and bicycles, as well as automobiles, this center should be located at Victoria Parkway and Baseline.

COMMUNITY FACILITIES

The Fire Station on Baseline near Day Creek will serve the fire protection needs of the community. Should the existing railroad right-of-way become a mass transit corridor, a mass transit station could be located in the area between Baseline and the railroad tracks, west of Day Creek Boulevard. Any parking that could not be accommodated on this site could ideally be located in the SCE corridor just east of the road. A mass-transit tie to the regional center from this location would also be appropriate. This site would also be an ideal location for other community related facilities because of its central location and excellent access.

Should this area not be suitable for a rapid transit station or other community facilities, it should be developed with residential land uses. Any land use consistent with the Medium Density ("M") land use would be appropriate. In addition, mobile home parks will be permitted here, and should be developed according to the standards of the City of Rancho Cucamonga.

V

*SAFETY & HEALTH
TRANSPORTATION SYSTEMS
COMMUNITY FACILITIES
RESOURCE CONSERVATION*

INTRODUCTION

The Victoria Community Plan recognizes its place in providing for the needs of its citizens with respect to city and regional infrastructure, community services and facilities, transportation, conservation, safety and health. Many of the issues arising from the needs must be addressed at a city or regional level, and the plan proposes solutions which are compatible with the goals and objectives outlined for Victoria as well as the City of Rancho Cucamonga and the region.

Basically, this plan encourages creative and innovative solutions. Alternative methods of transportation are encouraged through the structure of the community trail and circulation system. The extensive use of drought tolerant plant materials will conserve resources. Shared use of public facilities will insure maximum community benefit from all open space and public buildings. Health and safety needs can be met efficiently because of the ease with which long term plans can be made for the entire planning area.

In all of the areas which involve coordination with city or regional agencies, it is the intent of this plan to encourage cooperation in providing for broad community needs, while respecting the unique goals, objectives and planning concepts developed for Victoria itself.

SAFETY AND HEALTH

One of the advantages of an overall community plan is that infrastructure can be planned in detail for the entire community at one time, avoiding needless duplication of services and their attendant costs that might occur with piecemeal development. In Victoria, community-wide planning for storm drainage, sewerage and water supply will provide these necessary services in an economic and efficient manner.

STORM DRAINAGE

Property in Victoria will be developed so that it is adequately protected from flooding. Providing protection can be divided into four parts:

1. Acceptance of upstream flows
2. Protection from a major flow outbreak
3. Internal site drainage
4. Downstream discharge

For each of these conditions there are several possible solutions, and each of these solutions will be examined to develop a plan which meets the needs of the community and conforms to criteria established by the city and county governments. The potential solutions to these problems are discussed more fully in the Draft Environmental Impact report (Draft EIR) that accompanies the plan.

SEWERAGE

As part of the Cucamonga County Water District (CCWD), Victoria will respect the standards and requirements of the CCWD Sewer System Master Plan (amended April, 1979). The more regional issue of additional treatment facilities is currently under consideration, and Victoria will align its facilities to conform with decisions that are made regarding this treatment facility. Should reclaimed water become available, its use for landscape irrigation or with the lake system will be considered and examined closely.

WATER

Victoria will also receive water from the CCWD, which has existing and proposed facilities that crisscross the planning area. The intent of this plan is to preserve and connect to existing facilities, and realign or reposition planned facilities as necessary to accommodate specific site conditions or road alignments. Water main sizes and pressure standards will respect requirements specified by the Foothill Fire Department and all new facilities will meet the requirements of the CCWD and the City of Rancho Cucamonga.

FIRE PROTECTION

Expansion of the existing facility at Baseline, east of Day Creek, will provide fire protection for all of Victoria. Its central location assures adequate response time, and planned water facilities will provide adequate water pressure for firefighting.

TRANSPORTATION WITHIN VICTORIA

ALTERNATIVE TRANSPORTATION METHODS EMPHASIZED

The planning objective of the Victoria Community Plan to develop a transportation system which provides adequate automobile circulation, while encouraging alternative means of obtaining access to the community. Accordingly, emphasis will be placed on the continuity of the trail and open space connections. The convenient location of usable open space, neighborhood commercial services and community facilities within each village will promote the use of the alternative transportation methods.

LOCAL TRAILS AND STREETS

At the neighborhood level, it is vital that connections be provided as shown on the Land Use Plan and the Trail System Conceptual Plan. Connections can be made by trails through detached, attached and multi-family residential projects, along roadways, through public utility corridors, or within the community park system. Local public or private streets will be designed to be in scale with the neighborhood environment - curving and tree-lined with cul-de-sacs to discourage through traffic.

LOCAL RESIDENTIAL ROADS FOR VILLAGE CIRCULATION

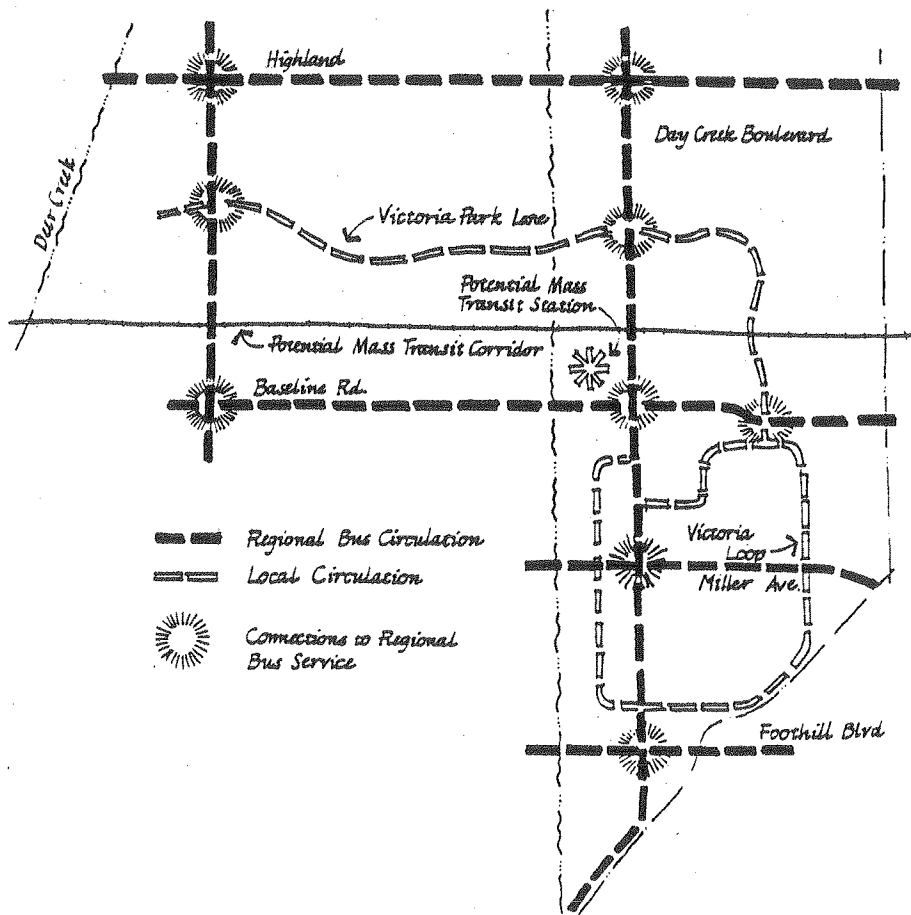
The local residential roads will provide village-level automobile connections to Victoria Parkway or external arterial roads. Their character will be enhanced by a regular street tree planting and curving alignment.

VICTORIA PARKWAY: A ROAD IN A PARK

Victoria Parkway is unique in its relationship to both the residential automobile circulation system and Victoria Linear Park. As its name suggests, Victoria Parkway is a road in a park. It also serves residential circulation, but this function must remain subordinate to its role within Victoria Linear Park. To achieve this, it is essential that the parkway be one lane of travel in each direction separated by a generous median. Victoria Linear Park will be used extensively by bicyclists, joggers, pedestrians and horsemen because of its park-like quality and the connections it makes to community facilities and open space. A high speed road that maximize automobile circulation efficiency at the expense of the Linear Park would not be appropriate in this context, and is not consistent with the objectives of this plan. Because of the importance of the Linear Park in linking Victoria together, the Design Criteria section of this plan contains many specific criteria for the design of Victoria Linear Park.

PUBLIC TRANSPORT AND MASS TRANSIT

Victoria will accommodate several levels of public transportation. At a local level, mini-bus service on Victoria Park Lane could provide efficient community access to the regional center and related commercial and office uses from the residential villages. City-wide bus service on arterial roads will assure adequate connections to city services and facilities. Should rapid transit facilities be built within the existing railroad right-of-way, the plan proposes a possible location for a rapid transit station at Baseline and Day Creek Boulevard.



SPECIAL BOULEVARDS TO PROTECT RESIDENTIAL NEIGHBORHOODS

Special boulevards are arterial roads that will be designed to move regional automobile traffic efficiently and safely, but which will also be attractive aesthetically, with medians and landscaped edges. Bicycle trails will be part of the roadway and will assure convenient city-wide bicycle circulation. These roads will be at the edges of the villages and will protect residential neighborhoods while providing needed regional circulation.

ACCESS TO THE REGIONAL CENTER

Modifications to the existing Devore Freeway/Foothill Boulevard interchange will be proposed to provide acceptable levels of service to the regional center and the surrounding commercial and office uses as the specifics of the center are developed.

COMMUNITY FACILITIES

The Victoria Community Plan will provide community facilities to meet community needs. These facilities will be convenient to village residents and will minimize dependence on facilities outside the community.

PARKS AND OPEN SPACE

Parks and open space have been integrated into the community to form the character and identity of each individual village, as well as Victoria as a whole. This will be accomplished by locating park and community facilities at the center of each village, connecting the residential areas to the central facilities via an extensive trail system, and then connecting each village open space by Victoria Linear Park. Village Parks in residential areas will be large enough to accommodate facilities for use by all village residents. Placing parks next to school playgrounds and facilities will also maximize efficient use of community open space.

The lake system will serve as the open space for the residential, commercial and office spaces that surround them, and as an integrative open space element for all of Victoria. This is assured by the public lake edge, which will be accessible to all, and the sequence of land uses on the lake which will create an exciting transition from the residential land uses to the north, and the regional commercial center at the southern terminus.

In addition to the open space provided by Village parks, Victoria Linear Park, and the Victoria Lakes open space that is part of the flood control or Southern California Edison will serve many community needs. Where appropriate, regional trail connections can be made through the Day Creek

or Deer Creek flood control channel right-of-ways. Edison corridors can provide land for agricultural uses such as turf farms, nurseries, orchards, or community gardens. Flood control land and public utility corridors will otherwise remain in a natural condition, providing habitat and refuge for native wildlife.

SCHOOLS

Schools have been planned for Victoria to complement and reinforce the open space system. They will be located close to the village parks in the residential neighborhoods, and access by foot or bicycle via the trail system is assured. Joint use of school facilities for broad community needs will be encouraged to make the schools a hub of activity for village activities.

CHURCHES

In addition to the several sites that exist within the core planning area, this Plan has proposed several new sites. They are indicated on the Land Use Plan. Other arrangements are also encouraged for religious groups including:

- a. Shared facilities at local schools. Though this is not a suitable permanent solution, it can serve temporary needs.
- b. Shopping centers or adjacent to shopping centers, including Village Commercial, Regional Related and Regional Center, are permissible locations for churches. They can share use of the commercial uses parking during the off-hours of Sunday morning.
- c. Condominium churches permit joint use church complexes and a reduced land cost for church groups. Such innovative uses and plans are encouraged within the Planned Community.

CONSERVATION OF RESOURCES

Within the Planned Community many elements have been structured to provide for the conservation of resources. The following discussion summarizes how this is accomplished.

USE WATER FOR PLANT MATERIALS WHERE THE COMMUNITY BENEFITS MOST

Water will be conserved wherever possible by using low maintenance or drought tolerant plant material. For example, Victoria Parkway will have a rustic, naturalized character created by informal plantings of trees, shrubs and grasses. This character exists now and is a conscious way of extending the naturalized landscape to create a strong community identity. Allowing the grasses and shrubs to establish a naturalized character will preclude the necessity of watering these plants heavily in the dryer months and will lower future maintenance costs.

Conserving water in naturalized areas will allow the use of water where it will be most effective for intense community use, such as in the parks and lakes. The parks, therefore, will have some lawn areas which will accommodate the many uses that require a well maintained lawn.

A LAKE SYSTEM CAN SAVE WATER

A properly installed and well maintained lake system also conserves water in several important ways. First, for lakes of the size proposed in this plan, the actual water use on a year to year basis is about the same or less as for an equivalent amount of carefully cultured open space. They will actually use less water than an equivalent amount of land in the same place used as a golf course. Second, a

properly designed lake will minimize percolation of water into the ground water table, so that actual net losses will only be due to evaporation. Third, the lake system could use recycled water, should it become available. The lake system is not used as a conveyance for project storm flows. During storm conditions, North and Central Victoria Lake inflows from direct precipitation over the water surfaces and the surrounding lake edges can overflow and spill into South Victoria Lake from which flows are directed to Day Creek. Finally, a body of water of the size proposed by this plan will have a greater impact as a unique environmental amenity than an equivalent amount of well irrigated open space, and at the same time could result in a net water savings. In the case of Rancho Cucamonga, a lake solution is the most resourceful way to develop an interesting and viable community center.

EXTENSIVE TRAIL SYSTEM ENCOURAGES USE OF BICYCLES AND PEDESTRIANS

Trail systems work best where they connect the community residents to the places they use most. The trail system in Victoria is extensive and will provide the connection that will be most useful to its residents. Direct access is provided to local parks and schools, to village commercial centers, to regional trails and open space, and to other villages. On a regional level, connections will be made to the community's major commercial centers, to the developing industrial center and to neighboring Terra Vista. A Community Trail is planned along the north side of Miller Avenue that will connect the village to the greenway trail in Terra Vista. This system will encourage bicycles and pedestrians thereby reducing automobile traffic and improving air quality.

PUBLIC TRANSPORT ENCOURAGED IN VICTORIA

The Victoria Community Plan recognizes the energy savings possible from the use of public transport facilities. At a local level, bus routes will be conveniently located to serve public needs. At a regional level, the plan shows a potential mass transit station location at Day Creek Boulevard and the existing railroad line. Should this corridor become a mass transit route, this location could serve Victoria and the entire community.

PASSIVE SOLAR ENERGY CAN SAVE ENERGY

Site planning to take advantage of passive solar energy will be encouraged in Victoria. The kinds of plant materials and their location, window exposure, roof overhang, and building alignment should be manipulated to maximize the "free" energy the sun provides daily.

In those places where "active" solar collectors can be used, access to the sun's radiation should be preserved and maintained in accordance with AB 2321 and AB 3250 (1978).

ARCHITECTURAL DESIGN CRITERIA TO CONSERVE RESOURCES

Within individual residential units, this plan encourages the use of devices that conserve resources. Shower heads that save water and low water flush toilets are already available. These devices are inexpensive and easily installed. As other comparable devices become available and prove themselves to be cost efficient conservers of resources, their use within Victoria is also encouraged.

VI
STATISTICAL SUMMARY

STATISTICAL SUMMARY

PLAN EVOLVES THROUGH TIME

The purpose of this section is to explain how residential density and the amount of acreage devoted to the land uses proposed can work together to make the Victoria Community Plan a reality. As the plan is presented here, the optimum residential yield is 8,255 dwelling units, or approximately residential dwelling units per acre within the planned community.

SUMMARY OF LAND USE ACREAGE

The following acreages represent the proportions devoted to each land use as shown on the Land Use Plan.

These calculations are based on planimeter readings and are subject to corrections based on an accurate survey.

LAND USE ACREAGE

<u>1. Commercial</u>	<u>Acres</u>
Regional Center (including lakes within Regional Center area only)	100
Regional Related Commercial	200
Village Commercial Centers	35
<u>2. Community Facilities</u>	
Schools	40
<u>3. Parks, Lakes and Community Trails</u>	
Village Parks (The Groves, The Vineyards, The Windrows, including lake in Windrows Park)	40
Lakes and Lake Edges (Village of Victoria Lakes only, not including lakes in Regional Center area)	20

Victoria Linear Park (not including paved area of Victoria Parkway)	35
Community Trails	5
4. <u>Unimproved Open Space</u>	
Southern California Edison Corridors	170
Day Creek Channel; Flood Control Basin	125
Southern Pacific Right-Of-Way	25
5. <u>Roads</u>	
Milliken, Highland, Baseline, Foothill, Day Creek Boulevard and paved area of Victoria Parkway (pavement width)	120
7. <u>Residential Land Uses</u>	<u>955</u>
Total Planned Community	1870
Land Within Planning Area Not A Part of Planned Community	280
Total Planning Area Acreage	2150

APPROACH TO RESIDENTIAL DENSITY

The Land Use Plan for Victoria contains six different kinds of residential land use designations. As described in the plan, each residential land use designation will contain a variety of housing types at various densities that are compatible with that land use designation. Generally, the types of housing products allowed within a given land use area will be in a specific density range. These density

1

ranges are summarized in the following chart:

RESIDENTIAL DENSITY RANGES

Low Density

Single Family attached and detached homes on large lots, or clustered to maximize the effective use of open space

2-4 Dwelling
Units Per
Adjusted Gross
Acre

Low Medium Density

Single family attached and detached homes on lots varying from 3000 SF to 7200 SF

4-8 du/A

Medium Density

Attached single family homes, including duplexes, fourplex/condominiums, townhomes (front and rear loaded) and condominiums. Detached homes may be possible at the lower end of the density range.

8-14 du/A

Medium High Density

Multifamily homes with common open space

14-24 du/A

High Density

Multifamily homes; typically a 3 story building with covered and uncovered parking

24-30 du/A

THE OPTIMUM YIELD CONCEPT

Although a density range is a useful tool in determining the upper and lower quantity of residential units proposed for any land use, it is misleading to use as the precise guide

in determining the actual number of dwelling units within any specific village or land use category when both the city and its developers must commit a certain level of expenditure to implement the plan. Therefore, the method of dealing with density in this plan is based upon a realistic understanding of units which are necessary to provide the amenities described, the infrastructure needed, and the demands of the housing market. This number of units, or optimum yield, has been established for each village as an optimum number of units for that area based on the residential and community uses within that village. This planning approach provides density control, allows future flexibility, and establishes a realistic allowance for planning infra-structures. The key is to provide enough units of housing to assure housing needs can be met from local employment growth at prices locally employed workers can afford.

A REALISTIC APPROACH TO RESIDENTIAL DENSITY

Several reasons point to the optimum yield approach: First, simply using the density range as a multiplier for each respective land use acreage produces a numerical range of dwelling units that is artificially skewed on both high and low ends. The high end number of dwelling units assumes that every land use category could be developed at the maximum density allowable. This does not reflect the intention of The William Lyon Company nor market needs which must be met over a long period of time. Initially, lower

density will be needed most, but as the area matures, the majority of housing required to meet the City's needs will have to be met by attached housing. The low number of dwelling units is also unrealistic in that it would force prices too high, and thereby not allow the developer to meet the need for housing generated by the expanding local employment base. The optimum yield approach proposes an optimum number of units based on a sound understanding of what could reasonably be built on this site, retaining benefits both for the City of Rancho Cucamonga and the developers over a long period of time. This assumes a balance between short and long term needs for housing: in the short term, a majority of detached houses; in the long term, a majority of attached ownership houses. The optimum yield allows for adequate infrastructure planning and commitments to open space and recreational facilities. Other amenities and necessities can be properly sized and space allocated. Second, there are actual housing types that yield specific residential densities within the density range of each land use category which are most realistic.

For example, category "LM", single family residential, includes products that yield from 4.5 du/acre to 6.8 du/acre. In determining the expected yield of dwelling units, these different products have been allocated throughout the "LM", category land use areas to provide a mix which represents today's best understanding of present and future market demand, and to provide the mixture of housing opportunities specified in the Rancho Cucamonga Proposed General Plan.

HOW THE OPTIMUM YIELD METHOD WORKS

Using the system outlined above, a specific number of dwelling units has been established for each of the Villages and for the total community. Those numbers are shown on the Density Distribution Plan. Developing the indicated number of units for each Village makes possible the amount of open space and other amenities indicated by the land use plan and in the land use acreage summary.

OPTIMUM YIELD BY LAND USE

The optimum yield according to each land use is listed in the following chart:

<u>Land Use/Density Range</u>	<u>Acres</u>	<u>Optimum Yield</u>
"L" - Low Density 2-4 du/A	230	645 Units
"LM" Low Medium Density 4-8 du/A	370	2120 Units
"M" Medium Density 8-14 du/A	215	2395 Units
"MH" Medium High Density 14-24 du/A	100	1945 Units
"H" High Density 24-30 du/A	<u>40</u>	<u>1150 Units</u>
Totals	955	8255 Units

OPTIMUM YIELD AS A PLANNING TOOL

The optimum yield can be used as a planning tool by the city and the developers to determine the need and size of infrastructure, schools, community facilities, and public utilities. It can be used as the basis for projecting future population. Using an optimum yield of 8,255 dwelling units allocated in the manner described in this plan, the future population of Victoria can be

summarized as follows:

<u>Residential</u> <u>Category</u>	<u>Persons Per</u> <u>Household</u>	<u>Dwelling</u> <u>Units</u>	<u>Population</u> <u>Projection</u>
"L" Low Density	3.4	450	1530
"LM" Low Medium Density	3.4	2120	7208
"M" Medium Density	2.5	2395	5988
"MH" Medium High Density	1.5	1945	2917
"H" High Density	1.5	<u>1150</u>	<u>1725</u>
Totals		8255	20,031

OUTLINE OF COMMUNITY DESIGN CRITERIA

I INTRODUCTION

II VICTORIA PARKWAY

Basic Design Concepts: Overview

Planting Concept

Basic Grading Concept

Right-of-Way Width Criteria

Typical Intersection Conditions

Typical Edge Condition Cross-Sections

III LANDSCAPE STANDARDS

Introduction

Drought Tolerant Plants

Windrow Style Planting

Entry Hierarchy Plan

Plant Palette Plan

Typical Landscape Edge Condition Cross-Sections

Landscape Grading Standards

IV SOLAR ACCESS STANDARDS; SITE PLANNING STANDARDS

PART 2

I
INTRODUCTION

INTRODUCTION: HOW THE DESIGN CRITERIA ARE TO BE USED

The urban design criteria section contains the standards that will be applied as Victoria develops. The standards are in the form of brief concept statements, plan drawings that locate a special condition requiring a certain type of landscape treatment, cross-sections that show precise measured distances, cross-sections that show landscape plant materials, or plans that illustrate solutions to typical conditions. These typical plans and cross-sections represent most of the conditions that are likely to occur as the Victoria Community Plan is implemented, and should serve as a guide to city officials and developers. Over time, it is possible that situations may arise for which no standards are specifically stated. In this situation the short concept statements which accompany a similar standard or criteria should be used to develop a new standard. Part I of the Community Plan should also be referenced to see that the new standard conforms in concept with the total community plan goals and objectives.

II
VICTORIA PARKWAY
DESIGN CRITERIA

VICTORIA PARKWAY DESIGN CONCEPTS: OVERVIEW

Victoria Parkway is a key element in the overall plan for Victoria. It is the continuous open space element that ties the fragmented parcels of land that currently exist within the planning area into a coherent whole. For the residents of Victoria, it will be a regular part of their lives and a significant ingredient in the image they have of their home. Therefore, the actual design of Victoria Parkway is crucial.

The following pages contain the basic criteria that should be followed to create the kind of linear park open space that is proposed by this plan. The Basic Design Concepts: Overview, sets out in a general way basic design considerations. The sections on planting and grading apply to the whole parkway and should be followed closely. The varying right-of-way will contribute to the park quality by making the park edge more ambiguous. The criteria for where the right-of-way is narrow and where it is wide are found in the following section. The Typical Intersection Conditions can be seen in an overview and in blow-up sketches that illustrate in a general way what is proposed when other roads intersect the Parkway. The Typical Edge Condition Cross-Sections are also shown in an overview reference plan and in blow-up cross-sections that follow. The sections are referenced to the roadway and to the adjacent uses. For example, cross-section A represents a typical condition where the road is next to the bicycle trail and an "LM" land use. Specific criteria are given for when that condition occurs in the blow-up sections. These sections should be combined with the right-of-way width and intersection criteria to form the basic design constraints for the design of the Parkway.

VICTORIA PARKWAY Basic Design Concepts

LINEAR PARK TERMINUS

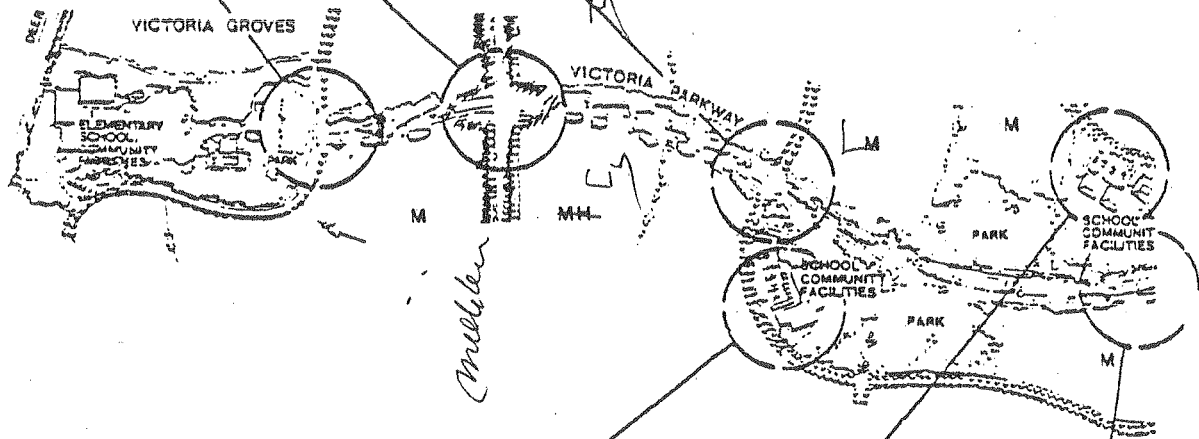
Peppertree grove in Victoria Groves park is visual terminus for Linear Park and Victoria Parkway

SPECIAL RESIDENTIAL ENTRIES

Special planting at Victoria Parkway and intersection with arterials marks entry to residential villages

OPEN INTERSECTIONS

Intersections should remain open; ornamental grasses, wild flowers, and shrubs should dominate. Where appropriate, open views should be framed by parkway planting.



BUILDINGS AT BACK OF OPEN SPACE

Park and school buildings should be located as far from Victoria Parkway as possible to maximize effective open space

SCHOOL ACCESS FROM LOCAL ROADS

Taking access to school and park facilities from local residential roads will reduce traffic congestion on the parkway

NARROW R.O.W. NEXT TO OPEN SPACES

Parkway right-of-way can be narrow when adjacent to open areas. Parkway planting should extend into open space whenever possible to increase perceived linear park area.

VIEWS INTO OPEN SPACE

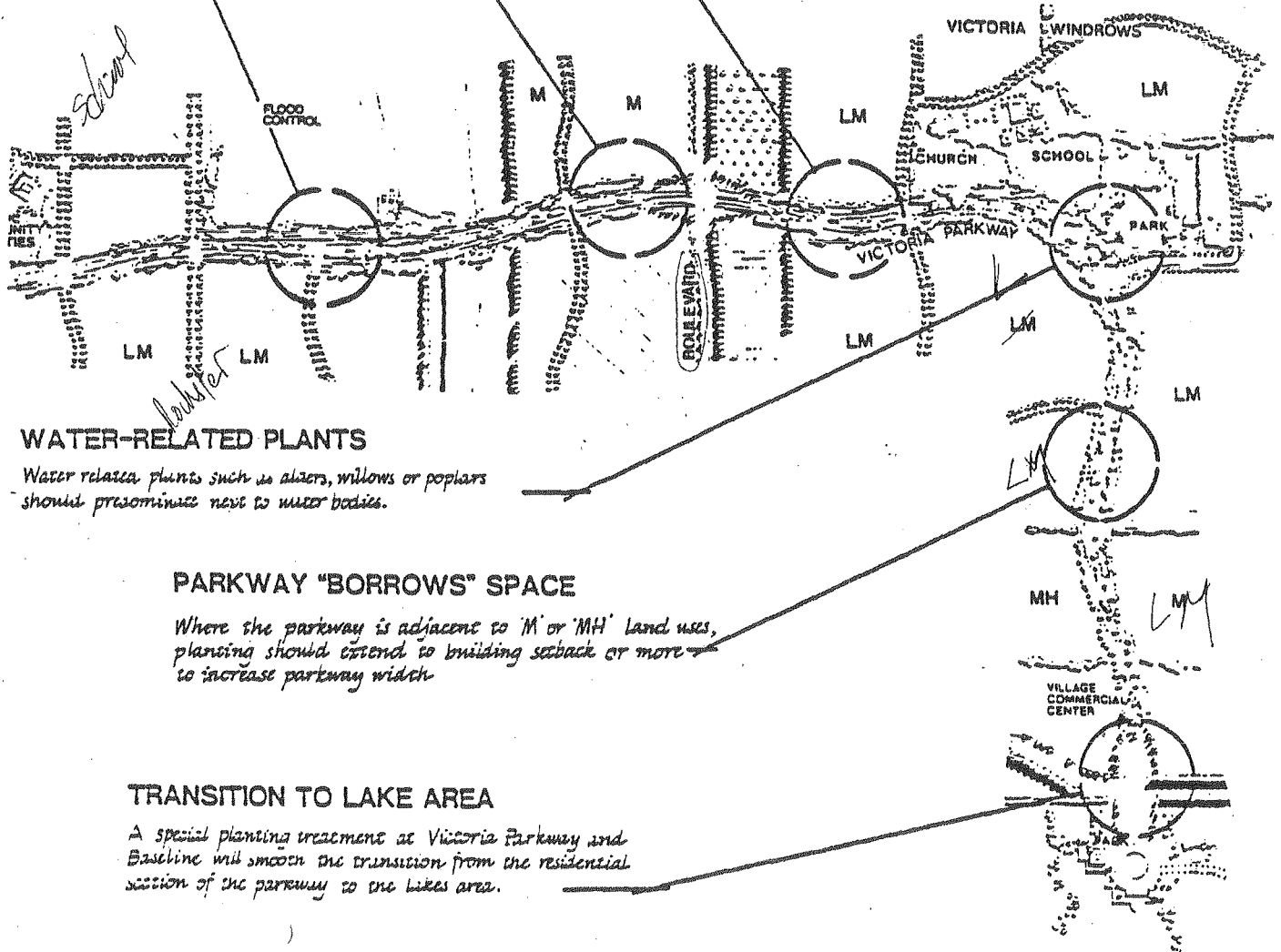
Parkway planting should open at parks and schools to frame long views into open space.

R.O.W. WIDENS AT RESIDENTIAL AREAS

Parkway Right-of-way should be widest at places where there are residential land uses on both sides of the parkway.

PARKWAY PLANTING

Parkway planting dominated by drought tolerant plant materials; at parks and open spaces, parkway planting to wrap into open areas to reinforce connection of Linear Park and adjacent open areas.



WATER-RELATED PLANTS

Water related plants such as alders, willows or poplars should preponderate near to water bodies.

PARKWAY "BORROWS" SPACE

Where the parkway is adjacent to 'M' or 'MH' land uses, planting should extend to building setback or more to increase parkway width.

TRANSITION TO LAKE AREA

A special planting treatment at Victoria Parkway and Baseline will smoothen the transition from the residential section of the parkway to the lakes area.

VICTORIA PARKWAY Planting Concept

Plan

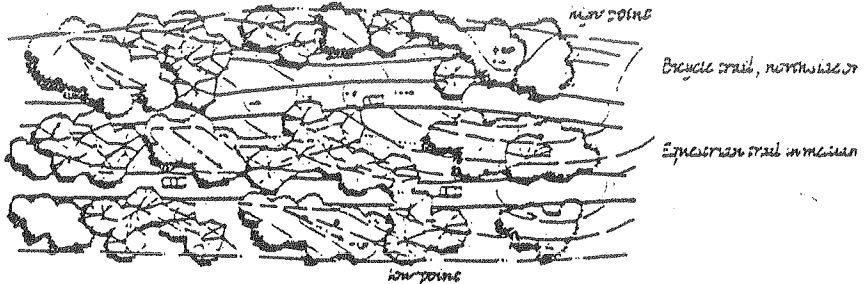
The plan shows a typical planting scheme for Victoria Linear Park. The basic concept is to create a naturalized landscape with the three basic trees and complementary shrubs and grasses. Irrigation is limited to overhead heads, or similar system in low areas or following drainage swales. Trees should be planted singly and masses for maximum effect. Landscape treatment should extend to crossing sections in M or MH land uses.



Sycamores are common to both under and pine areas. They should be planted in large masses, small groupings, or as single specimens.

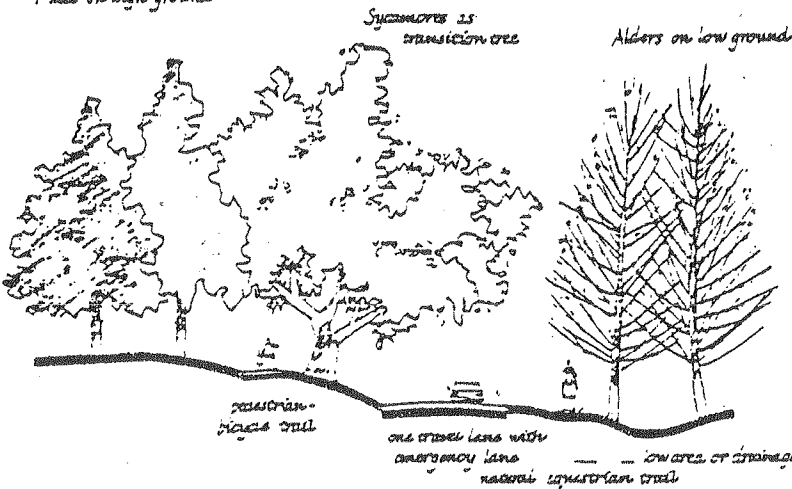


Pines should be used in large masses or smaller groupings in places perceived to be "high ground".



Alders follow natural drainage swales or low areas designed to simulate natural drainage swales.

Pines on high ground



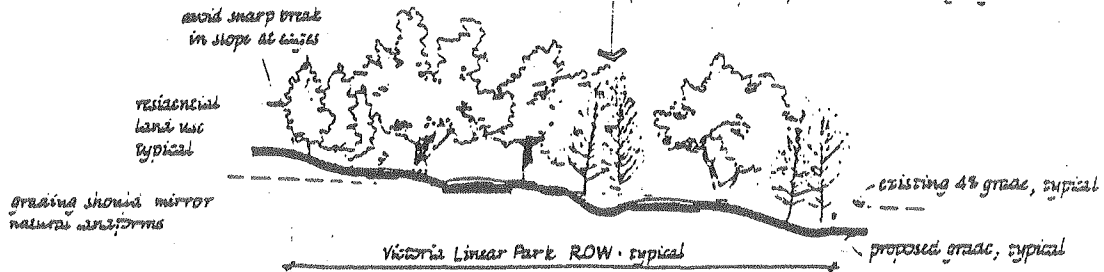
Section

Three trees form the basis for the parkway planting scheme: Alders, California Sycamores, and Pines. The alders should be planted in low areas or drainage swales to recall their natural habitats along stream banks. The pines should be drought tolerant species and should be planted on high ground. The sycamores are transition trees found in association with both the alders and the pines. They should be the most widely planted parkway tree.

VICTORIA PARKWAY Grading Concept

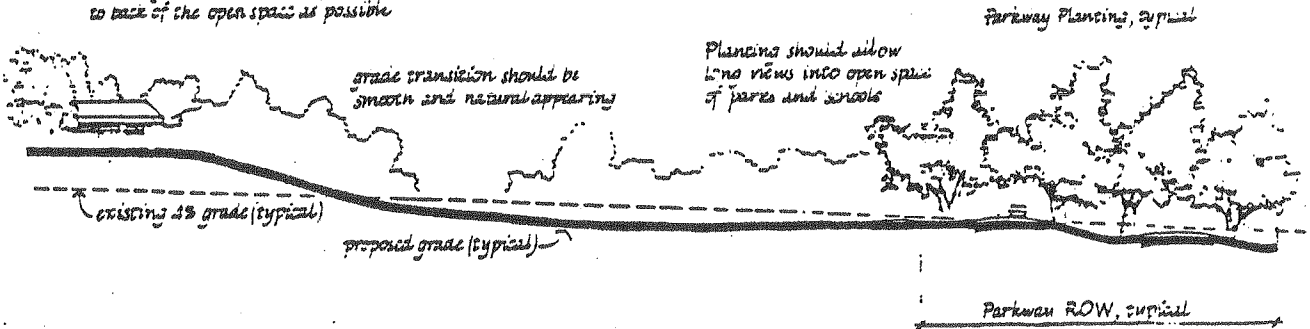
Use mass grading to maximize effect of grass channels within parkway right-of-way

drainage swales are design elements and should have a natural appearance, but need not function as part of the storm drainage system

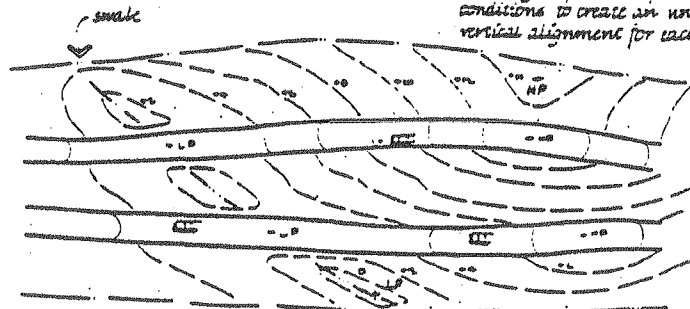


Cross grade at intersections should be determined by the importance of the intersection. At arterial roads, cross grade should be minimized; at feeder roads, some cross grade is acceptable or even desirable

Buildings should be set as far to back of the open space as possible



Roadways respond independently to grade conditions to create an unobstructed vertical alignment for each travel lane



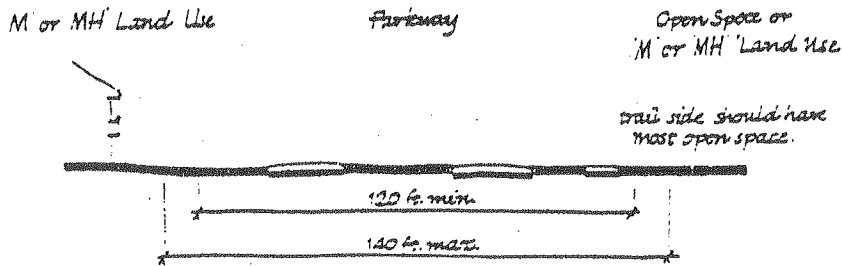
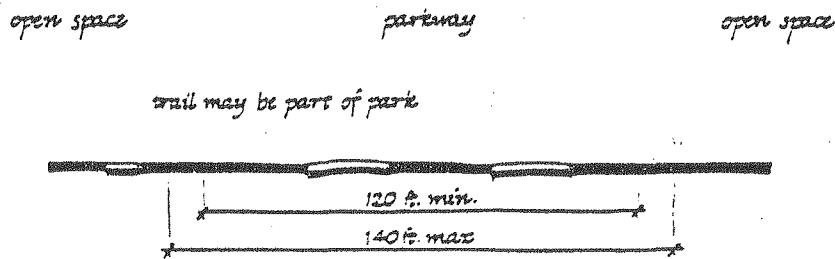
Natural appearing drainage swales cross driveway or parallel roadway and drain to provide interest and simulate habitats for birds.

VICTORIA PARKWAY

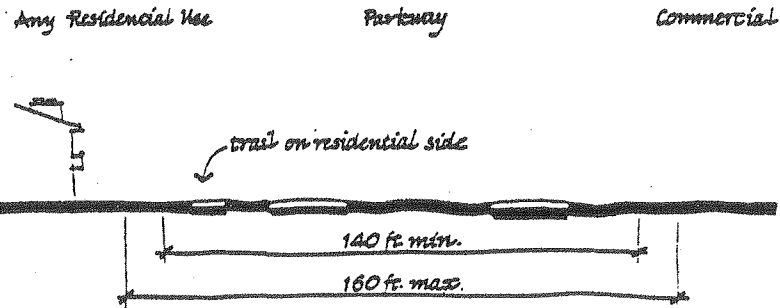
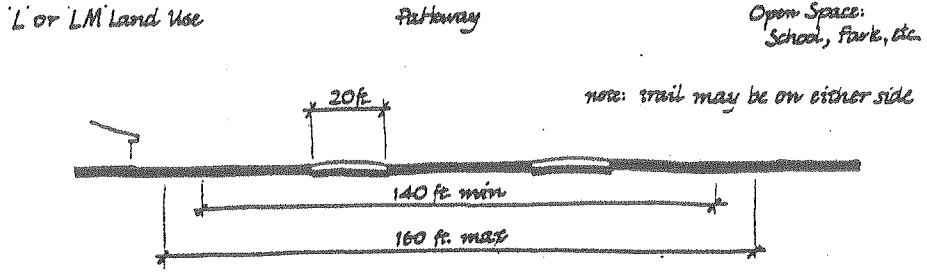
Right-of-Way Width Criteria

The cross-sections below can be used to determine the appropriate right-of-way width for the varying conditions that occur along the parkway. The right-of-way varies from 120 to 160 feet, depending on the land uses that occur on either side of the parkway. Based on those land uses, the parkway can be narrow (120 - 140 feet), medium (140 - 160 ft), or wide (160 - 180 feet). By examining the cross-sections, any stretch of parkway can be placed within one of the three categories. When designing stretches of the parkway, and using the right-of-way width criteria as a guide, smooth transitions should be made from an area where one criterion applies to another area where another section applies.

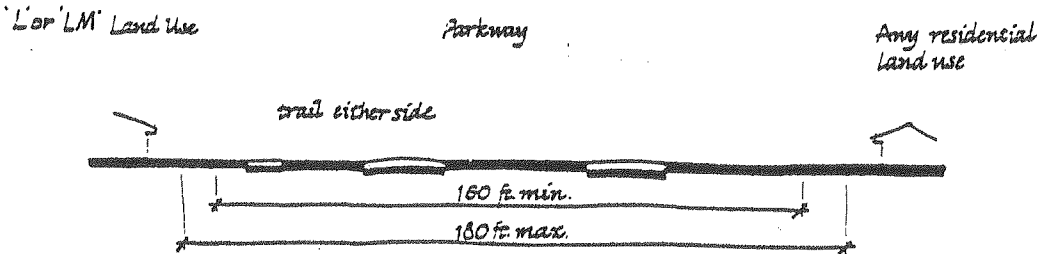
RIGHT-OF-WAY 120 - 140 FEET



RIGHT-OF-WAY 140 - 160 FEET






RIGHT-OF-WAY 160-180 FEET

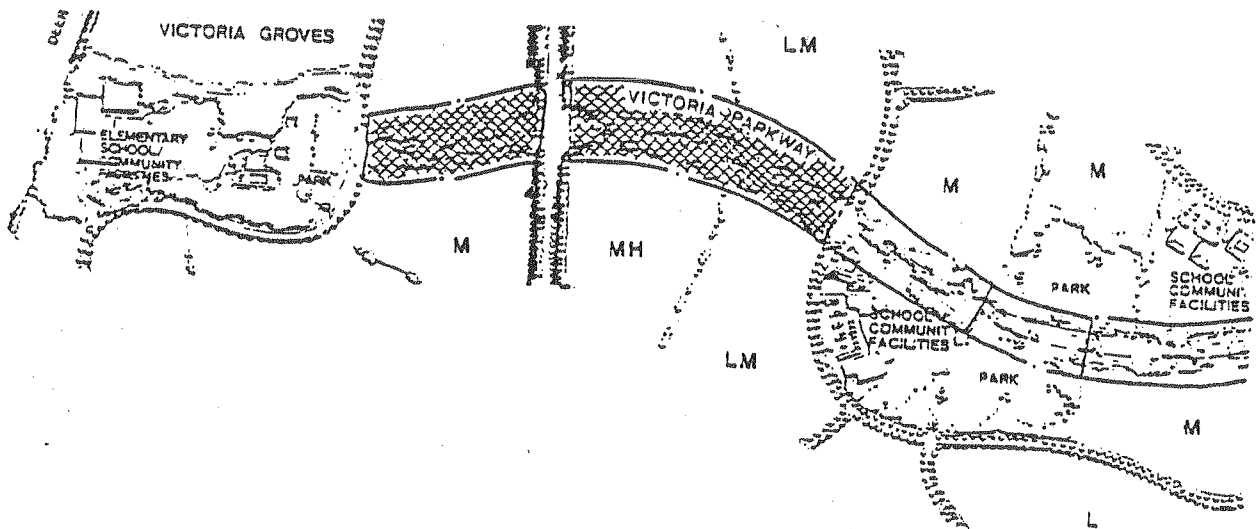


VICTORIA PARKWAY

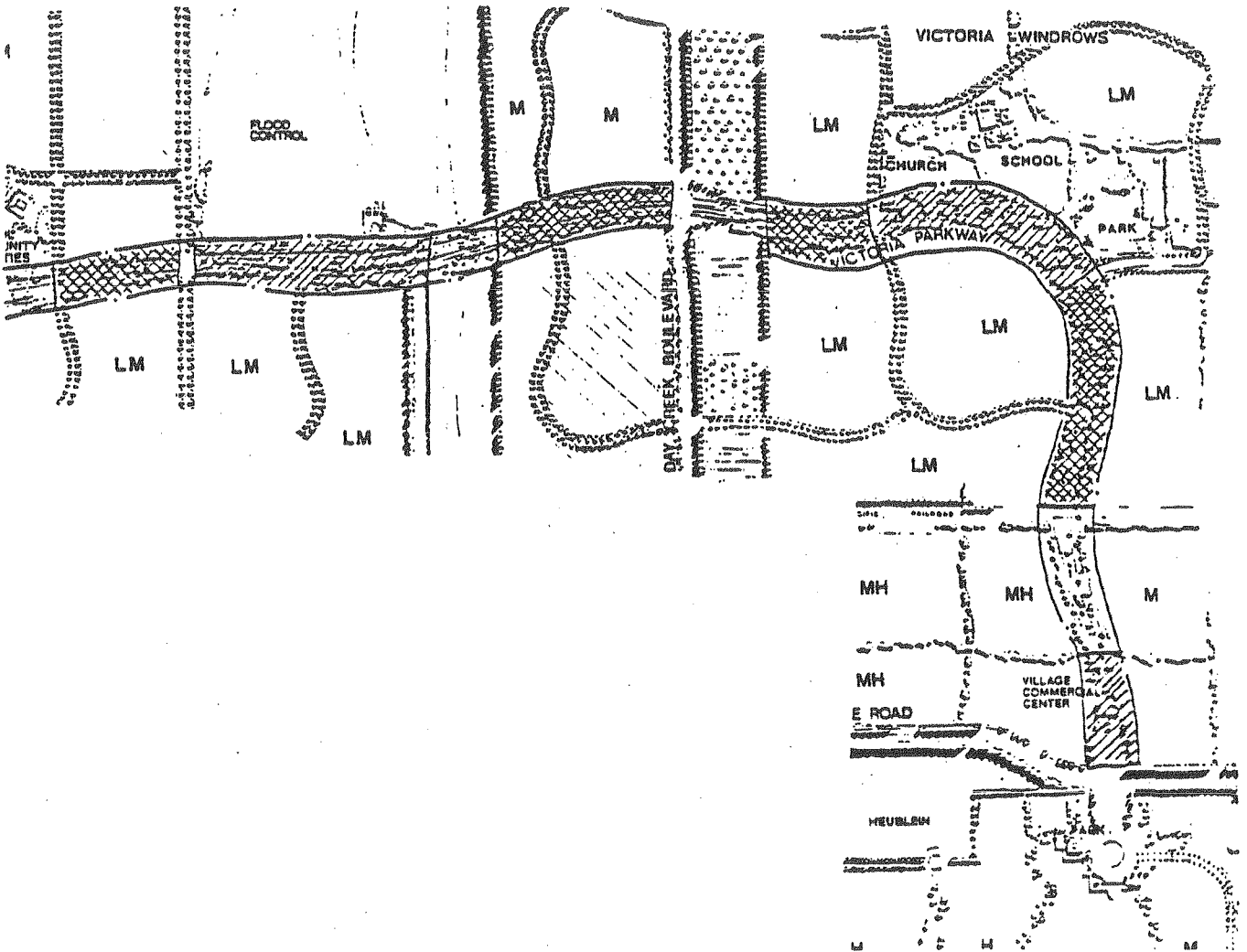
Right-of-Way Width Criteria

LEGEND:

-  Right-of-way 160-180 feet
-  Right-of-way 140-160 feet
-  Right-of-way 120-140 feet



The plan illustrates the application of the right-of-way width criteria to the Community Land Use Plan as it exists in its present form. Changes in the land use designations and their location will require changes in the right-of-widths as they are shown here. However, over the entire length the Parkway, the average right-of-way will be approximately 150 feet. Total acreage for park and roadway should be approximately 40 acres.



VICTORIA PARKWAY

Typical Intersection Conditions Overview

A. LINEAR PARK TERMINUS

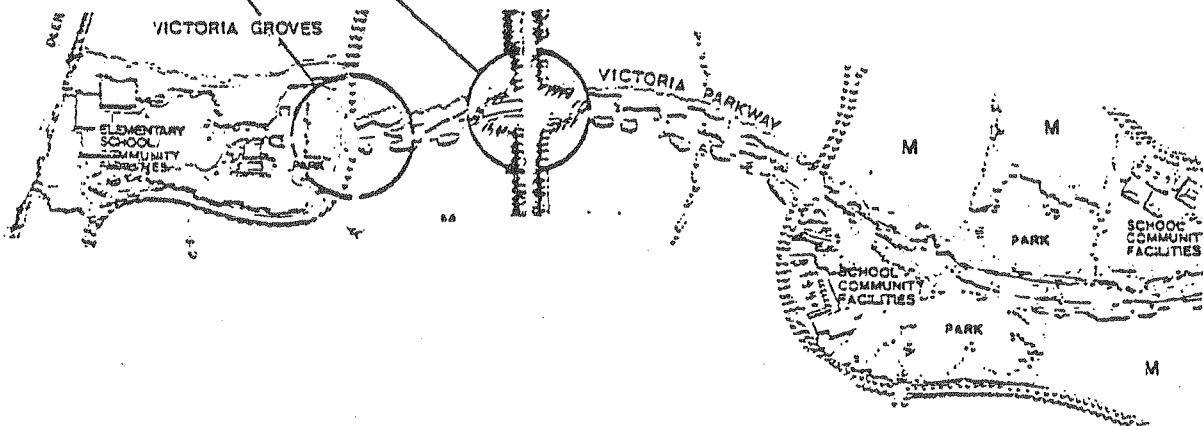
Linear park terminates at pepper tree grove where it connects with bicycle, equestrian and pedestrian system of trails.

B. INTERSECTION WITH ARTERIAL ROADS

A large open space with a dense informal planting of tall trees with a vertical growing habit around the edges.

C. "T" INTERSECTION: TRAIL OPPOSITE

Windrow planting extends into parkway to mark trail location



D. "T" INTERSECTION: "M" OR "MH" LAND USE OPPOSITE

Align road to allow views into residential 'green-belts'; Maximize land in park by extending parkway planting to building setback or beyond.

E. "T" INTERSECTION: "LM" LAND USE OPPOSITE

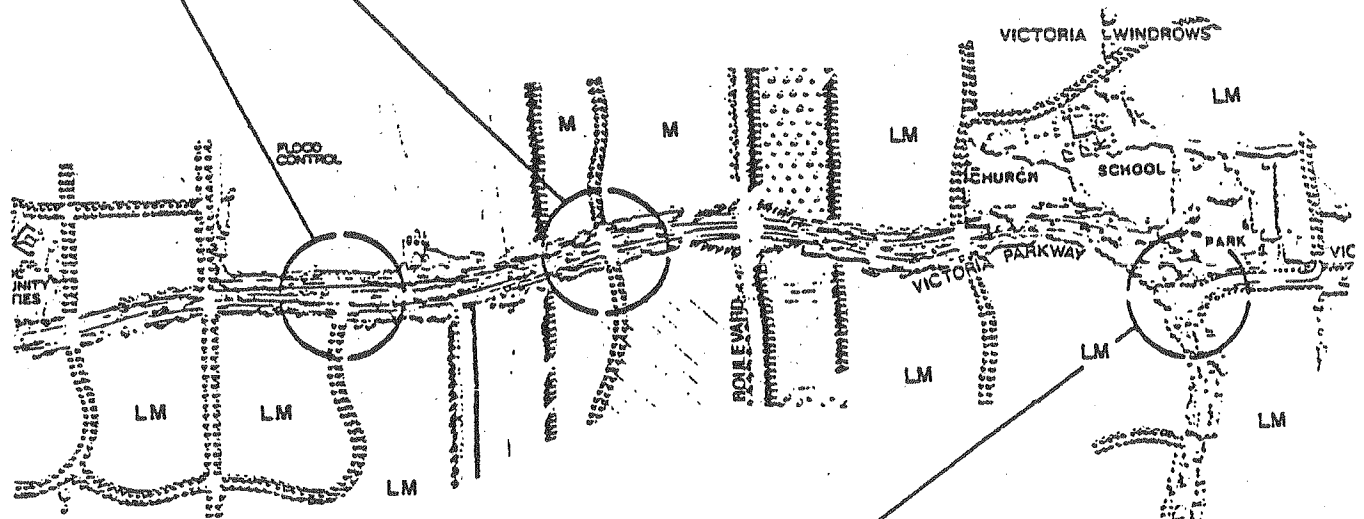
An opening in the parkway planting at the intersection; trees and shrubs screen views of the adjacent residential land use

F. "T" INTERSECTION: OPEN SPACE OPPOSITE

Use parkway plant material to frame long views into open space.

G. "X" INTERSECTION

An opening in the parkway planting with naturalized grasses, scattered trees, and shrubs for seasonal color or to attract birds

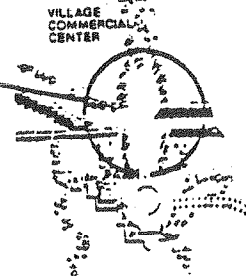


H. INTERSECTION WITH VICTORIA STREET

Planting opens to allow views of lake in Windrows Park; Planting on Victoria ties to existing Estuanda character

I. INTERSECTION WITH BASELINE

Extend special planting used south of Baseline north to create a smooth transition from the informal parkway planting to the urbanized lake edges.

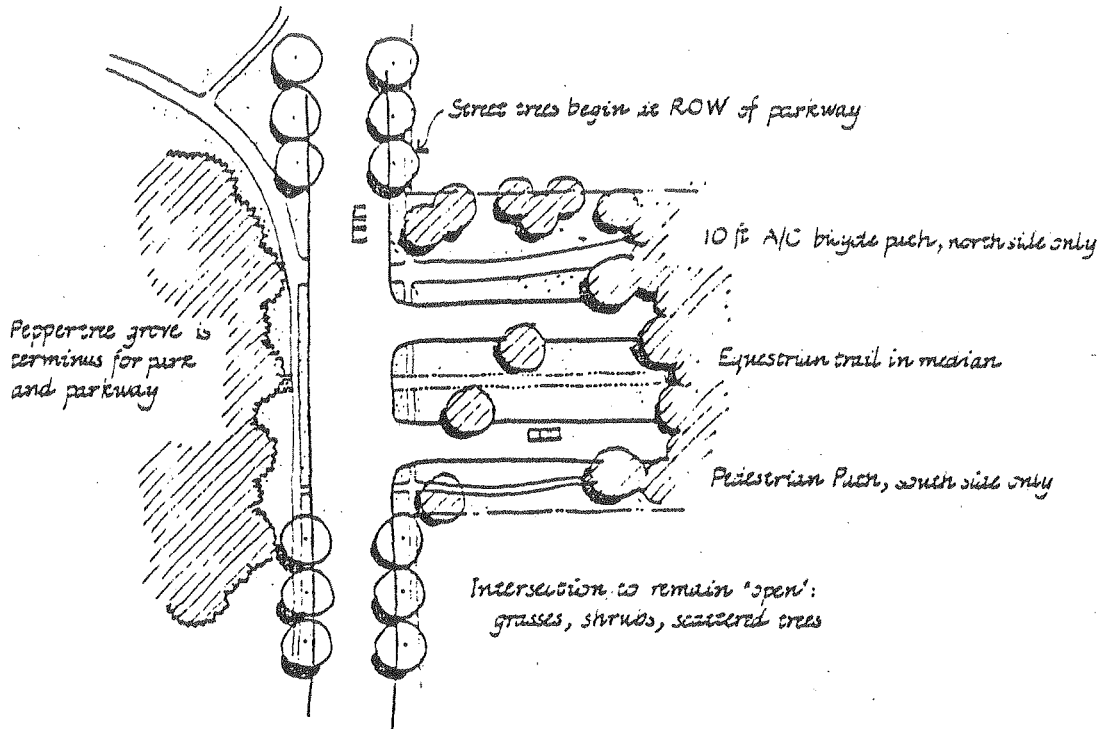


VICTORIA PARKWAY

Typical Intersection Conditions

A. LINEAR PARK TERMINUS

Linear park terminates at peppertree grove where it connects with bicycle, equestrian and pedestrian system of trails.

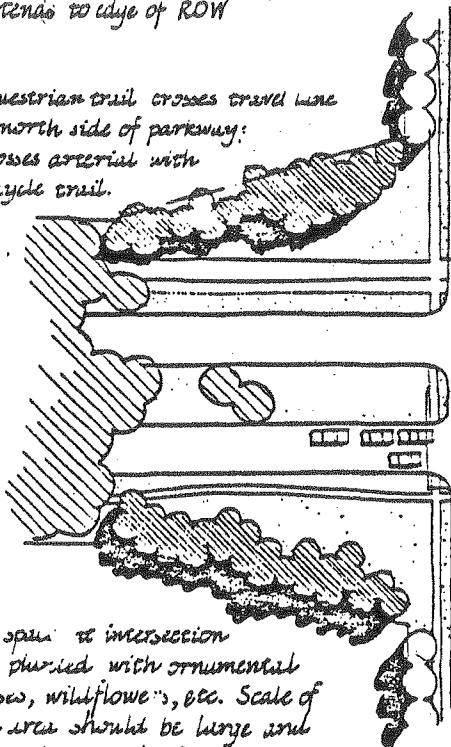


B. INTERSECTION WITH ARTERIAL ROADS

A large open space with a dense informal planting of tall trees with a vertical growing habit around the edges.

Wimborow style planting of Special Boulevard extends to edge of ROW

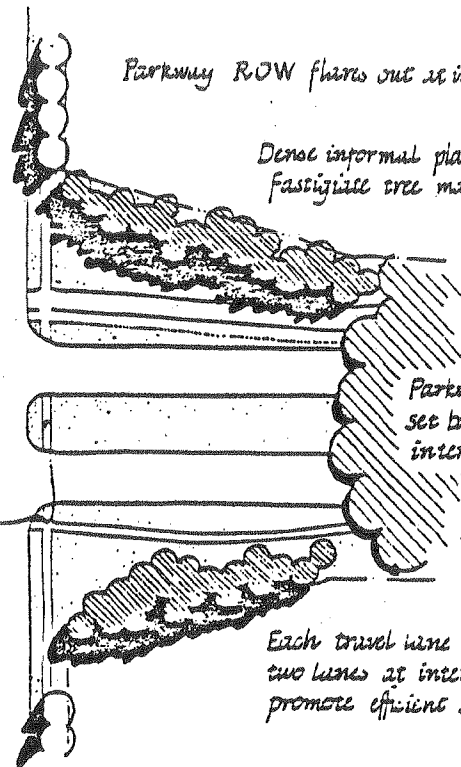
Equestrian trail crosses travel lane to north side of parkway; crosses arterial with bicycle trail.



Open space at intersection to be planted with ornamental grasses, willflowers, etc. Scale of open area should be large and appropriate to the importance of the entry.

Parkway ROW flares out at intersection

Dense informal planting of fastigiate tree marks intersection

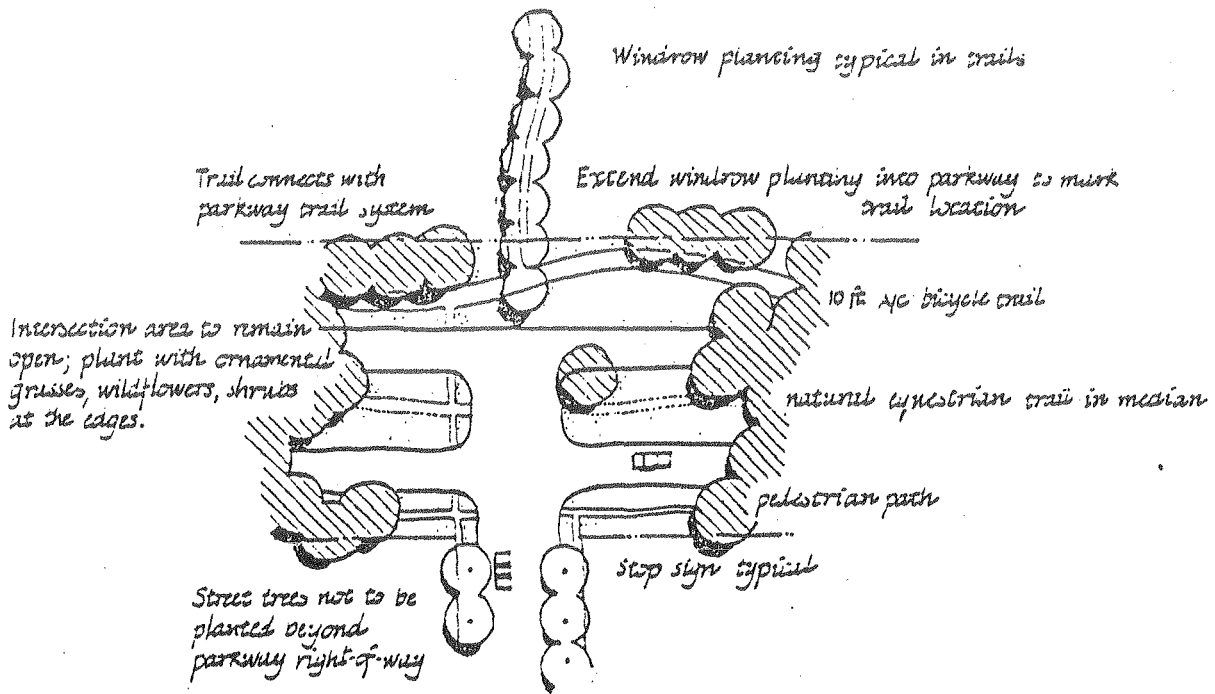


Parkway planting see back from intersection = 150 ft.

Each travel lane widened to two lanes at intersection to promote efficient circulation.

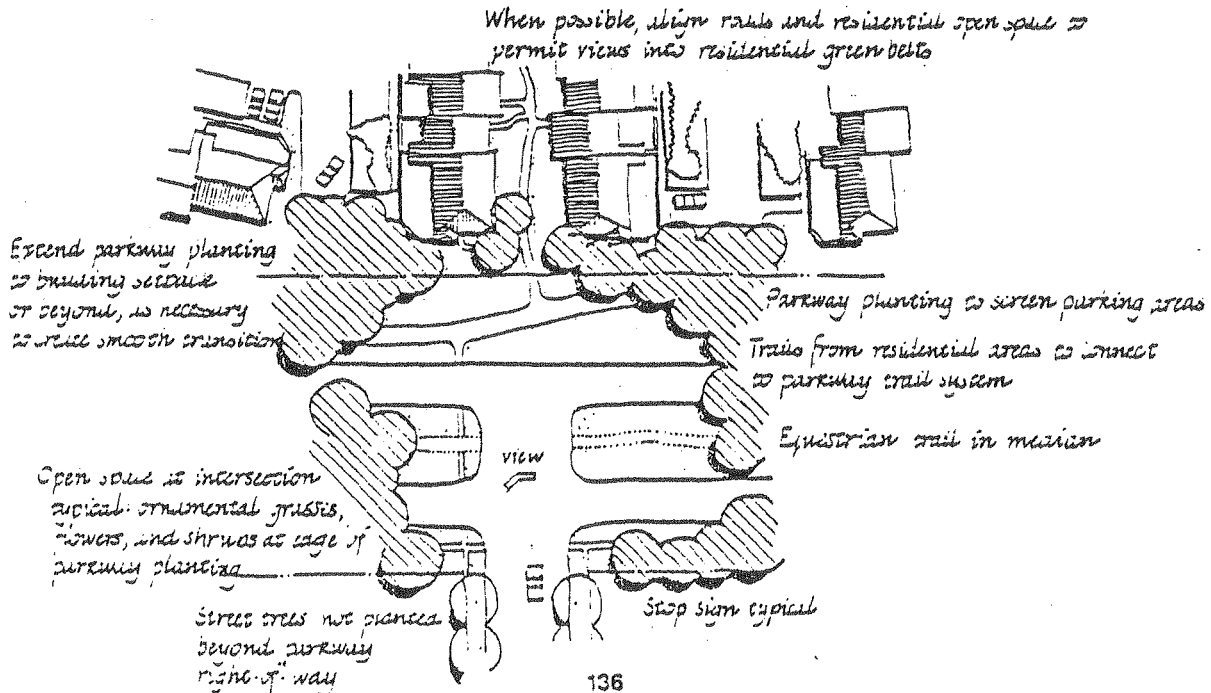
C. "T" INTERSECTION: TRAIL OPPOSITE

Window planting extends into parkway to mark trail location.



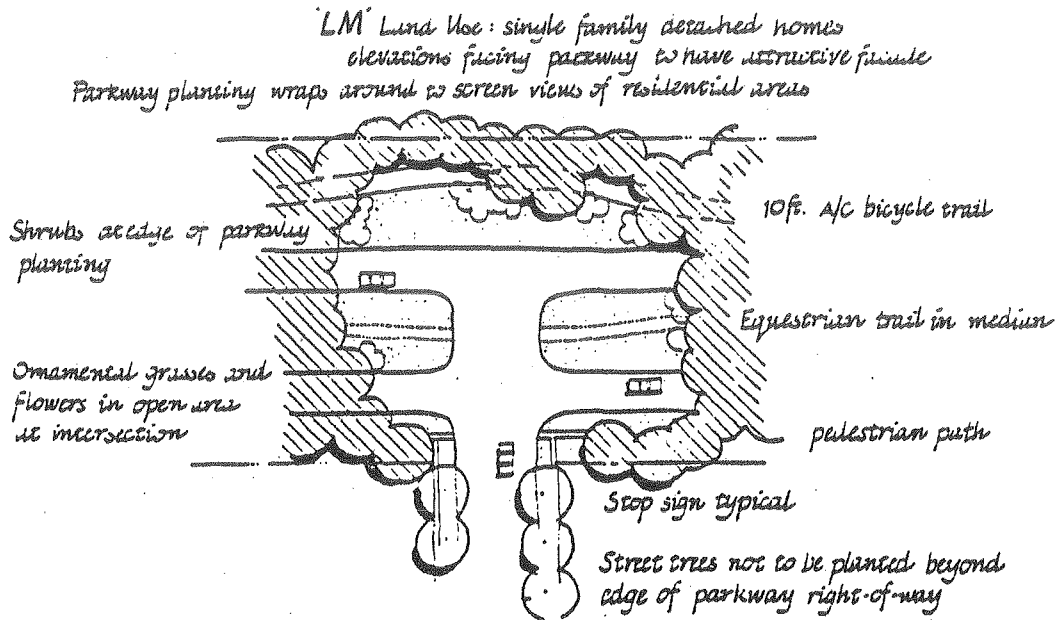
D. "T" INTERSECTION: "M" OR "MH" LAND USE OPPOSITE

Align road to allow views into residential "green-belts"; Maximize land in park by extending parkway planting to building setback & beyond.



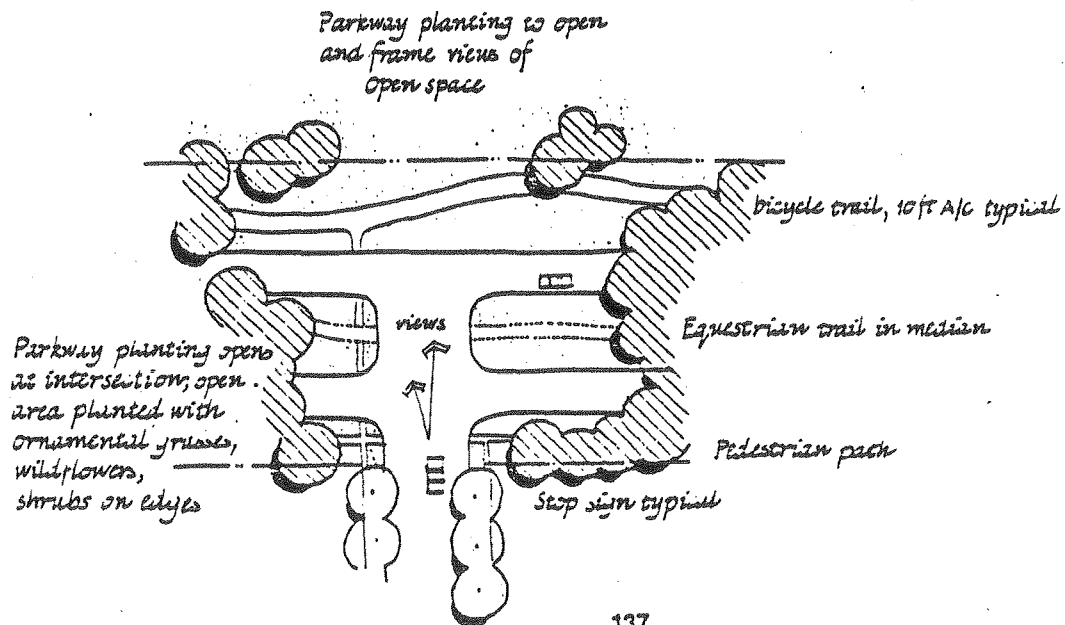
E. "T" INTERSECTION: "LM" LAND USE OPPOSITE

An opening in the parkway planting at the intersection; trees and shrubs screen views of the adjacent residential land use



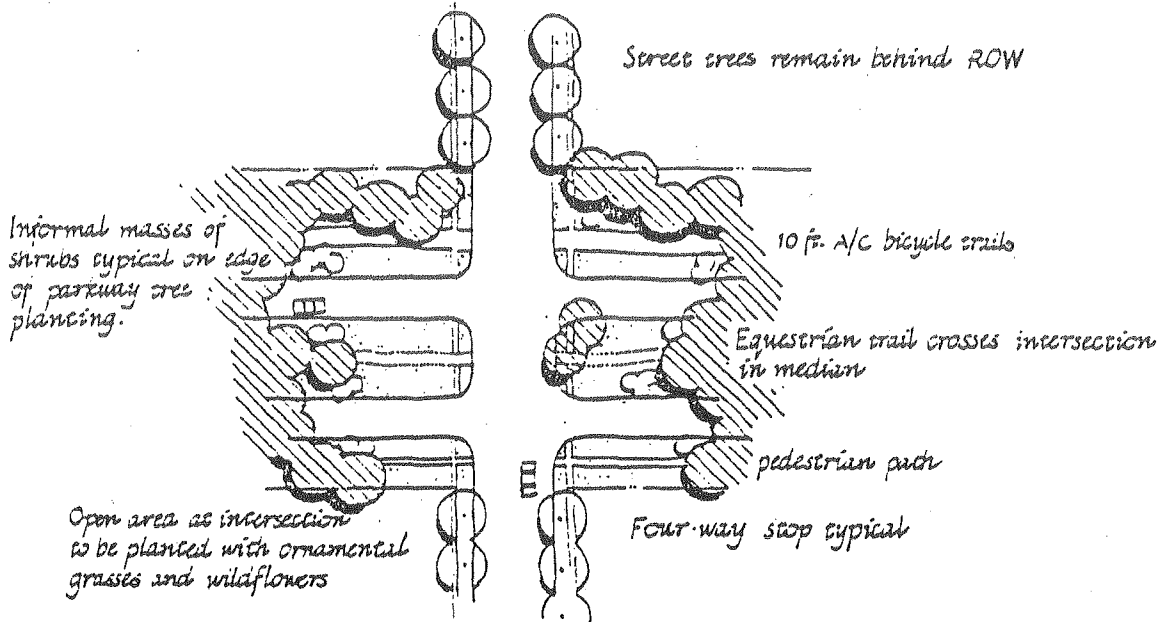
F. "T" INTERSECTION: OPEN SPACE OPPOSITE

Use parkway plant material to frame long views into open space.



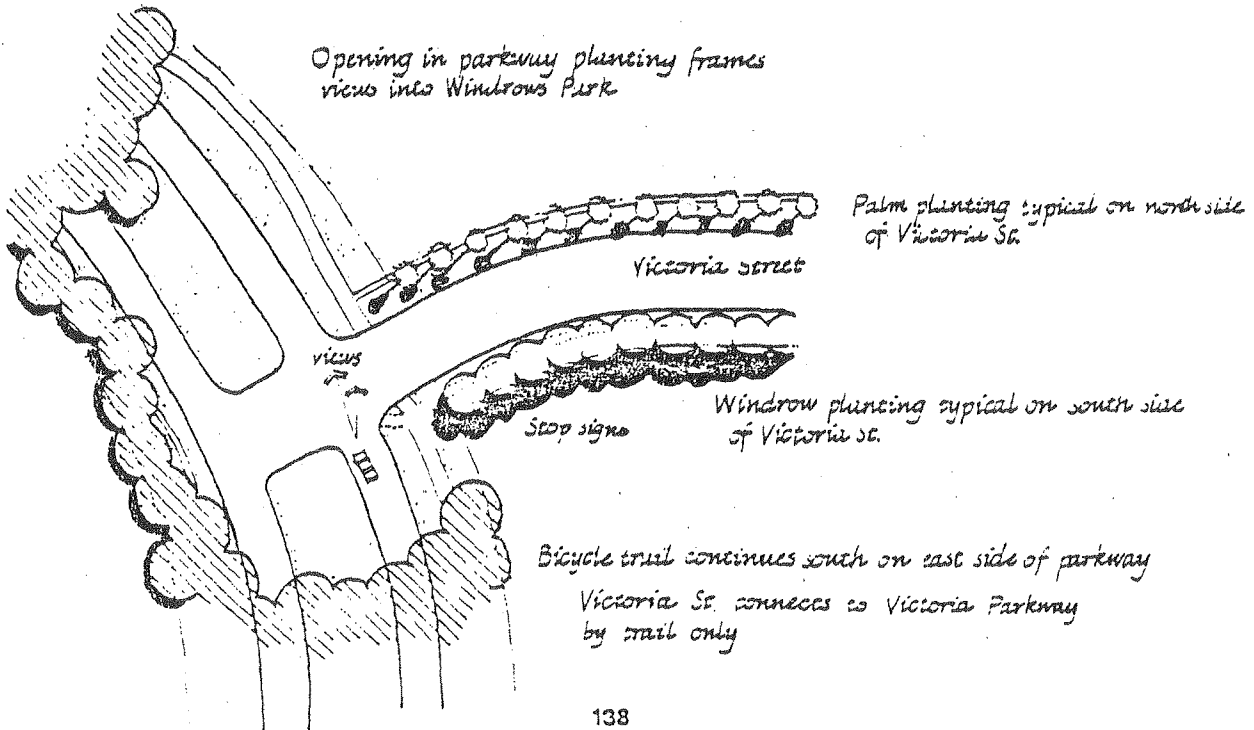
G. "X" INTERSECTION

An opening in the parkway planting with naturalized grasses, scattered trees, and shrubs for seasonal color or to attract birds



H. INTERSECTION WITH VICTORIA STREET

Planting opens to allow views of lake in Windrows Park; Planting on Victoria ties to existing Etiwanda character

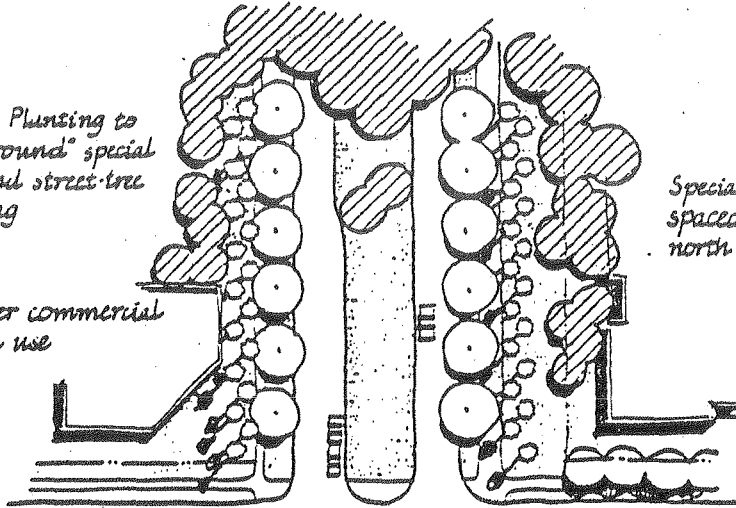


INTERSECTION WITH BASELINE

- Extend special planting used south of Baseline north to create a smooth transition from the informal parkway planting to the urbanized lake edges.

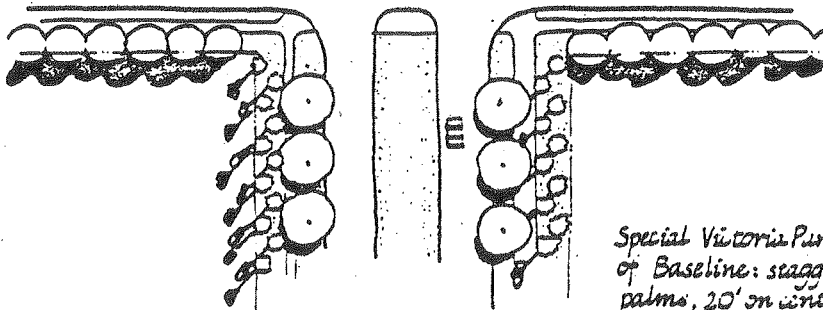
Parkway Planting to "wrap around" special palm and street tree planting

Corner commercial land use



Special Palm and regularly spaced street tree planting continues north of Baseline = 200ft.

Baseline Road



Windrow style planting typical on Baseline

Special Victoria Parkway Planting, south of Baseline: staggered double row of palms, 20' on center, regularly spaced street trees in parking strip, 30' on center

VICTORIA PARKWAY

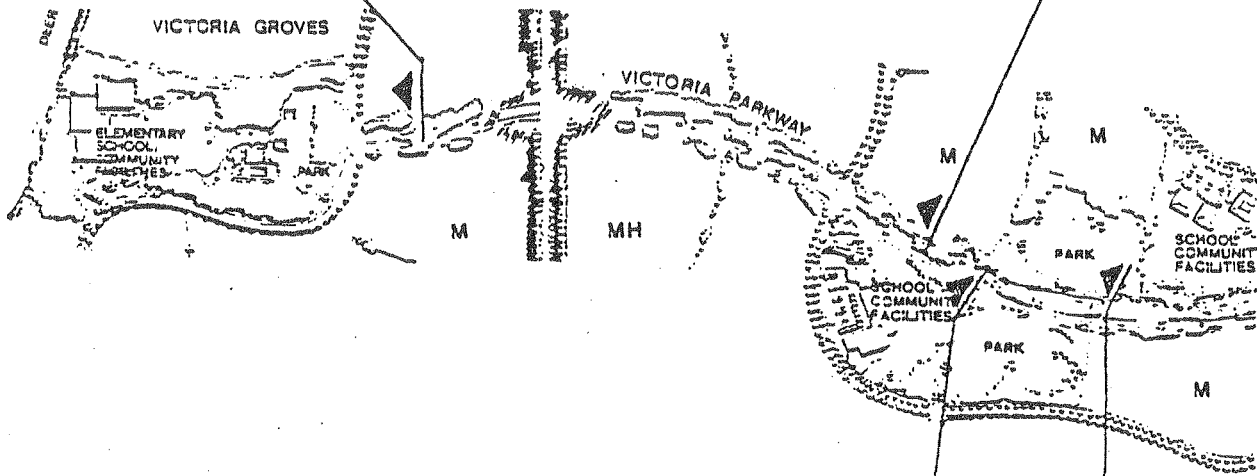
Typical Edge Condition Cross-Sections

A. ROAD - TRAIL - "LM" LAND USE

A dense planting and wide setback will screen views of residential areas and will provide an attractive setting for the trail.

B. ROAD - TRAIL "M" OR "MH" LAND USE

Parkway "borrows" space from adjacent attached or multi-family dwellings; trail has wide setback in which to meander.



C. ROAD - PUBLIC LAND

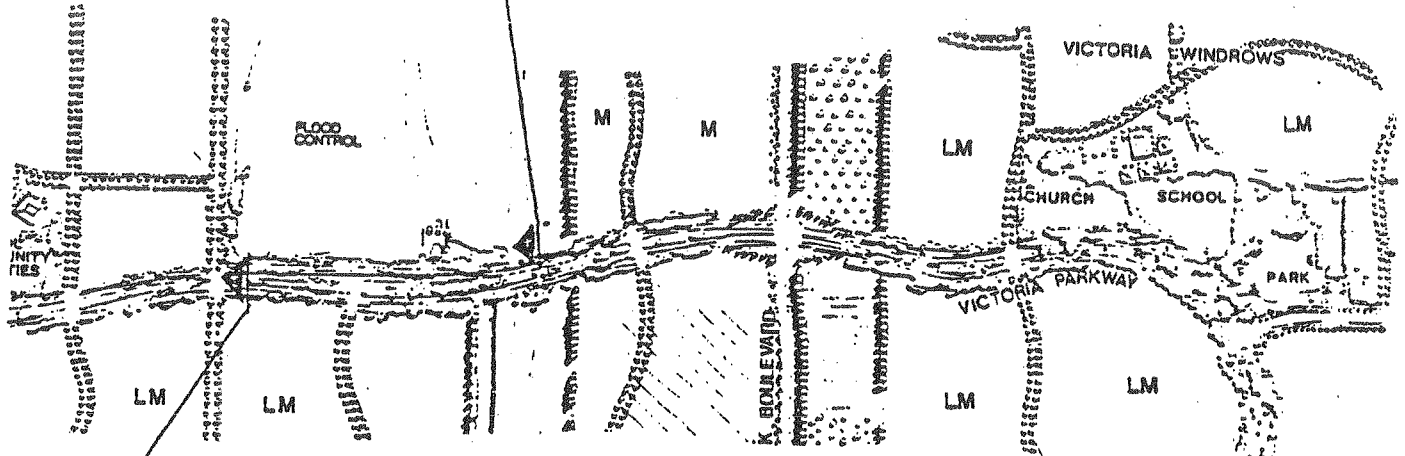
Maximize perceived open space by extending parkway planting and allowing pedestrian path to meander into adjacent area.

D. ROAD - TRAIL - PUBLIC LAND

Use parkway right-of-way together with open space to form one space by extending parkway planting and letting trail become part of the park or school grounds.

E. ROAD - TRAIL - QUASI-PUBLIC LAND

Use adjacent open space where possible to extend parkway planting to maximize effective open space.



F. ROAD - "LM" LAND USE

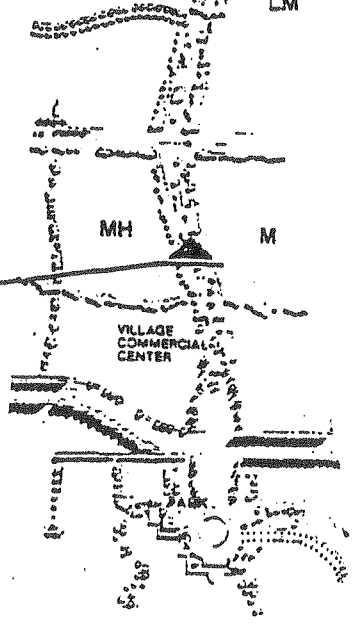
A moderate setback with a dense planting and attractive elevations on homes facing the parkway

G. ROAD - "M" OR "MH" LAND USE

"Borrow" space from the adjacent land use by extending the parkway planting to the building setback or beyond, when possible.

H. MEDIAN

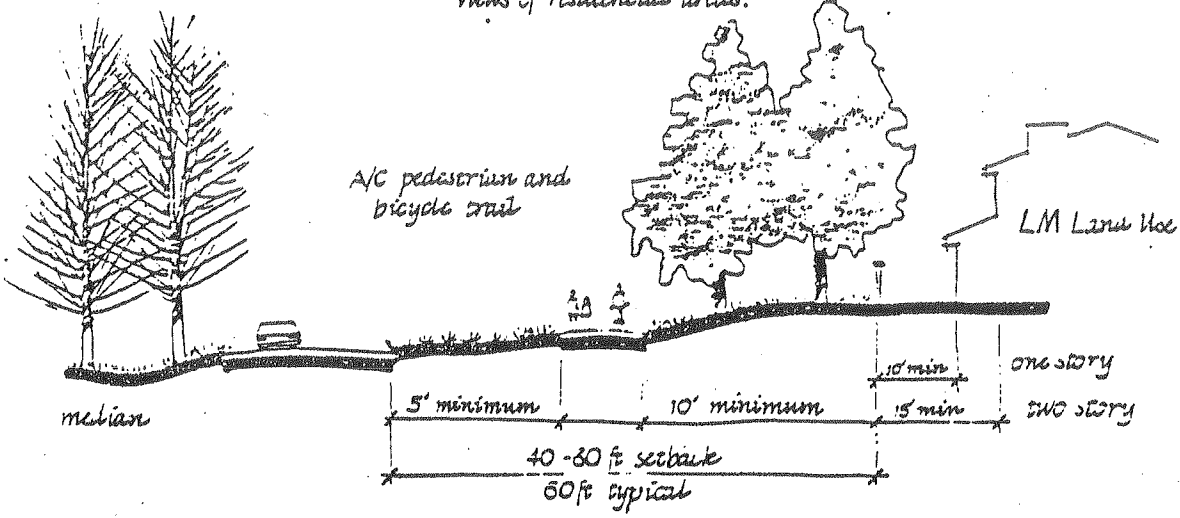
A natural equestrian trail meanders within a median that varies in width.



A. ROAD - TRAIL - "LM" LAND USE

A dense planting and wide setback will screen views of residential areas and will provide an attractive setting for the trail.

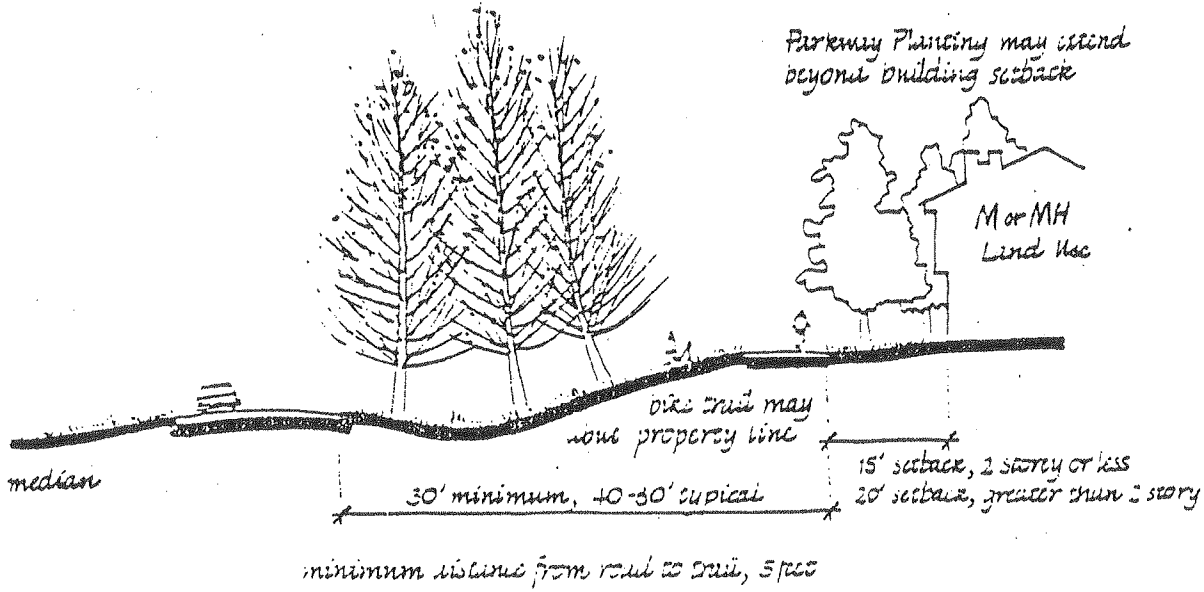
where appropriate, dense planting should screen views of residential areas.



B. ROAD - TRAIL - "M" OR "MH" LAND USE

Parkway "borrows" space from adjacent attached or multi-family dwellings; trail has wide setback in which to meander.

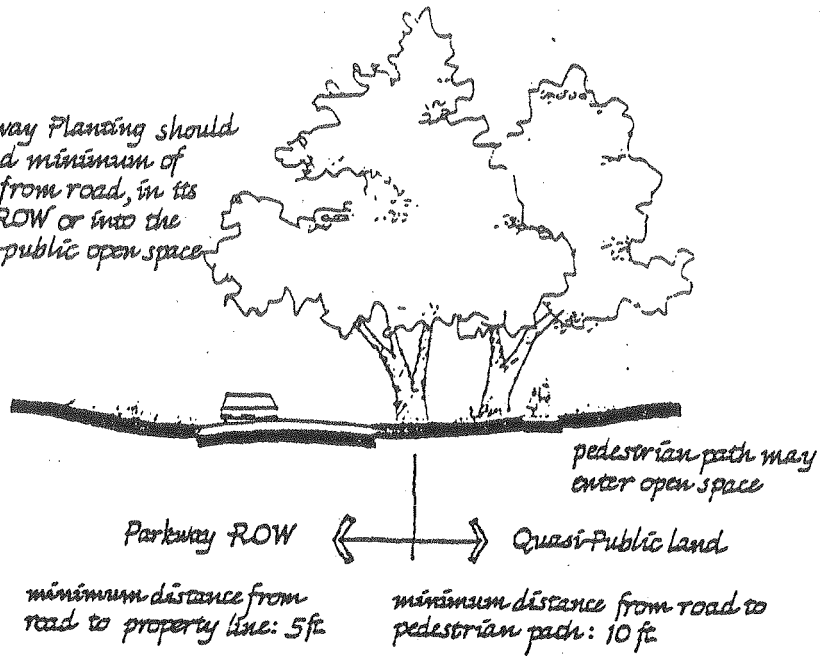
Parkway Planting may extend beyond building setback



C. ROAD - PUBLIC LAND

Maximize perceived open space by extending parkway planting and allowing pedestrian path to meander into adjacent area.

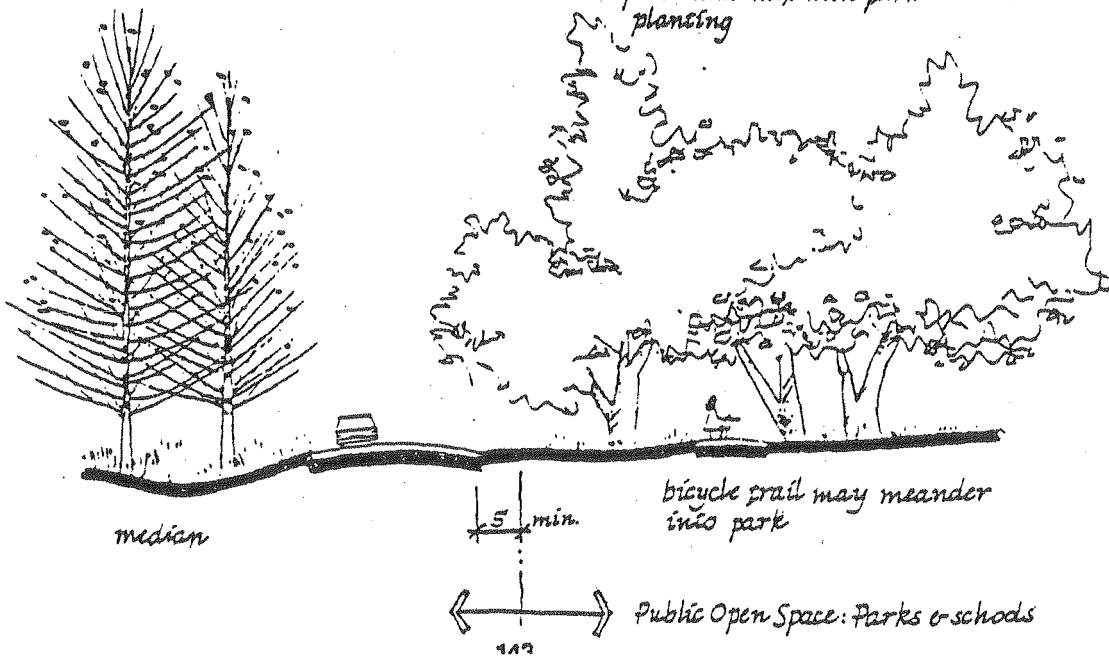
Parkway Planting should extend minimum of 20 ft. from road, in its own ROW or into the quasi-public open space.



D. ROAD - TRAIL - PUBLIC LAND

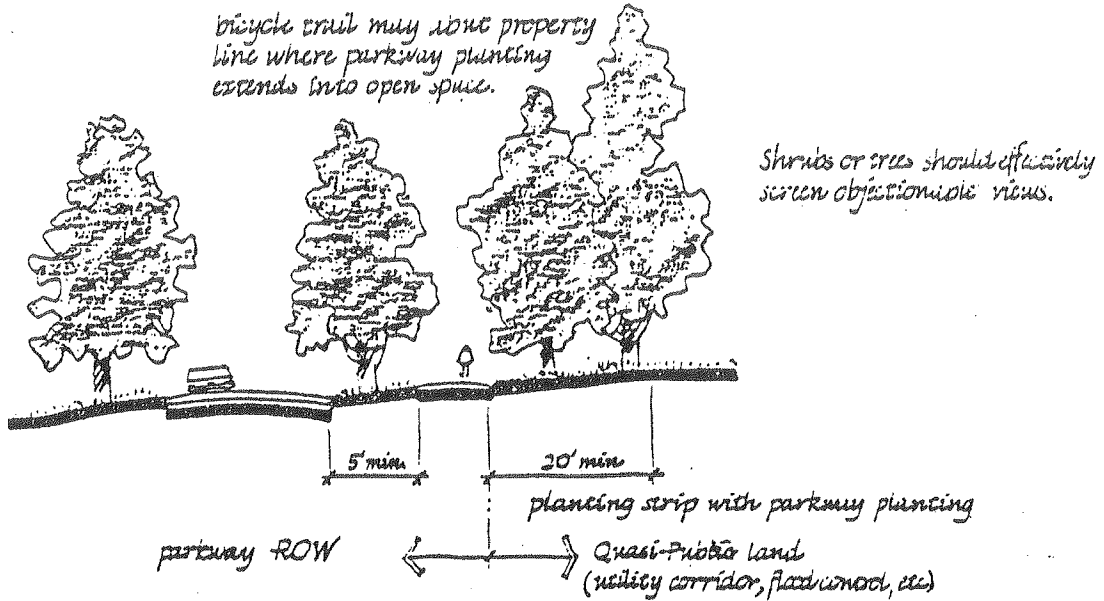
Use parkway right-of-way together with open space to form one space by extending parkway planting and letting trail become part of the park or school grounds.

Parkway Planting should extend into park and mix with park planting



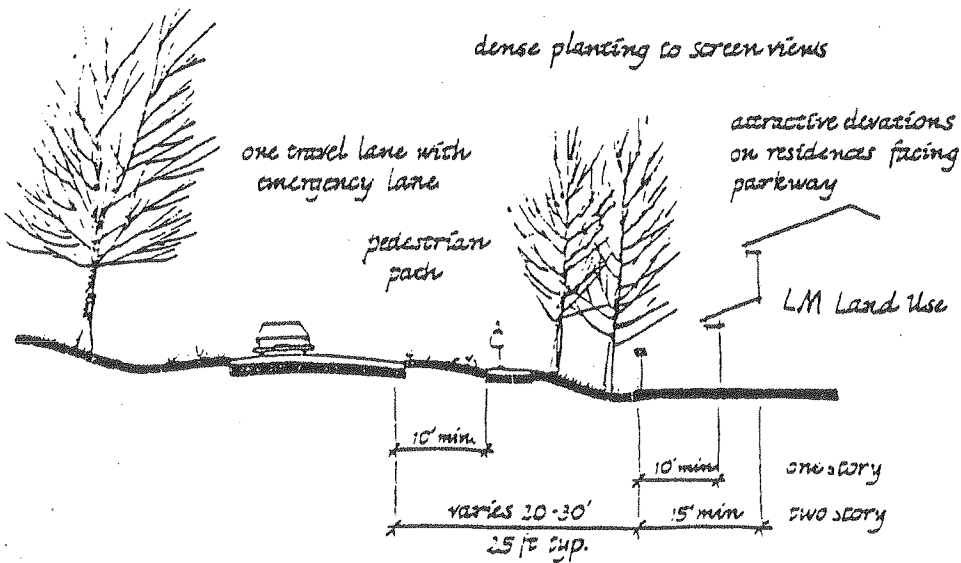
E. ROAD - TRAIL - QUASI-PUBLIC LAND

Use adjacent open space where possible to extend parkway planting to maximize effective open space.

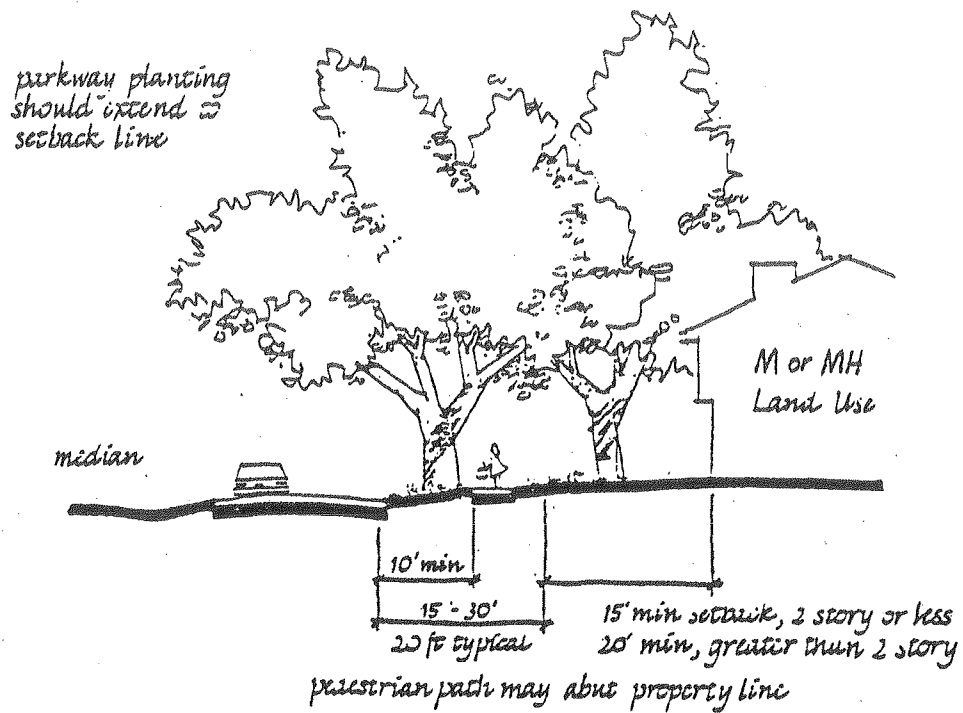


F. ROAD - "LM" LAND USE

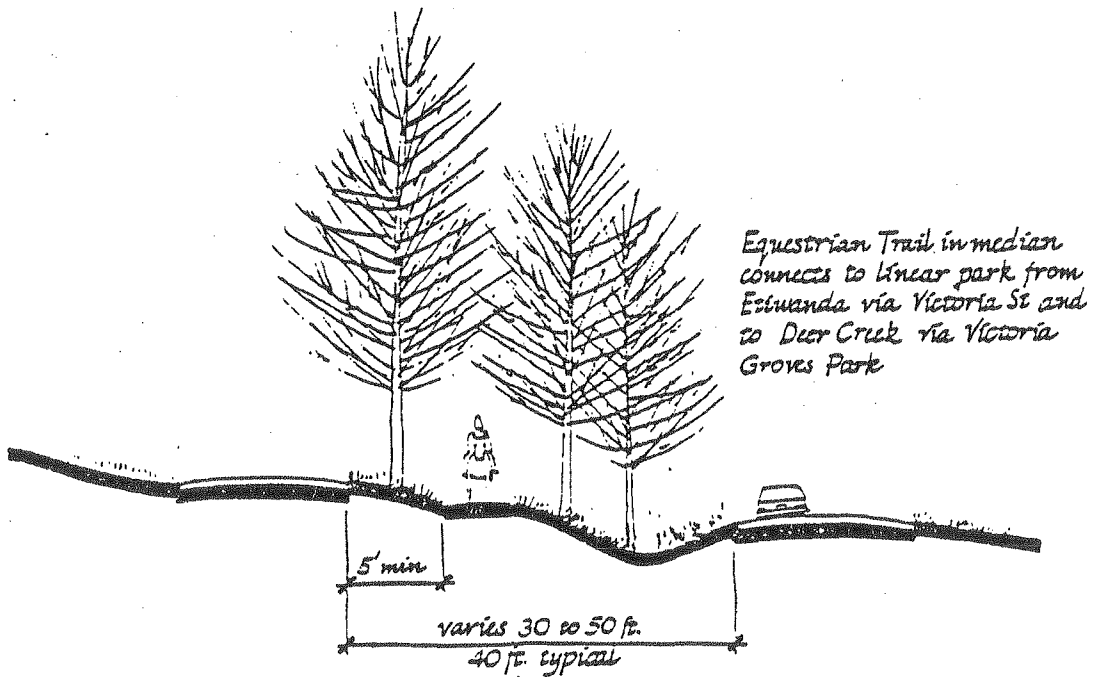
A moderate setback with a dense planting and attractive elevations on homes facing the parkway will create a pleasing landscape.



G. ROAD - "M" OR "MH" LAND USE
 "Borrow" space from the adjacent land use by extending the parkway planting to the building setback or beyond, when possible.



H. MEDIAN
 A natural equestrian trail meanders within a median that varies in width.



III
LANDSCAPE STANDARDS

LANDSCAPE STANDARDS

INTRODUCTION

The Landscape Standards section contains specific criteria for the landscaped areas of Victoria. This includes not only parks and open space, but road edges, trails, utility corridor edges, and school grounds. These landscaped areas have a significant role in the image that is created of a community and the landscape criteria are designed to create a beautiful community while addressing basic planning goals and concepts, as well as community-wide issues of conservation and urban design.

This section has several parts. The first two are general discussions of the "windrow" style of planting and the use of drought tolerant plant materials. Both of these concepts are crucial to the plan. The Entry Hierarchy Plan shows the location of places that will have a special planting treatment to reinforce the sense of entry into the community. The Plant Palette Plan shows the location of the different kinds of plant material for the entire planning area. It should be used in combination with the Typical Landscape Edge Condition Cross-Sections that follow. These cross-sections have specific criteria for areas to be landscaped within Victoria.

DROUGHT TOLERANT PLANT MATERIALS

Drought tolerant plant materials will be used extensively throughout Victoria. Their use will accomplish several important community planning goals: first, they will enrich the existing landscape character, which is dominated by drought resistant plants; second, their use will conserve water and energy; third, they are economical to maintain; and fourth, in the proper place, they can serve the image forming needs of the community as well as plants that may require more water.

The plan does recognize that drought tolerant plants will need some irrigation, especially in the first few growing seasons. Once the plants are established, irrigation will be required about once a month during the dry months. This irrigation practice will promote deep root growth and a better tolerance for the hot, dry summer months. Irrigation methods will vary depending on the particular situation and the specific plants chosen. In some places, bubbler heads will provide the once-a-month deep watering. Other situations may be better adapted to some form of drip irrigation. Still others may require truck watering for the first few years, and no additional irrigation after that. Specific conditions will require specific solutions that can be implemented as the choice plant material and specific planting location is known.

The use of drought tolerant plant material also makes the use of ornamental native plants possible. Many natives are sensitive to over watering and could not be used unless watering is restricted during the dry months. Plants such as Toyon (Hetermoles arbutifolia), Hollyleaf cherry (Prunus ilicifolia) or Sugarbush (Rhus ovata) will thrive in dry conditions. These and other native trees and shrubs will be used in a natural way to create a pleasant naturalized landscape.

Non-native ornamentals that are drought tolerant will also be used. Acacia (Acacia spp.), Olive (Olea europea), Eucalyptus (Eucalyptus spp.) and Pines (Pinus spp.) are some of the plants that are suggested for use within Victoria. Palms (Washingtonia spp., Phoenix spp.) also contribute to the existing landscape character, are drought resistant, and will be used in Victoria.

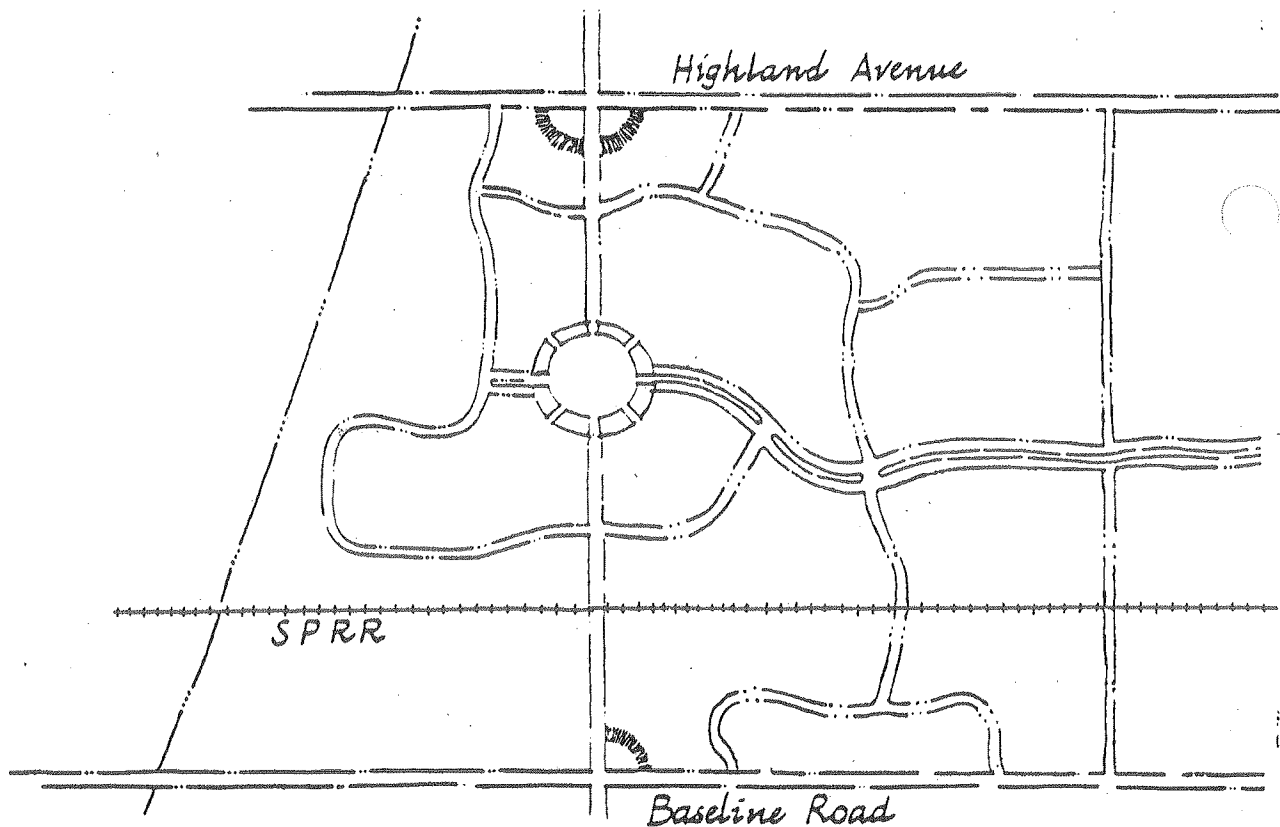
WINDROW STYLE PLANTING

Linear plantings of tall, upright species of trees will be found throughout Victoria and will make a major contribution to the urban design quality. Although the Blue Gum (Eucalyptus globulus) is the traditional windrow tree in California, many other trees can be used to create the same landscape character. Eucalyptus, Silk oaks (Grevillea robusta) and Lombardy Poplars (Populus nigra 'italica') are several of the many species that could be used in Victoria in areas designated for the windrow style planting. Among the areas that will have this treatment are: arterial road edges, along trails throughout residential areas, adjacent to the SCE corridors to screen views of the transmission towers, or along the flood control or railroad right-of-way.

The design concept is to borrow from the existing landscape vocabulary a strong element that can be spread throughout Victoria to provide community identity and character.






ENTRY HIERARCHY

The Entry Hierarchy Plan on the following pages shows the locations where a special landscape treatment should mark an entry into Victoria. Generally, trees and other plants should be the dominant elements of these entry statements. Large conspicuous monuments and signs are not appropriate for these permanent entry treatments, though temporary signs may be necessary during development, and are permitted.

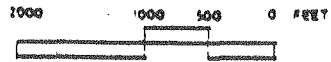
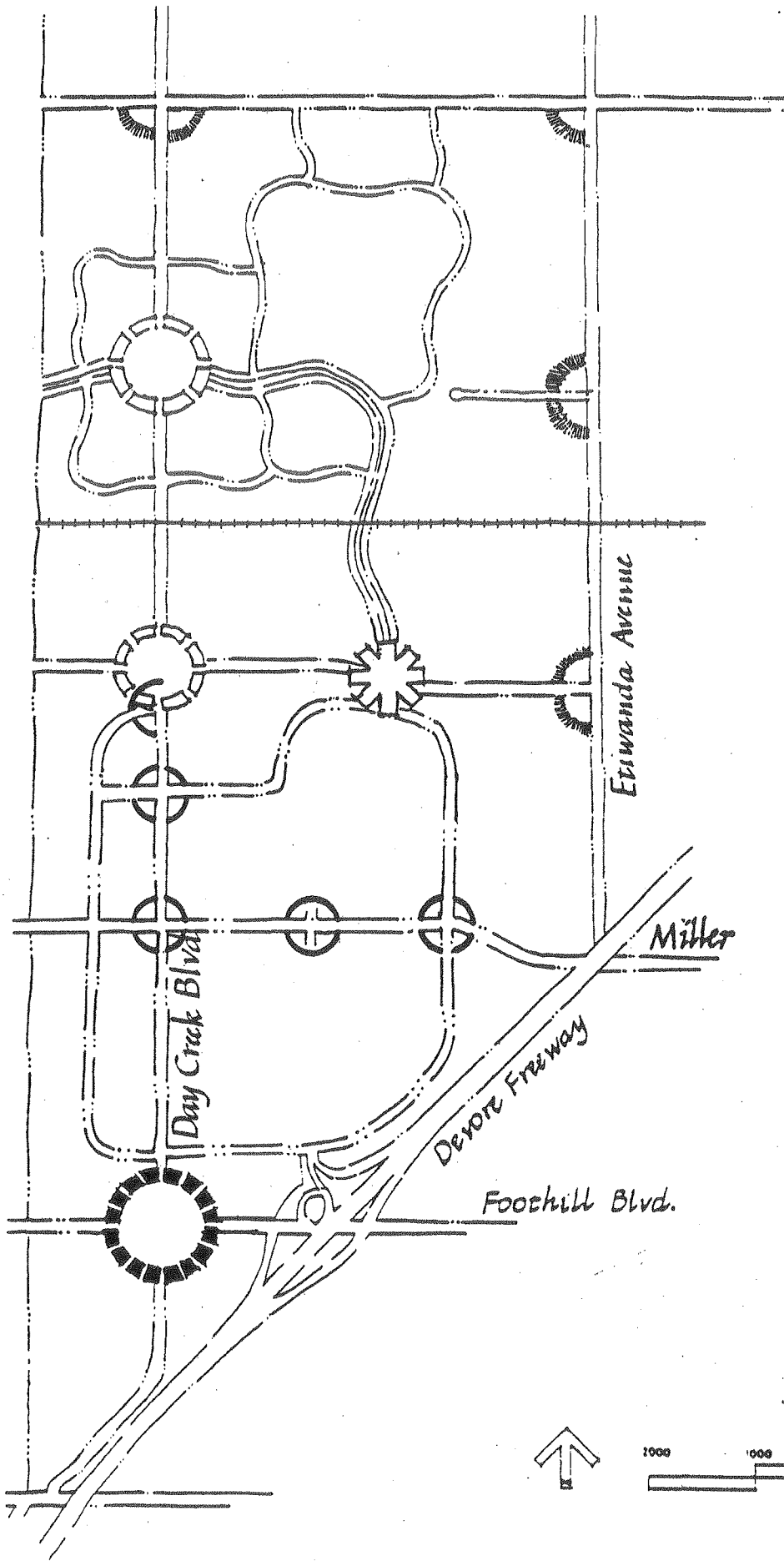


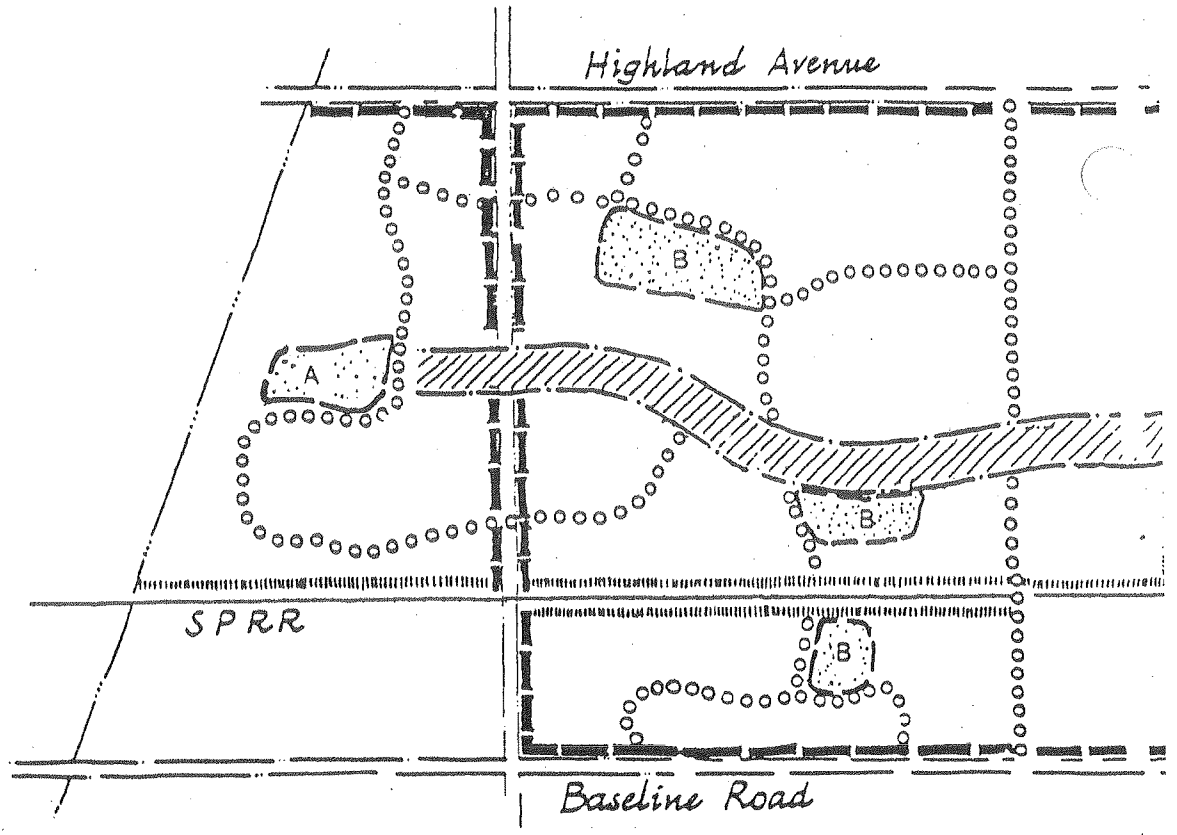
ENTRY HIERARCHY PLAN

LEGEND:

-  Major Community Entry
-  Major Residential Entry
-  Special Entry
-  Minor Community Entry
-  Minor Commercial Entry

The Entry Hierarchy Plan shows the locations where a special landscape treatment will mark an entry into Victoria. Generally, trees and other plants should be the dominant elements of these entry statements. Large conspicuous monuments and signs are not appropriate for these permanent entries, though temporary signs will be necessary during development.









PLANT PALETTE PLAN LEGEND

WINDROW STYLE PLANTING





Trees with a vertical growing habit and closely spaced, linear planting

-  Regional Circulation - residential edge
Drought tolerant plant materials; linear treatment broken by informal masses of trees with rounded form.
-  Regional Circulation - commercial edge
Planting to be reviewed during site plan review
-  Ellwanda Avenue & Victoria Street
Maintain and expand existing planting
-  Utility Corridor, Railroad, & Flood Control Edges
A solid line of Windrow trees planted in yards of homes in L or LM land use other land use areas
Community Trail System (not shown)
Drought tolerant trees to be used when trail is through an L or LM land use area. Other areas not required to use drought tolerant plant materials

VICTORIA LINEAR PARK

Informal masses and large drifts of drought tolerant trees, shrubs and grasses. Limited irrigation once established. Basic Parkway trees are Sycamores, Eucalyptus, Liquidambers, (Sweetgums), Jacarandas and Pines. Shrubs should be natives or naturalized and should provide seasonal color, screening or bird habitat. Ornamental grasses and wildflowers should be planted in open areas.

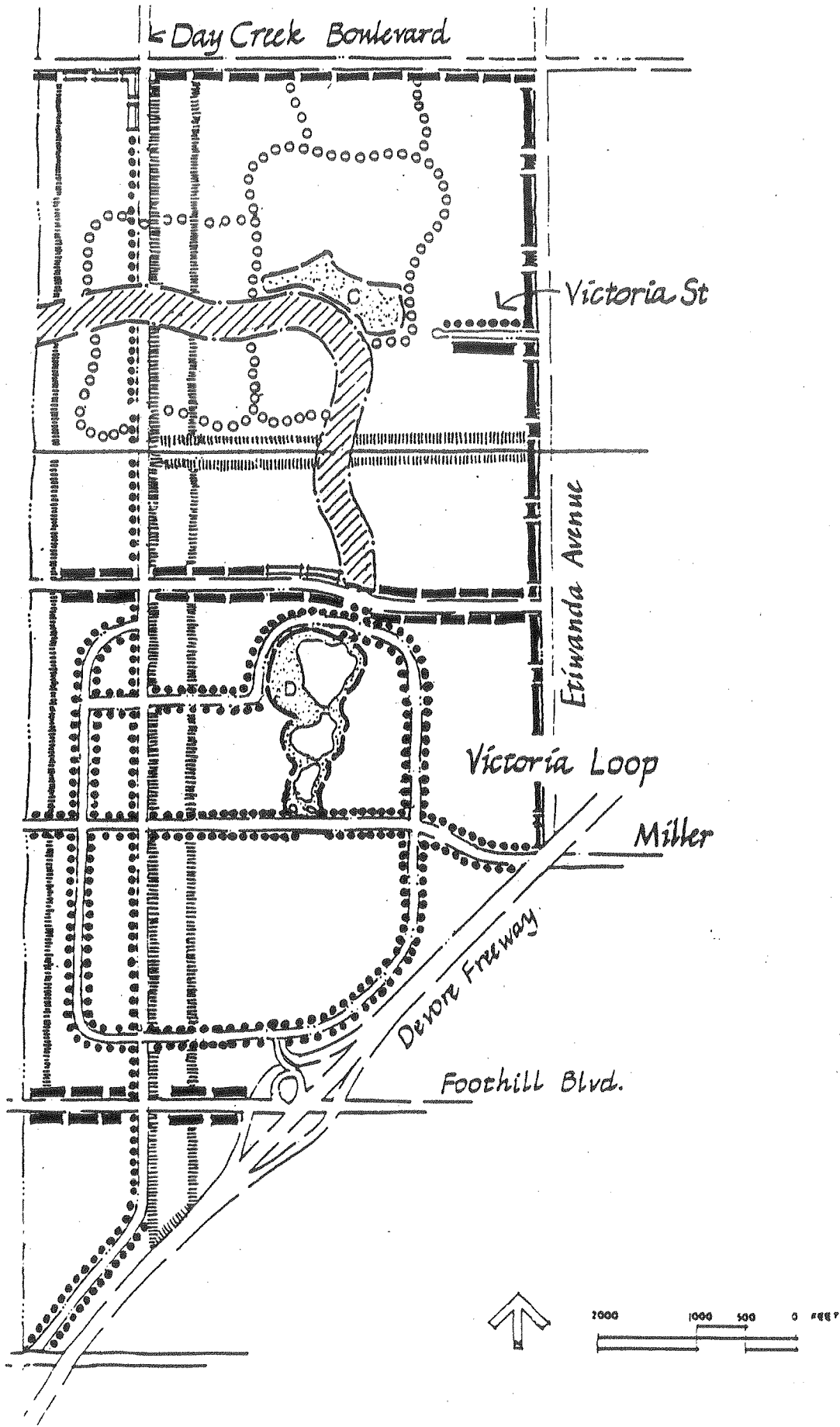
ROAD EDGES

-  Day Creek Boulevard
Windrow Style Planting on all edges shared with Sonoma California Edison; other edges to be planted with staggered double row of Palms.
-  Victoria Loop & Miller
Special Planting of palms and regularly spaced street trees
-  Local Residential Roads
Large, spreading trees, regularly spaced, when mature, canopy should cross over streets.
-  Local Public Streets (not shown)
Medium size trees, regularly spaced with specific quality such as attractive form, showy flowers, fall color, bird-attracting fruit, fragrance, etc.

PARKS & SCHOOLS

In residential villages, the landscape character of the Linear Park should spill into the park and school open space. Each park should have an identity built in part by the plant materials that predominate.

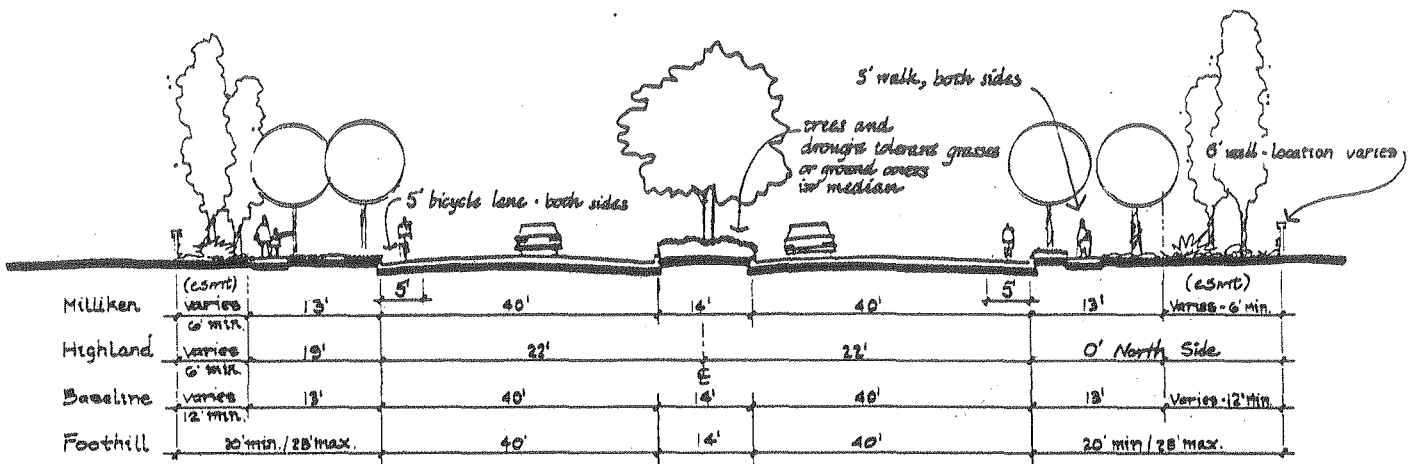
- A Victoria Groves Eucalyptus and Pepper Trees
- B Victoria Vineyards Pines and Sweet Gums
- C Victoria Windrows Eucalyptus
- D Victoria Lakes Palms and Sycamores



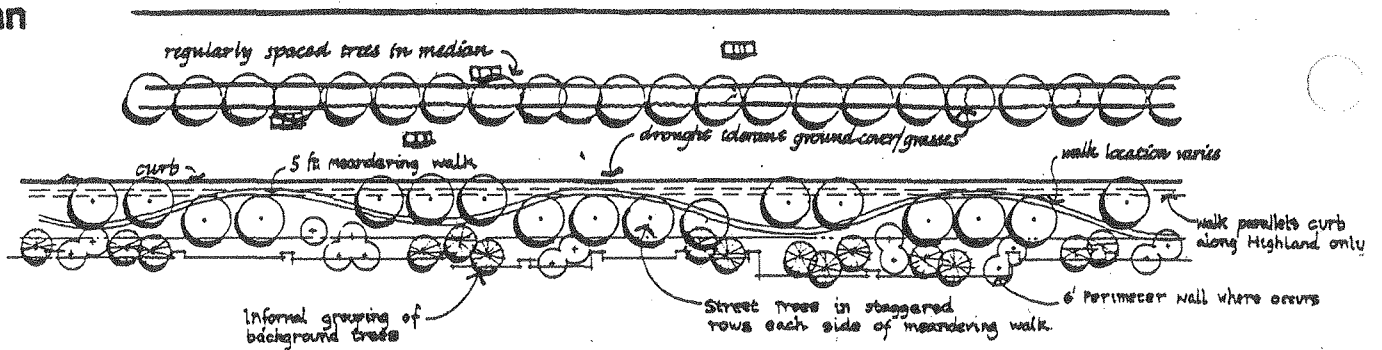
TYPICAL EDGE CONDITIONS

MILLIKEN, HIGHLAND, BASELINE, FOOTHILL

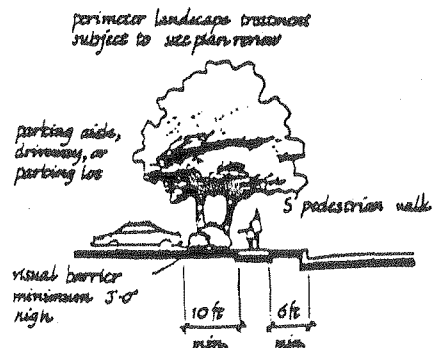
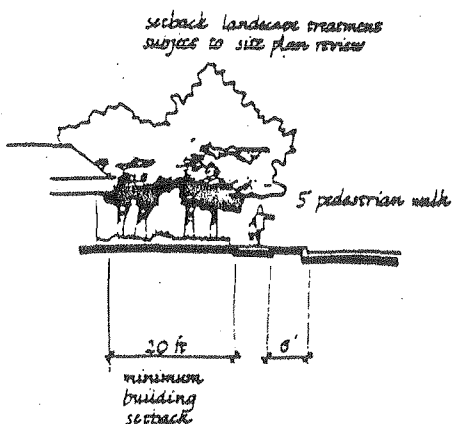
Section at Residential Land Uses



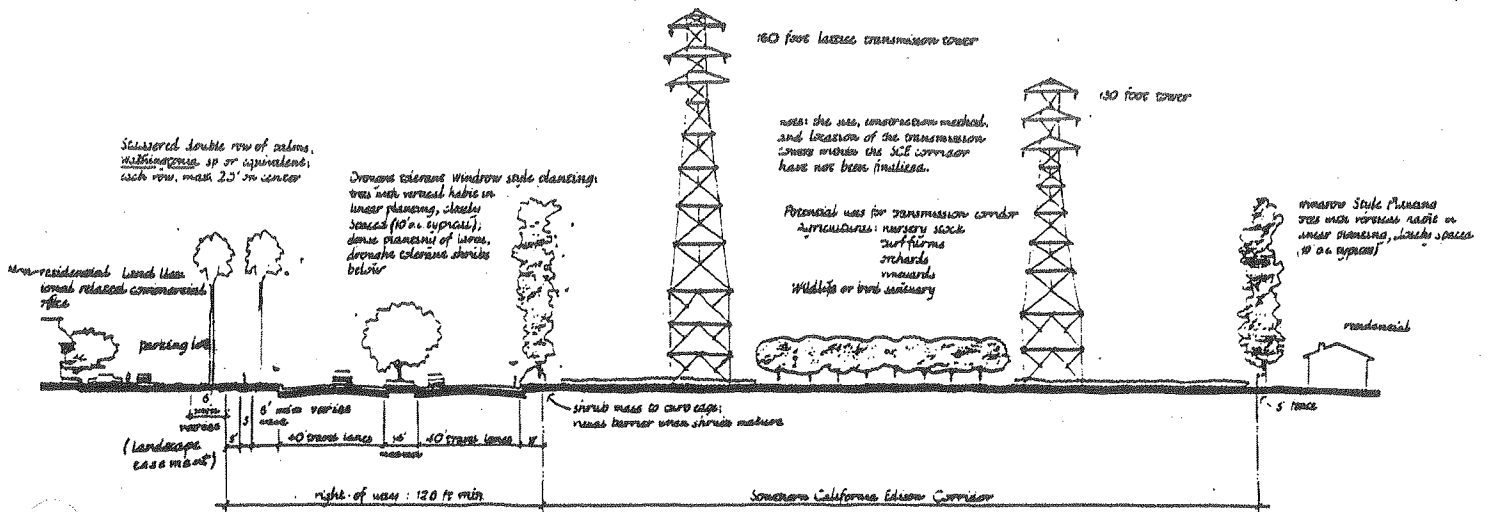
Plan



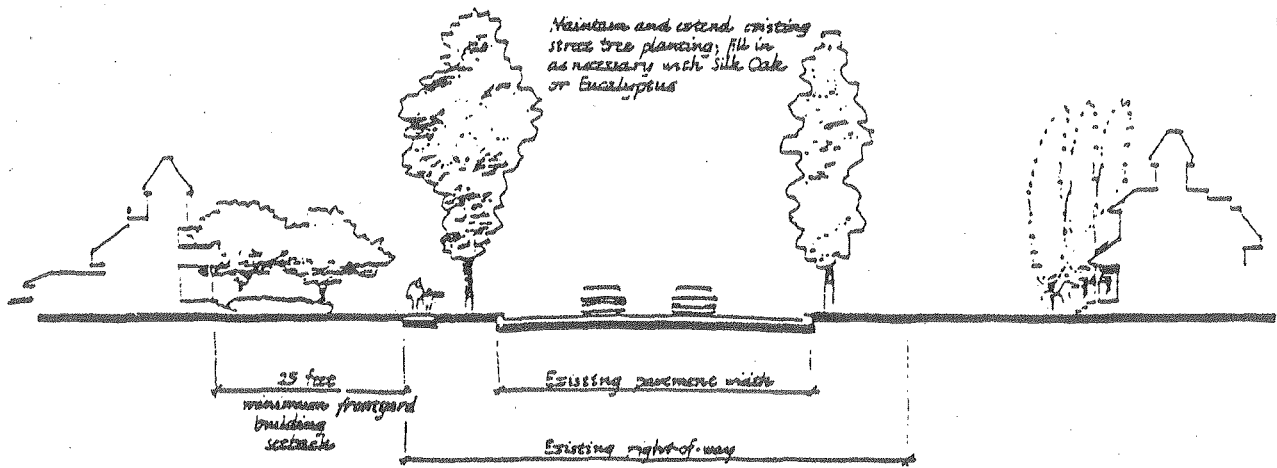
Section at Commercial Land Uses



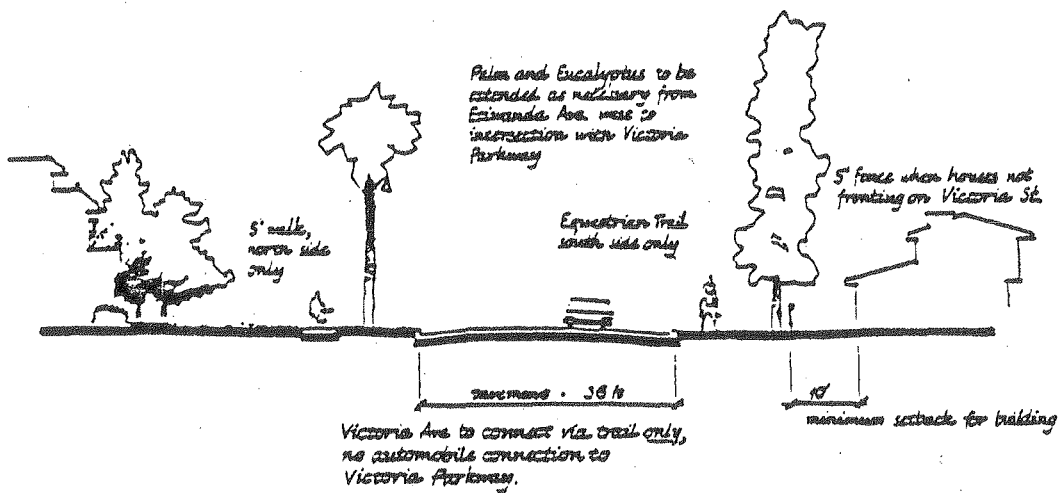
DAY CREEK BOULEVARD



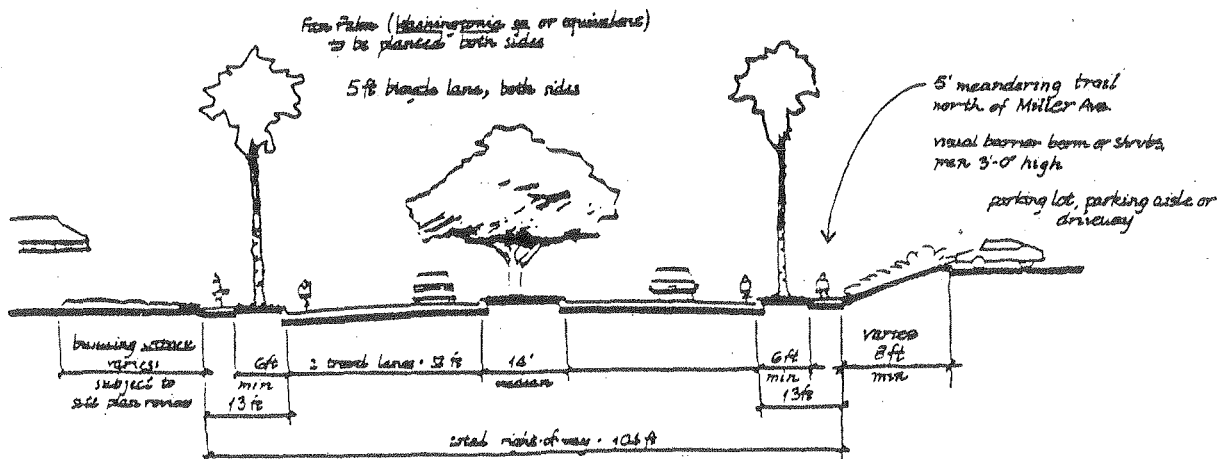
ETIWANDA AVENUE



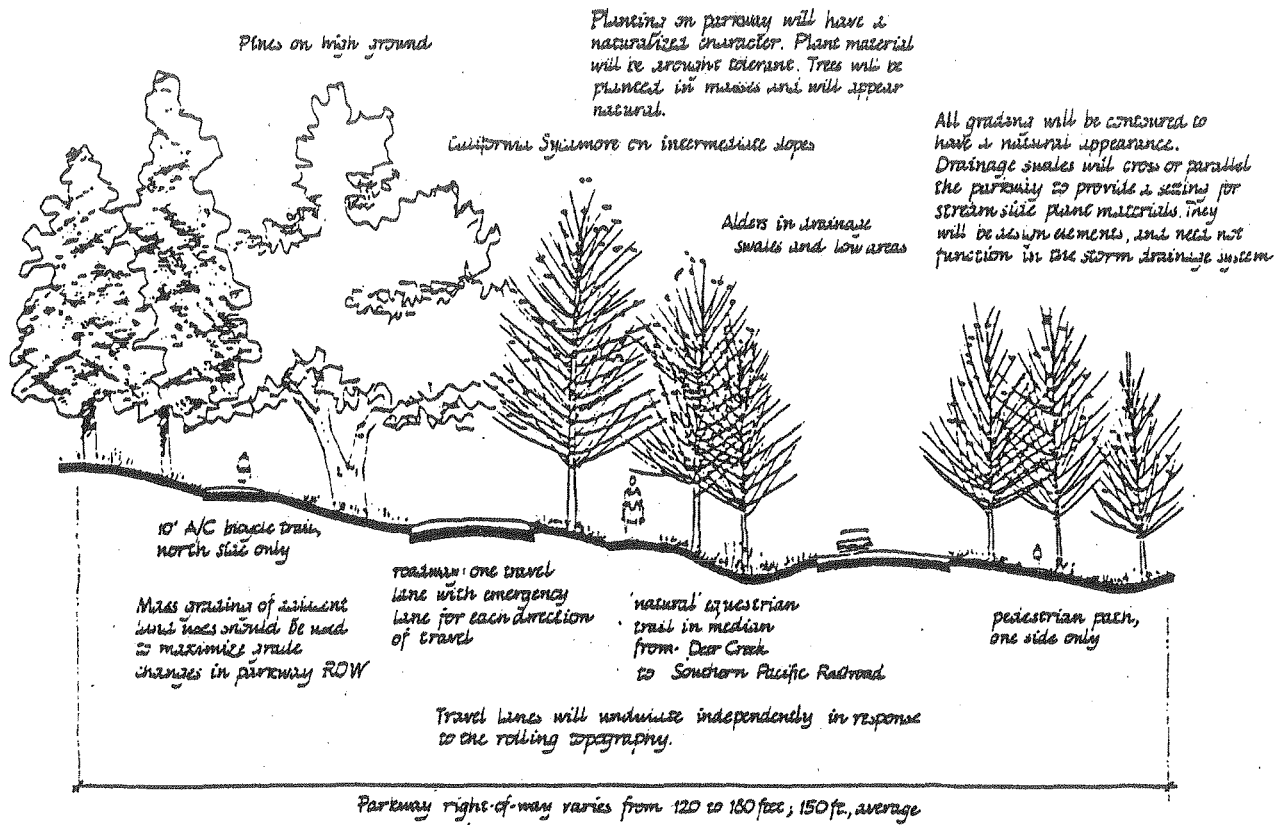
VICTORIA STREET



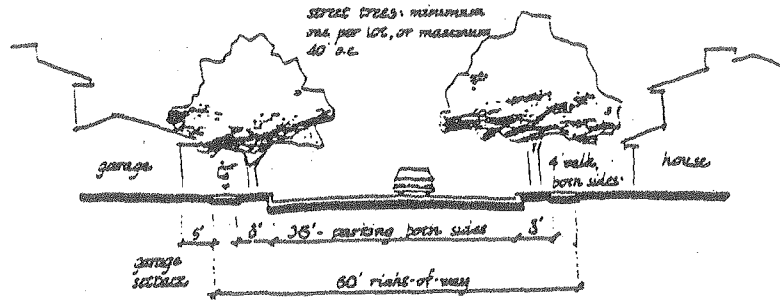
MILLER VICTORIA LOOP



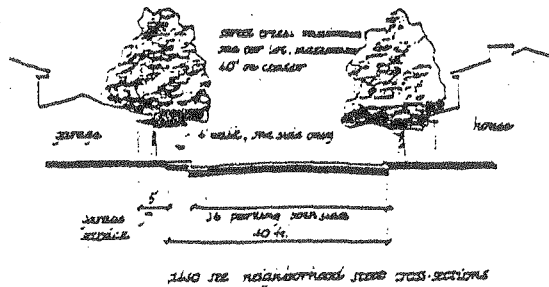
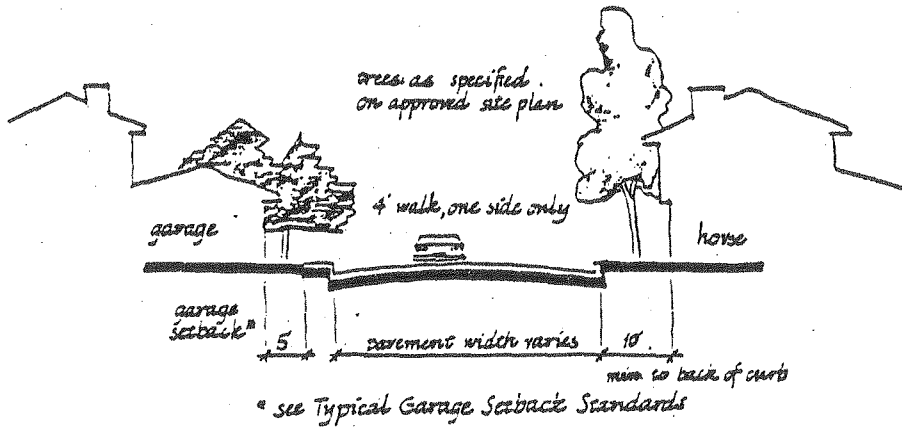
VICTORIA PARKWAY



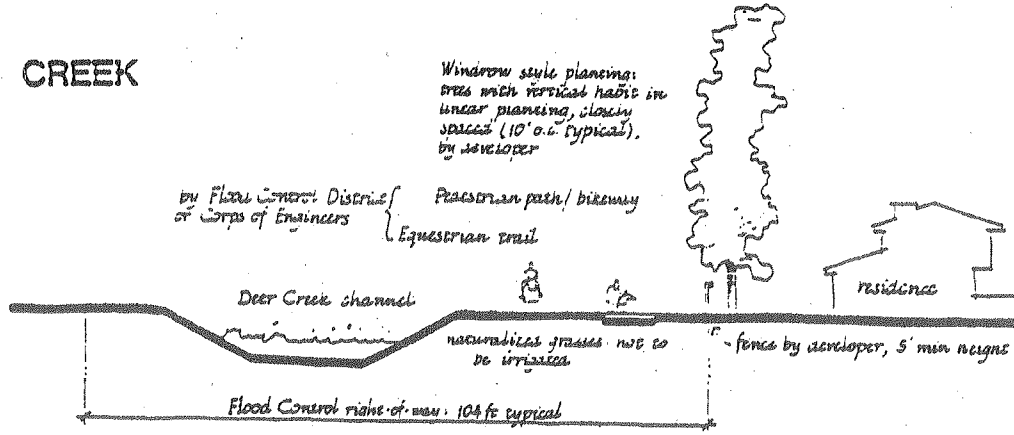
LOCAL-RESIDENTIAL ROAD



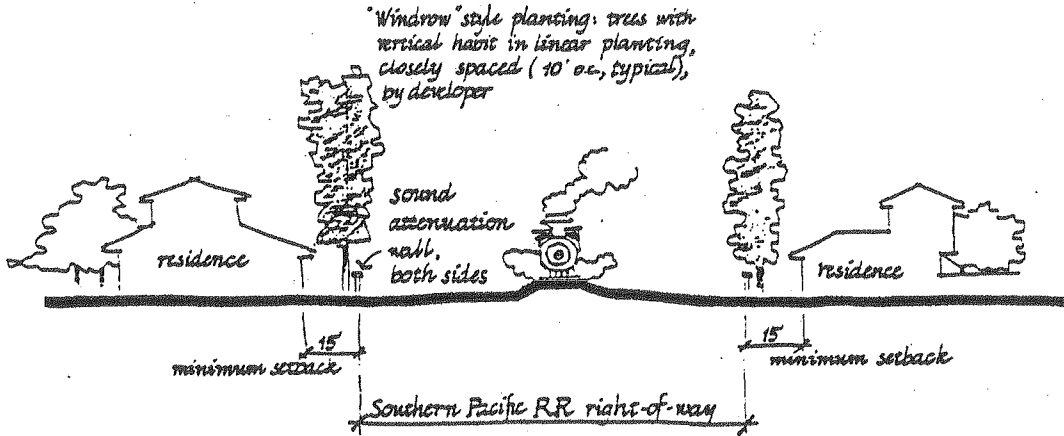
NEIGHBORHOOD STREETS



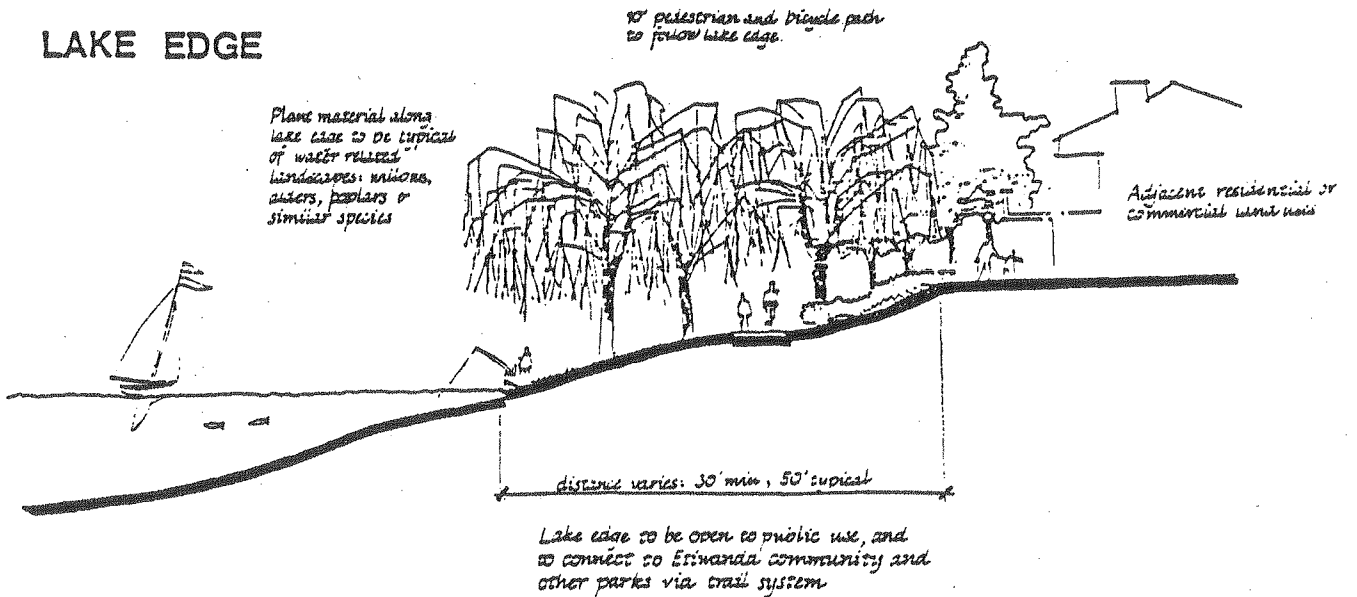
DEER CREEK



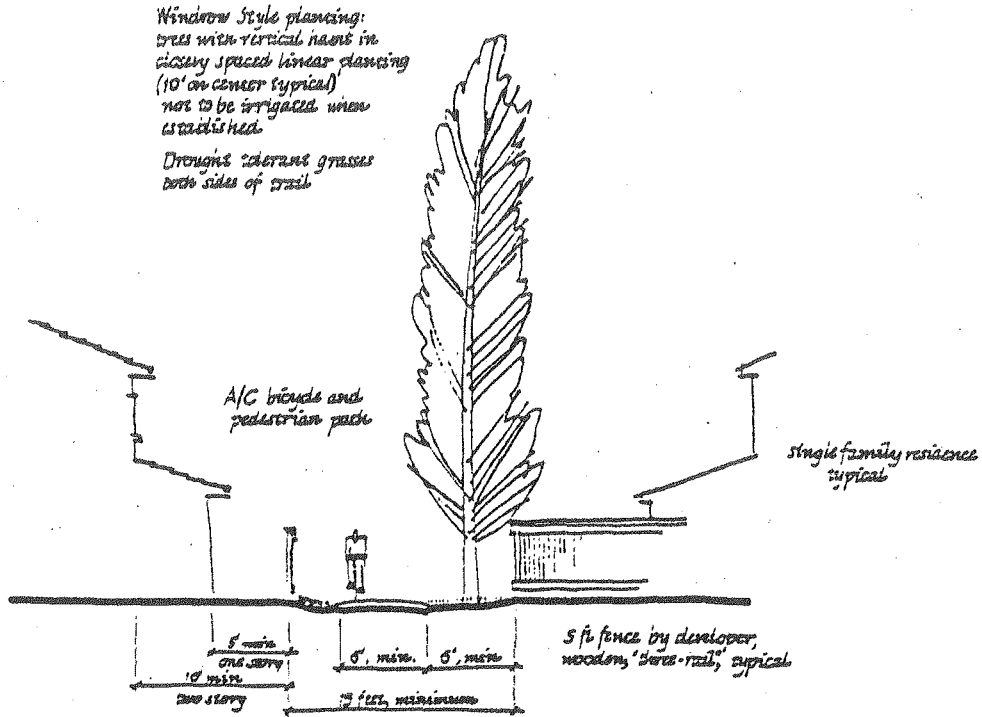
SOUTHERN PACIFIC RAILROAD



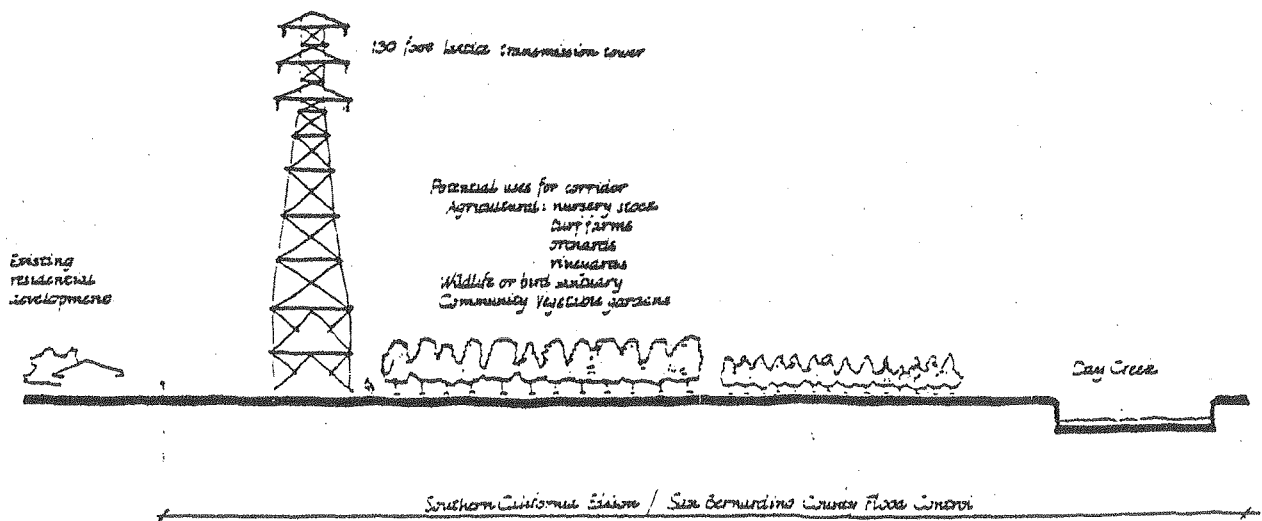
LAKE EDGE



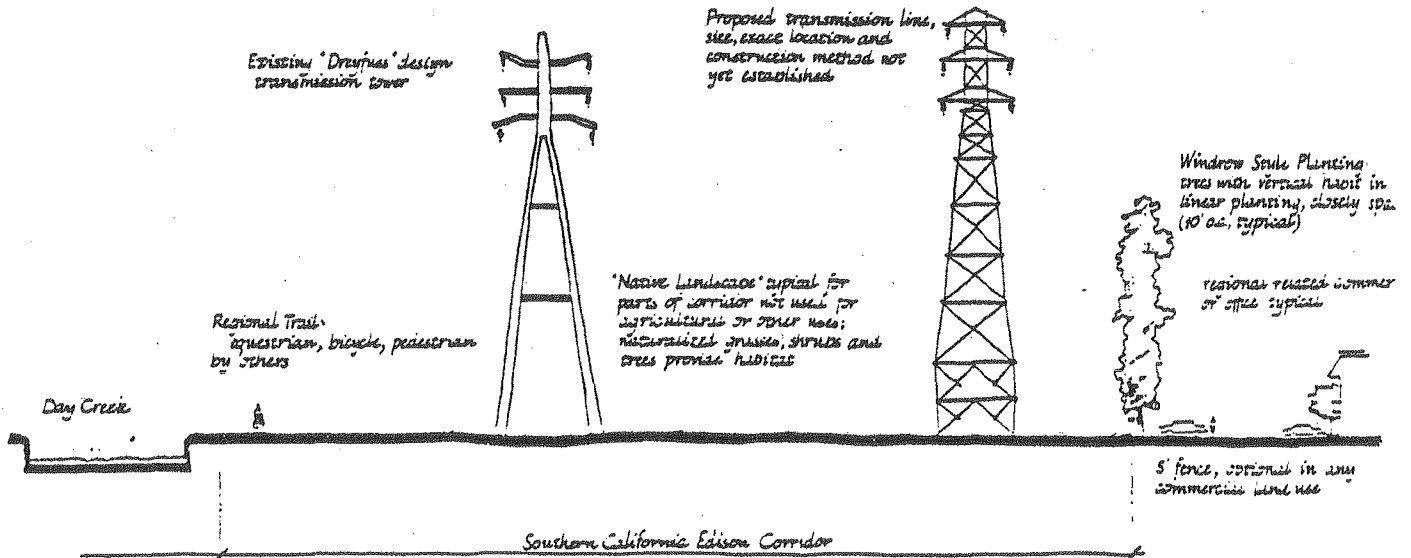
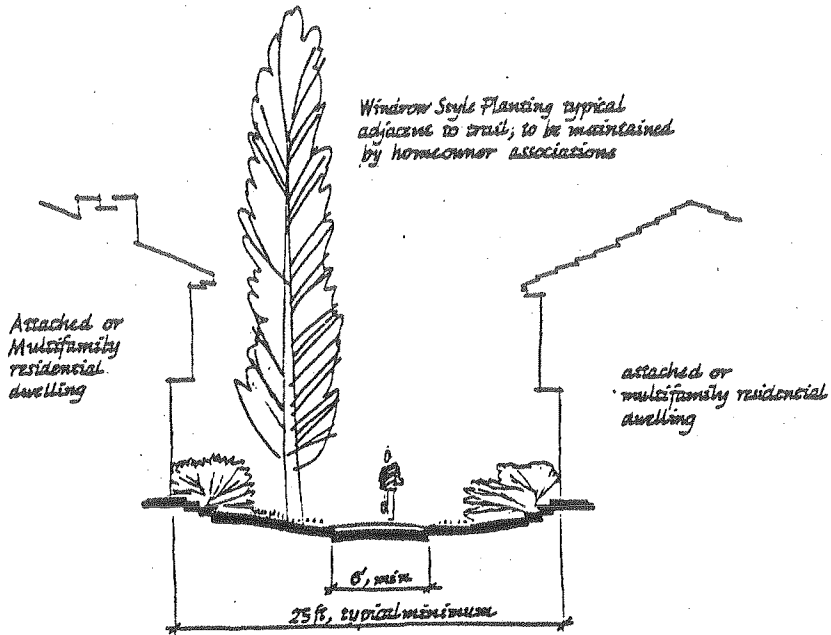
COMMUNITY TRAIL : Through "LM" Land Use



DAY CREEK / SCE



COMMUNITY TRAIL: Through Other Residential Areas



LANDSCAPE GRADING STANDARDS

Grading is often overlooked as a way to achieve an integrated community design. The purpose of this section is to provide general criteria for landscape grading within Victoria. These criteria are intended to create a pleasant aesthetic environment by working together with landscape planting, circulation, and land use as well as other elements of the total community. The concept drawings in this section are intended to show general conditions and are not keyed to specific locations. They are intended to serve as guidelines that can be used in evaluating proposed grading plans.

MASS GRADING TO ENHANCE COMMUNITY DESIGN

The land within the planning area is a gentle, 4 to 5 percent South facing slope, falling 4 to 5 feet in each 100 horizontal feet. To create large, flat buildable sites, mass grading will be used throughout the planned community. When properly done, this type of grading not only serves engineering considerations, but can be used creatively to enhance the quality and character of the community. By concentrating grade changes in open space areas such as Victoria Parkway, the local parks, or within the community trail system, relatively small grade changes can be used to bring greater visual interest to the community.

GRADE TO CREATE A NATURAL EFFECT

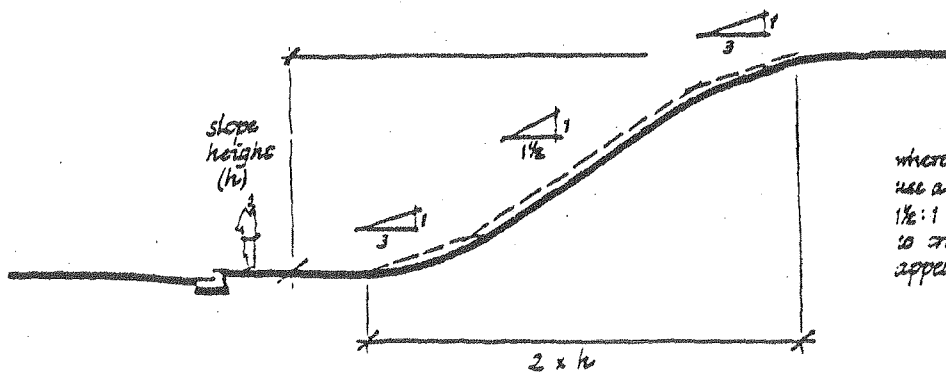
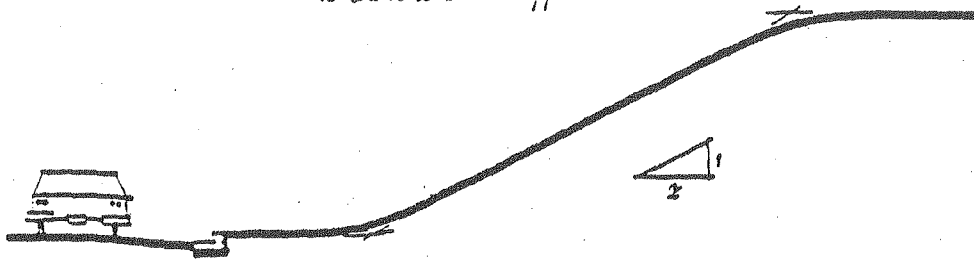
The aesthetic goal of all grading operations will be to imitate natural landforms. Long, monotonous, unchanging slopes with severe breaks in slope have an unnatural man-made appearance, and should be avoided. Rolled edges with gently sloping transition areas are more desirable and

pleasing to the eye. However, excessive grade manipulation should also be avoided, for it also creates "unnatural" land forms. Berms can be a useful and beautiful way to solve many landscape problems, but they should be large enough to actually have a strong visual impact, or they could have an undesirable "dumped wheelbarrow" appearance. Man-made hillsides with this problem also have an unnatural "mashed potatoes" visual effect.

Landscaped planting should use grade changes imaginatively, accenting or de-emphasizing the change in grade as necessary to achieve community design goals. Circulation elements such as trails and paths can effectively respond to grade conditions by meandering in long graceful curves. In contrast, walks that switch direction too often in response to poorly conceived landscape berms, or walks that go up and down over small berms have an unnatural appearance and should be avoided.

GRADE TO CREATE A NATURAL APPEARANCE

*"roll" the edges of all 2:1 slopes
to create a natural appearance*

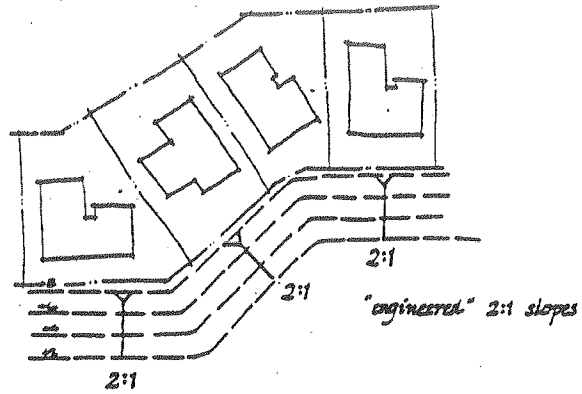


*where soil conditions permit,
use a combination of 3:1 and
1 1/2:1 slopes with rolled edges
to create a more natural
appearing transition in grade*

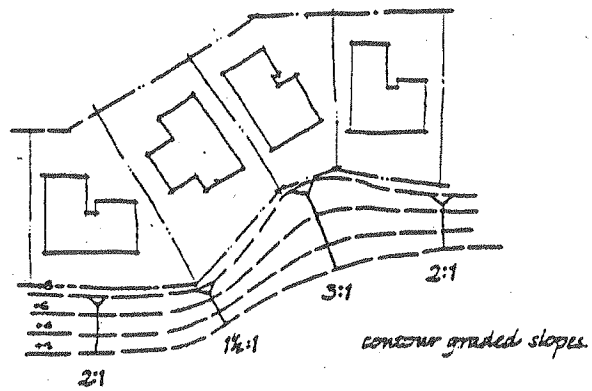
CONTOUR GRADING

PLAN - NO SCALE

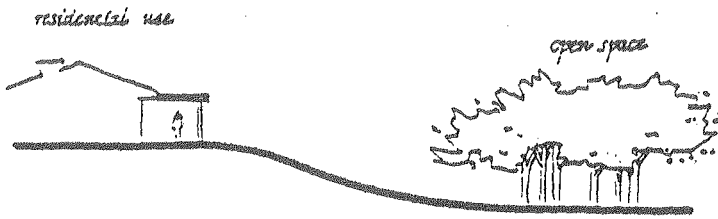
Avoid long, continuous, "engineered" appearing slopes that have hard edges and no transition areas at the top or the foot of the slope.



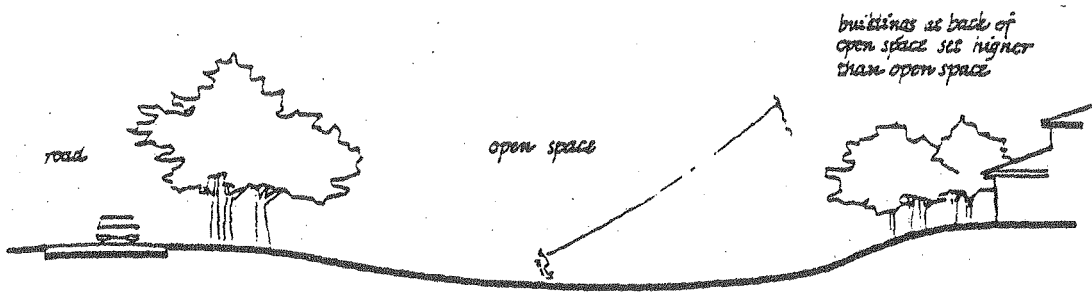
Use contour grading, varying slopes when possible, to create a more natural appearing slope.



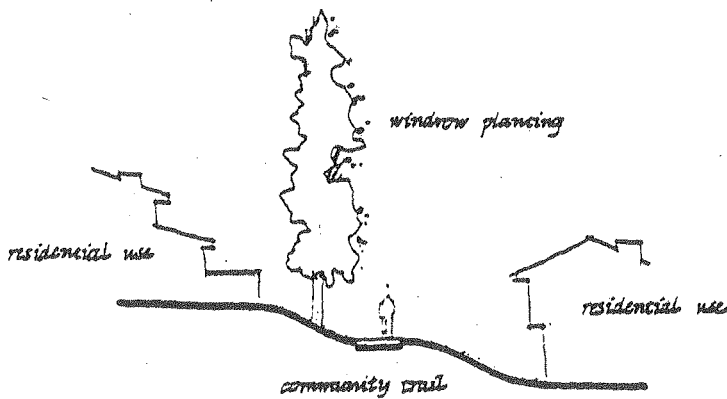
GRADE OPEN SPACE TO CREATE VIEWS AND TAKE UP GRADE



When possible, set grade of residential dwellings so that they overlook attractive open spaces such as parks, schools, lakes, etc.

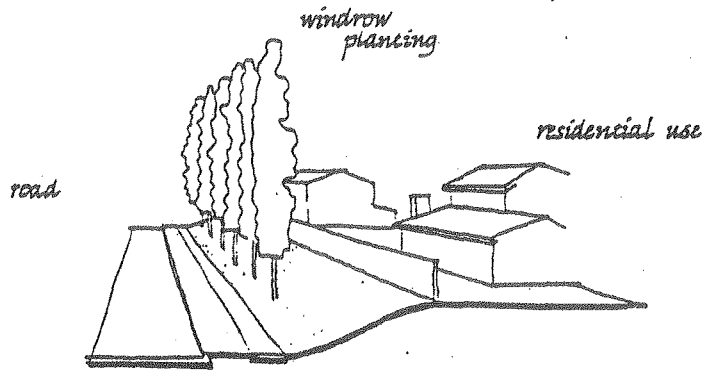


Set grade of roads so that long views into adjacent open spaces are created

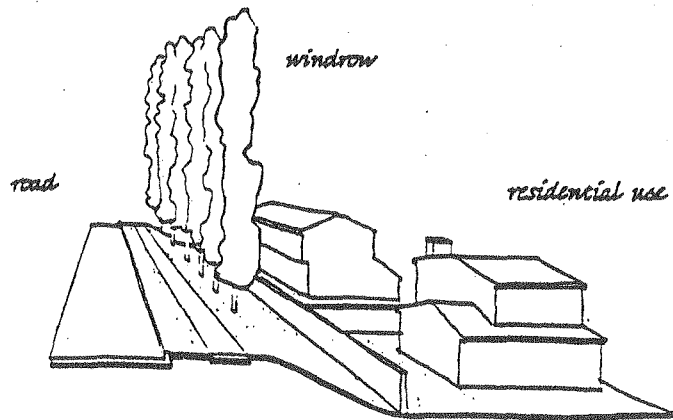


Use transition spaces between adjacent land uses to take up grade

GRADE ARTERIALS TO BUFFER OR OPEN VIEWS



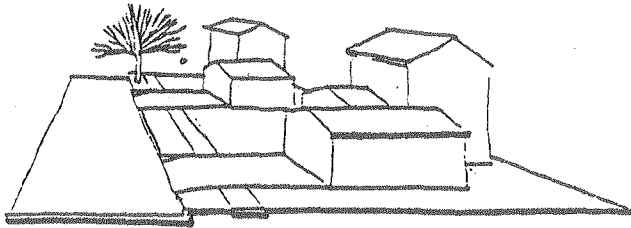
Raising grade of residential areas relative to roadway allows good exposure of landscaped area to persons on road or walk



Lowering grade of residential areas relative to road will permit views to distance

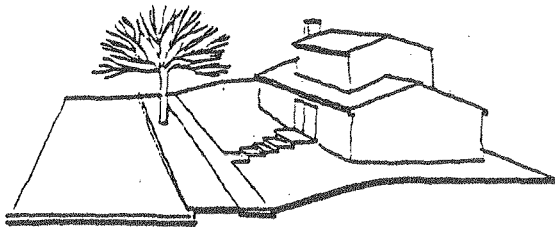
GRADE ROADS TO PROVIDE VISUAL INTEREST

FRONT LOADED DWELLING FACING ROAD



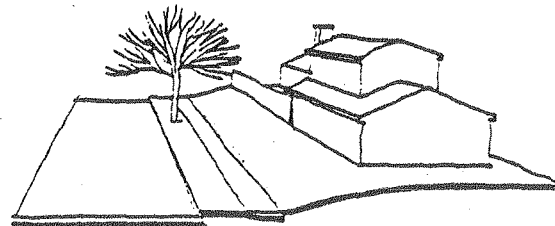
Building pads set just high enough above road to drain properly to minimize impact of driveways on visual quality of streetscape

REAR LOADED DWELLING FACING ROAD



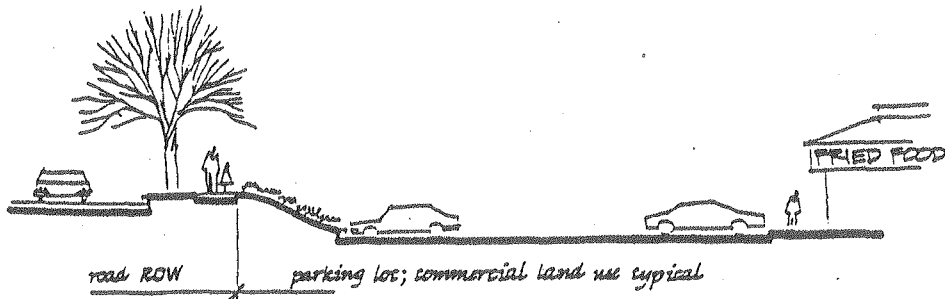
Building pads elevated above road grade to maximize views of landscaped yards

ANY DWELLING WITH SIDE OR REAR ELEVATION FACING ROAD

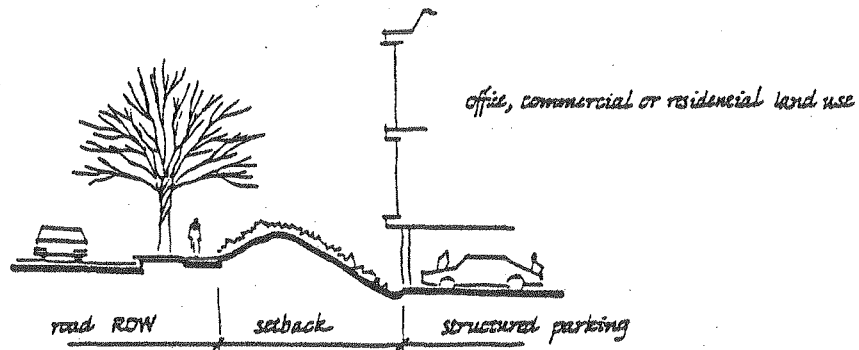


Building pads elevated above road to maximize views of landscaped edges.

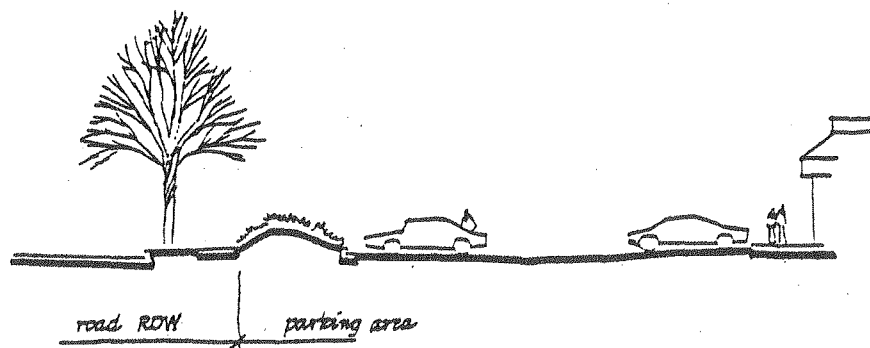
GRADE PARKING AREAS TO SOFTEN VISUAL IMPACT



A parking area that is depressed slightly below the grade of the adjacent road will minimize the visual impact of the parking lot without obstructing views of signs, etc.



A low berm will screen structured parking and can be economically justified if the berm is created during preliminary grading operations.



A low berm with grass or shrubs will screen parking areas.

IV
SOLAR ACCESS
STANDARDS

SOLAR ACCESS STANDARDS

The Solar Access Standards illustrate the desirable site plan relationships in providing passive solar access to residential dwellings within Victoria.

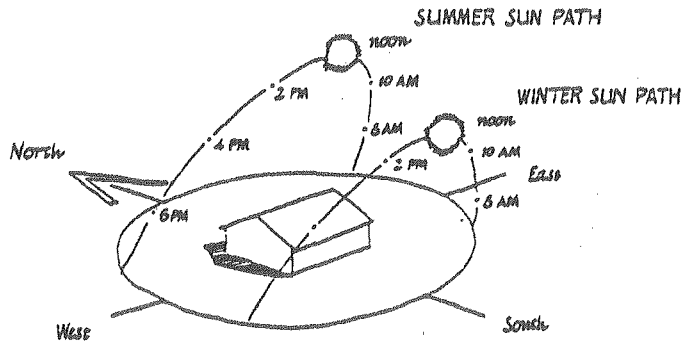
The following schematic diagrams show a few of the more important ways that a site plan can better respond to the sun. The concepts illustrated are mostly "passive" solar guidelines. That is, they do not involve technical or sophisticated hardware in order to work. "Passive" systems involve orienting buildings properly, planting trees in the right places, seeing that roof overhangs are adequate, or making sure that walls and ceilings are insulated properly. Passive systems presently return a large benefit in energy savings for a modest investment of time and resources. This plan encourages the use of these passive systems in site planning and architectural design.

"Active" systems may become more popular as energy costs rise, and integrating these "active" systems into the site planning process will depend on the design of the individual "active" system. "Active" systems are not required by this plan, though in some instances they are cost effective at today's energy prices. As "active" systems become more cost effective, their use in Victoria will become more common, and adjustments in site plans will need to be made that depend on the nature of the specific system proposed.

Each site plan should respond to passive solar access in as many ways as possible. The concepts offered here illustrate several of the many devices that may be used to achieve this. Trade-offs often need to be made among the many dependent variables involved, including road patterns, lot

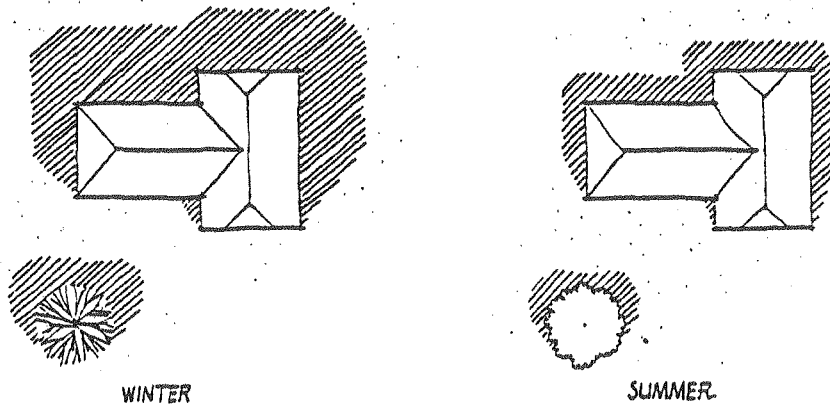
size, residential density, the adjacent land uses, engineering considerations and natural constraints. The recent focus on energy conservation and solar energy has made solar access a significant new variable in the site planning process, and these standards will assure that dwellings in Victoria are oriented properly.

SOLAR ANGLES/SKY SPACE



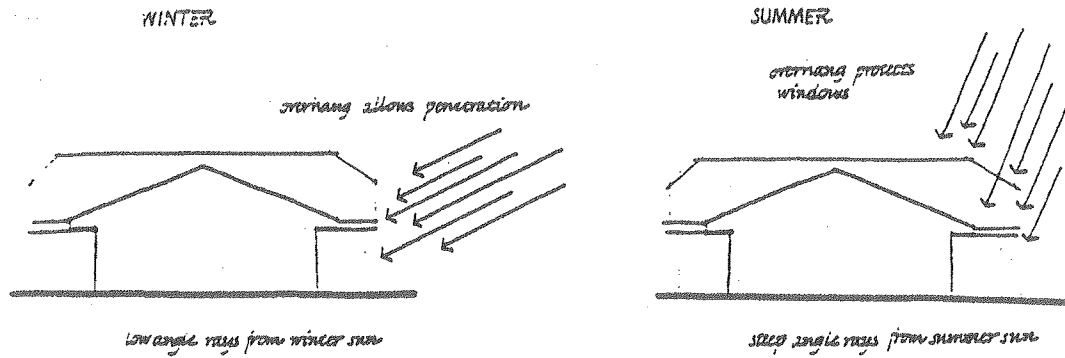
The area shaded by a residential dwelling varies by season and time of day. The maximum shaded area at mid-day occurs on the winter solstice (Dec 21), when the sun is low in the sky. The shaded area is smallest on the summer solstice (June 21)

SHADOW PATTERNS



The shadow cast by a typical single family dwelling varies greatly from winter to summer. The drawings illustrate the shaded area that would occur from 10 AM to 2 PM. Where solar access is important, the critical shadow patterns should be illustrated to aid in the satisfactory placement of buildings, windows or planes.

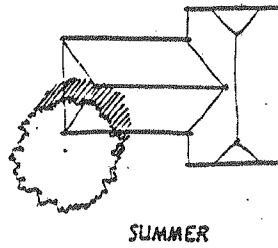
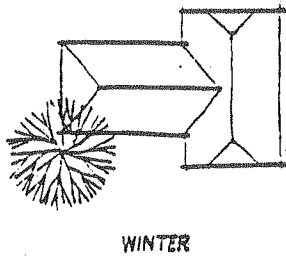
ROOF OVERHANG



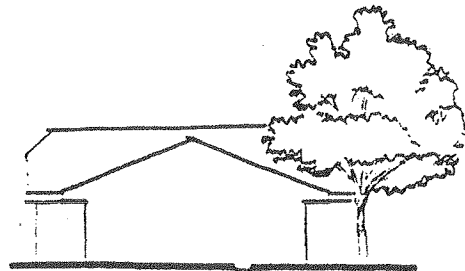
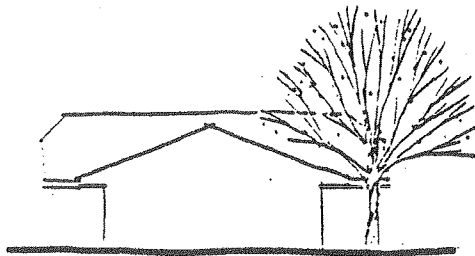
When possible, roof or building overhangs should allow direct solar access in the winter, but shade window areas in the summer. Windows exposed to the low angle, late afternoon rays of the summer sun should be shaded by planes or architectural elements whenever practical.

DECIDUOUS TREES

PLAN  NORTH



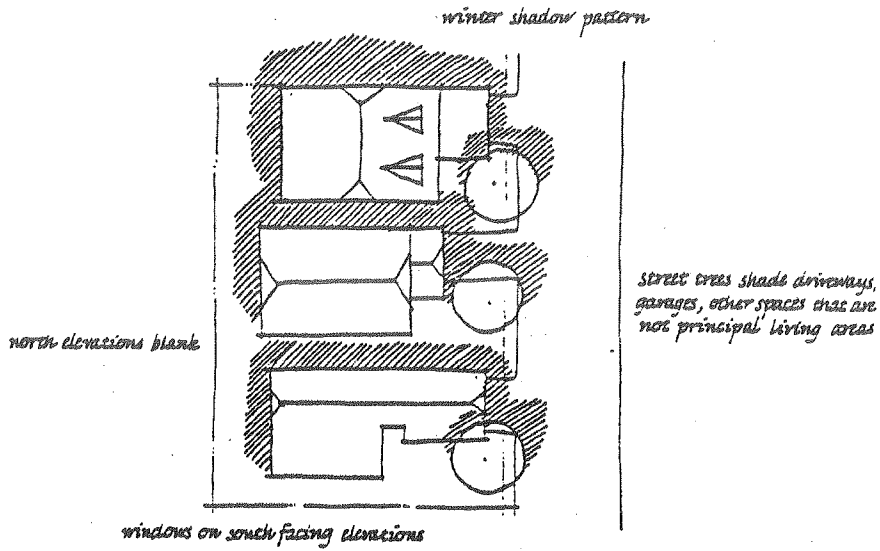
SECTION



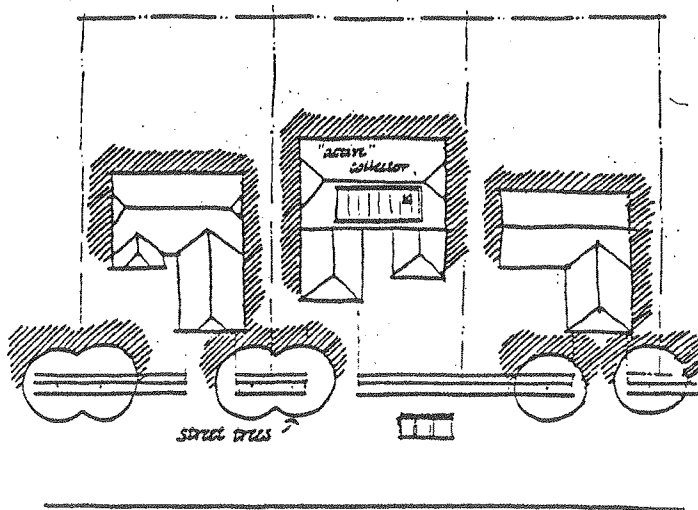
Deciduous trees can provide shade in the hot summer months and will still allow solar access in the winter months. Trees should be carefully planned for optimum exposure during both seasons.

STREET AND BUILDING LAYOUT

PLAN



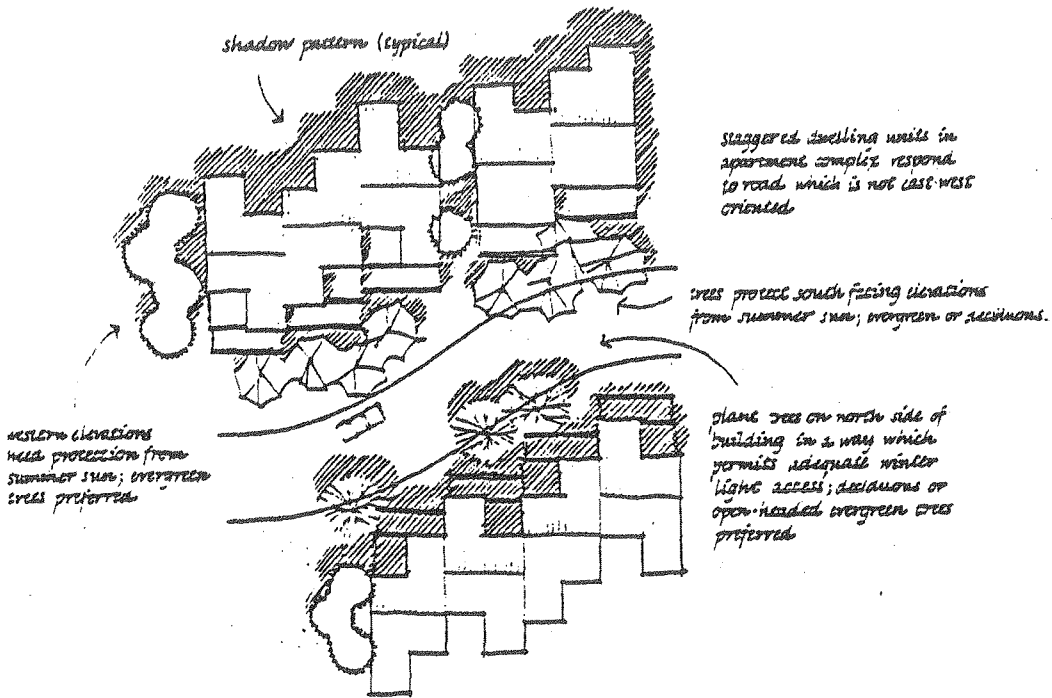
On some residential dwelling types a predominantly north-south road system will provide better solar access. It is desirable to have the long dimension of a dwelling aligned on an east-west axis with south facing windows. The zero-lot-line homes above are arranged to take advantage of their generally long, narrow form. Trees are arranged so that they do not shade living areas when direct solar access is desirable.



Whenever possible, site dwellings to minimize shading of adjacent buildings. The goal is to provide each dwelling with sufficient solar access during the winter. Active solar collectors must be protected from intrusions into their sky space that would significantly affect their performance. Group street trees when necessary to protect or enhance the solar access of private dwellings.

SOLAR ACCESS FOR MULTI-FAMILY DWELLINGS

PLAN



PART 3

I
REGULATIONS &
STANDARDS

VICTORIA COMMUNITY PLAN: REGULATIONS AND STANDARDS FOR DEVELOPMENT

This section is established to provide specific regulations and standards that all development in the Victoria Community Plan must meet. It is to be used in tandem with the Urban Design Criteria illustrated in Part Two, which complement these regulations and standards. To make use of this section more convenient, an outline is provided below.

- A. General Standards and Regulations
- B. Affordable Housing
- C. Project Phasing
- D. Community Facilities and Open Space:
Construction and Maintenance
- E. Provisions for Meeting City Park Requirements
- F. Distribution of Residential Dwelling Units
- G. The Planning Area and Planned Community Boundary
- H. Flexibility in Managing The Plan
- I. Legal Description of the Planned Community
- J. Residential Development Standards
 - 1. General to all residential areas: setbacks, fences, temporary uses, garage and carport placement
 - 2. Low Density Residential
 - 3. Low - Medium Density Residential
 - 4. Medium Density Residential
 - 5. Medium - High Residential
 - 6. High Density Residential
- K. Commercial Standards
 - 1. General to all commercial Land Uses: setbacks, site development standards
 - 2. Regional Center
 - 3. Regional - Related Land Uses
 - 4. Village Commercial

L. Community Facilities

M. Road Standards

1. Introduction and Intent
2. Standard Road Cross-Sections Reference Plan
3. Standard Road Cross-Sections
 - a. Milliken, Highland, Baseline, Foothill
 - b. Day Creek Boulevard
 - c. Etiwanda
 - d. Victoria Street
 - e. Miller, Victoria Loop
 - f. Victoria Parkway
 - g. Local Residential Streets
 - h. Neighborhood Streets

8. At such time as the site plans are considered, the developer shall submit plans demonstrating conformance with the Rancho Cucamonga Noise Ordinance and the Noise Element of the General Plan. The plans are subject to the approval of the City Planner.
10. Dedication and improvements of all rights-of-way shall meet with the approval of the City Engineer and be consistent with the adopted Circulation Element of the City of Rancho Cucamonga General Plan.
11. Prior to the approval of any tract map, geologic investigation reports may be required by the Director of Community Development.

AFFORDABLE HOUSING

"Affordable" housing will be provided throughout Victoria to meet the housing needs of families with low and moderate incomes. For the purposes of this discussion, two definitions are important:

- . An "affordable dwelling unit" is any residential dwelling purchased or rented by a low or moderate income family.
- . A low or moderate income family is one whose gross annual income is from 80% to 120% of the median family income for the region. The index used to determine median family income should be representative of the area in which the "affordable" housing will be built.

Within the Victoria Community Plan Area, fifteen percent (15%) of dwellings will be delivered to low and moderate income families. They will be dispersed throughout Victoria so that in each Village, approximately fifteen percent (15%) of the homes will be "affordable". "Affordable" housing will include a mix of attached, detached, and multi-family dwellings. The actual percentage of affordable dwellings within a Village, and the specific mix of units may vary somewhat from Village to Village.

It is the intent of this plan that "affordable" housing be delivered to the original occupants. Providing these dwellings at the time Victoria is developed assures that a large stock of "affordable" housing will be maintained through time.

Each landowner will be responsible for contributing his fair share of the total "affordable" units to be built in Victoria. To accomplish this, 15% of the units assigned by the Density Distribution Plan to a landowner's property will be "affordable". If a landowner is not able to provide these units on his property, he may arrange with other landowners to have them supply his share of the "affordable" dwellings. This should not, however, preclude the dispersal of "affordable" housing throughout Victoria, nor does it mean that each development project must contain fifteen percent "affordable" dwellings.

As an incentive to build this housing, a bonus of one dwelling unit will be awarded to each landowner for every "affordable" dwelling he delivers. This bonus would be added to the number of units assigned to a landowner's property by the Density Distribution Plan.

PROJECT PHASING

Construction of Victoria will begin in Victoria Windrows, and development is currently planned to proceed west along Victoria Parkway into Victoria Vineyards and Victoria Groves. When construction of the Regional Center begins, development will proceed south along Victoria Parkway into Victoria Lakes.

It is presently anticipated that, in the short term, demand for single family detached dwelling units will outpace demand for attached or multi-family residences. Therefore, residential development in the Low - Medium Density Residential category may begin in Victoria Vineyards before Victoria Windrows is completely developed. As demand for attached and multi-family homes increases relative to detached homes, development of these types of residential products will begin in Victoria Windrows and is currently planned to proceed west into Victoria Vineyards and Victoria Groves. Development in Victoria Lakes will begin at the same time as the regional center.

The construction of open space including parks, schools, lakes and parkways will be phased to coincide with residential construction so that increments of open space will be developed coincidentally with increments of housing.

COMMUNITY FACILITIES AND OPEN SPACE: CONSTRUCTION AND MAINTENANCE

Generally, the maintenance of all facilities designed for community wide public use such as parks, paseos, parkways and roadside planting belts, and other facilities will be by the city wide maintenance district. Facilities that are within attached residential projects that are intended for residents of, that project only will be maintained by a homeowners association. Land belonging to the private, public and quasi-public agencies, such as the San Bernardino Flood Control District, Southern California Edison and the Southern Pacific Railroad will be maintained by those landowners.

PROVISIONS FOR MEETING CITY PARK REQUIREMENTS

Victoria Park Lane, Victoria Groves Park, the parks in Victoria Vineyards, Windrows Park, the park/lakes in the Village of Victoria Lakes, and Paseos will be dedicated public land and will be maintained by a city wide maintenance district. They will receive 100% credit toward meeting the park requirements of the City of Rancho Cucamonga.

Privately owned and maintained open space that is provided for the use of the general public may receive up to 100% credit toward meeting the park requirements of the City of Rancho Cucamonga.

Privately owned open space that is not provided for the use of the general public may receive up to 100% credit toward meeting the park requirements of the City of Rancho Cucamonga and shall meet the requirement of Ordinance 105 of the City of Rancho Cucamonga.

The following chart summarizes the distribution of park acreage in Victoria. It is based on actual and projected areas dedicated to park and open space.

	Victoria Groves	Victoria Vineyards	Victoria Windrows	Victoria Lakes	Total by Park Type
Neighborhood Park	6	21	3	5	40
Victoria Park Lane	4	20	12	10	46
Community Trails	8	13	5	3	29
Lakes*				19	19
Total by Village	18	54	25	37	134 acres

* Lakes (Water Surface Area) = 13 Acres
 Lake Edge Trail = 6 Acres

Note: Acreages are rounded to the nearest acre.

DISTRIBUTION OF RESIDENTIAL DWELLING UNITS

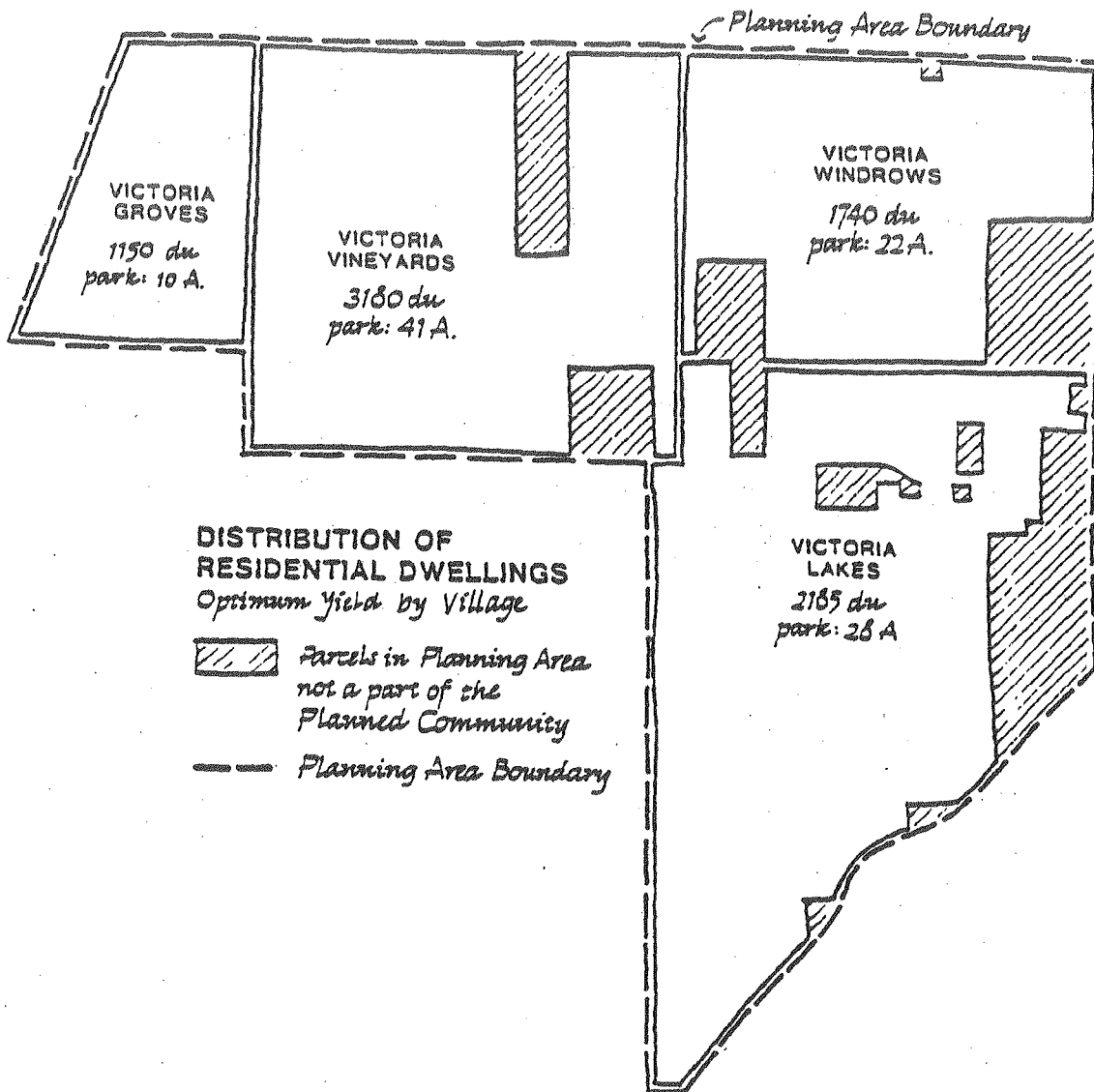
The plans on the following pages show the proposed distribution of the residential dwelling units within the four villages of Victoria. The actual number of residential dwellings within each village may vary somewhat according to the formula discussed in the next section, "Flexibility in Managing the Plan".

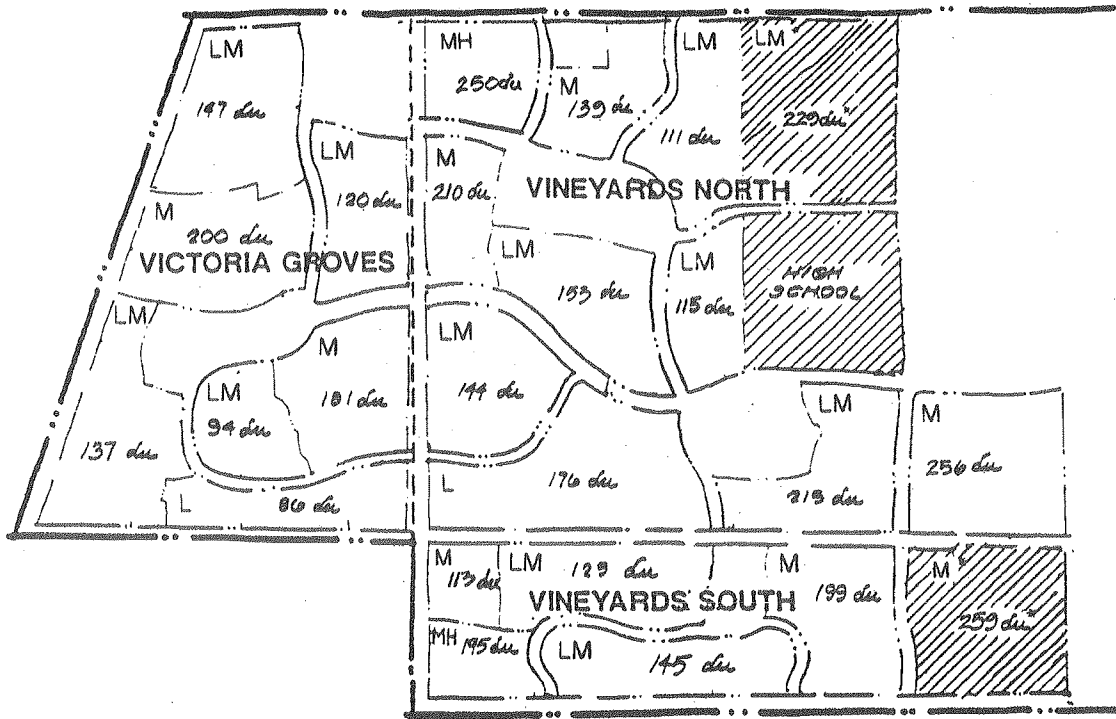
The total number of units shown for Victoria Lakes does not include any dwellings that may be placed in the area designated by an "S" on the Land Use Plan. If that area is developed as a Medium Density Residential Land Use or as a mobile home park, the maximum number of units permitted in Victoria Lakes within the Planned Community Boundary will be increased by an additional 100 units. Such increases in dwelling units pursuant to either, or both, of the above mentioned cases are in addition to the 8,255 dwelling units planned within the Planned Community Boundary.

PLANNING AREA AND PLANNED COMMUNITY BOUNDARY

The Planning Area is described and located on pages 5 and 6. The Planned Community is the area that is legally included within the Planned Community and a legal description of the Planned Community follows the Section, Flexibility in Managing the Plan.

The planning area includes approximately 280 acres of privately held land that is not a part of the Planned Community. These areas are shown graphically on the following page. The dwelling units assigned to each village on this diagram apply only to land within the Planned Community and do not include any dwellings that may be developed on parcels within the planning area that are not included within the Planned Community. Development of these parcels is controlled by the City of Rancho Cucamonga General Plan. However, for informational purposes and to assist in the planning and design of the whole planning area, some assumptions were made about the number of dwellings which might be developed on this land. Those assumptions are shown on the Density Distribution Plan, and are suggested as a guide for the City in planning these parcels. Based on these assumptions, the optimum yield for the entire planning area is 8865 dwelling units. 8255 dwellings are within the Planned Community Boundary, however.





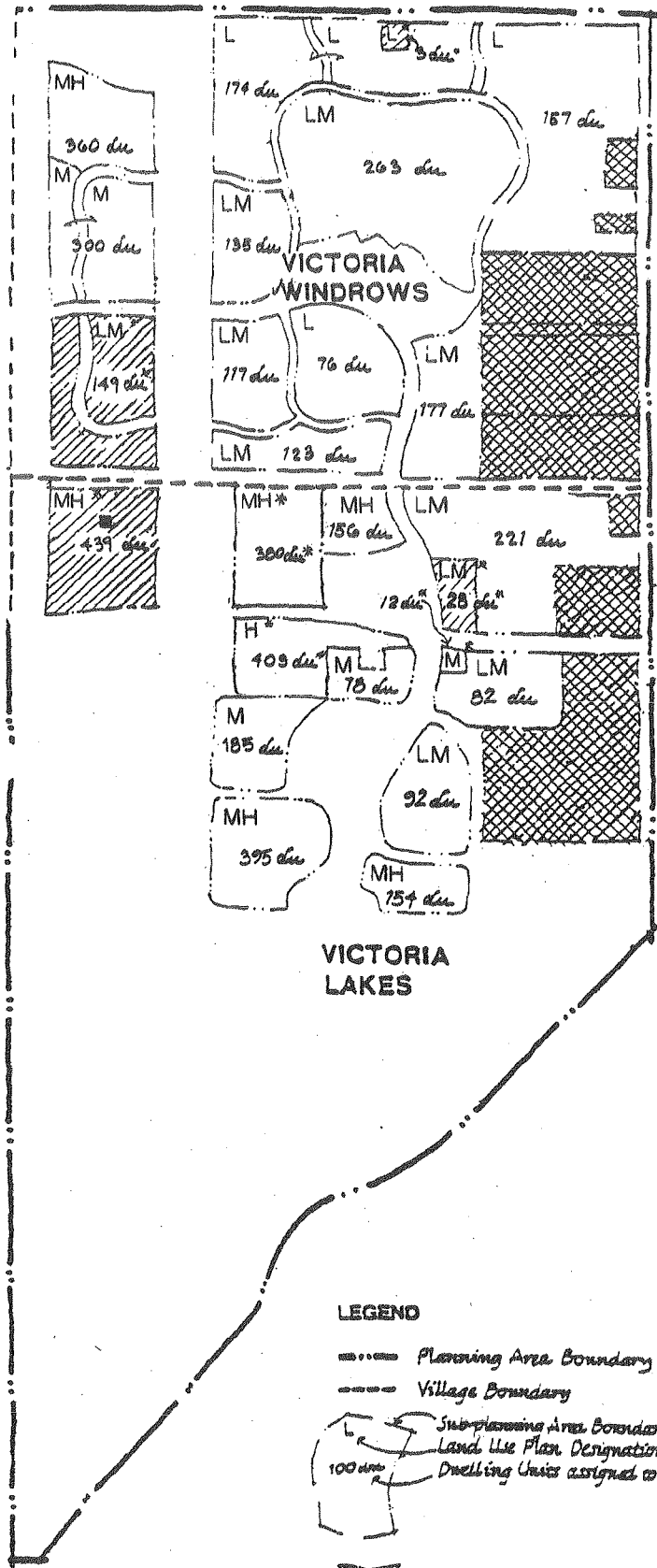
DENSITY DISTRIBUTION PLAN


Village	Land Use Designation					PC Total	added to PC by PC Ord. # 362	Planning Area Total
	L 2-9 du/acre	LM 4-8 du/acre	M 6-14 du/acre	MH 14-24 du/acre	H 24-30 du/acre			
Victoria Groves	810	490	301			965	-	965
Victoria Vineyards	176	1012	917			2570	488	3038
Victoria Windrows	407	815	300			1882	152	2034
Victoria Lakes		275	243	105		207	1363	2631
	689	2710	1861	1510		207	6760	8668
added to PC Ord. # 362	3	406	271	819	409	1908		
Planning Area Total	672	3126	2132	2329	409			8668


Dwelling units located on property owned by The William Lyon Company → 6760 / 1908
 Dwellings on property of other land owners →

* Land use designation and dwelling unit count for informational purposes only

■ Potential Mass Transit stop: if developed as residential, add 300 du to Victoria Lakes, 100 within PC and 100 in Planning Area



 Parcels not a part of the Original Planned Community (Amended by Ord. 362)

 Parcels part of the Etowanda Specific Plan

FLEXIBILITY IN MANAGING THE PLAN

In the future, conditions may arise that suggest rearrangements in the Land Use Plan designations for individual sub-planning areas as shown on the Land Use Plan and the Density Distribution Plan. The Community Plan is designed to provide flexibility in trading the residential densities in one sub-planning area for densities in other sub-planning areas to be developed in the future. The Community Plan must also accommodate change caused by political, economic, or market conditions, without detrimentally affecting the ability to construct the total Victoria community. By utilizing the Optimum Yield Approach in determining residential densities, such flexibility can occur within the design intent of this Community Plan without changing either the character and quality of each Village, or the total number of units designated for the Community Plan Area. Each Village can accommodate some change by increasing or decreasing the number of residential dwelling units in that Village without altering the intent of this plan.

Accordingly, the Optimum Yield Approach allows a landowner, within certain limitations, to redesignate the Land Use Plan designations for sub-planning areas he owns. In other words, a landowner may increase the number of dwelling units in one sub-planning area if he decreases the number of dwelling units in another sub-planning area he owns as long as he maintains the total number of dwelling units assigned by the Density Distribution Plan to his property. Additionally, density can be transferred from one landowner to another by mutual agreement, provided that other requirements set forth in this section are met.

To provide flexibility which maintains the integrity of the Community Plan and which promotes the goals and desires of the City of Rancho Cucamonga, the following guidelines shall apply to the transfer of density within the Community Plan Area.

1. The number of dwelling units proposed on the Density Distribution Plan may be increased for any sub-planning area. However, such increases should not alter the intent of this plan to provide a variety of residential housing types and densities throughout each Village and the entire Community Plan Area. Additionally, the number of units proposed for any subplanning area may be decreased without a corresponding increase in another subplanning area.

2. The Land Use Plan designation for any sub-planning area may be redesignated as follows:
 - a. "L" land use designations can be redesignated to "LM" land use designations for any sub-planning area.
 - b. "LM" land use designations can be redesignated to either a "M" or "MH" land use designation for any sub-planning area.
 - c. "M" land use designations can be redesignated to "MH" land use designations for any sub-planning area.
 - d. "MH" land use designations can be redesignated to "H" land use designations for any sub-planning area.

3. In order to provide flexibility in the development of the Victoria Planned Community over the 10-15 year life of the Plan, a maximum of 20% variation in the optimum yield may be allowed.

Any and all variations shall be reviewed and approved by the Design Review Committee, the Planning Commission, and the City Council.

The variation in the number of dwelling units within one village may require corresponding decreases in one or more of the other villages to insure that the total number of units do not exceed 8255 within the Planned Community boundary.

LEGAL DESCRIPTION OF THE PLANNED COMMUNITY

Those portions of Sections 31 and 32 in Township 1 North, Range 6 west of the San Bernardino meridian according to the township plat approved by the Surveyor General dated November 13, 1855 together with that portion of Section 36, Township 1 North, Range 7 west of the San Bernardino meridian according to the township plat approved by the Surveyor General dated June 20, 1884 together with those portions of Sections 5 and 6 in Township 1 South, Range 6 west of the San Bernardino meridian according to the township plat approved by the Surveyor General dated April 19, 1884 and that portion of Section 8, Township 1 South, Range 6 west of the San Bernardino meridian according to the Township Plat approved by the Surveyor General dated November 3, 1873, all in the City of Rancho Cucamonga, County of San Bernardino, state of California, described as follows;

Beginning at a point on the southerly line of the northwest quarter of said Section 8, distant easterly 60 feet from the southwest corner of said northwest quarter; thence northerly along a line that is parallel with and distant easterly 60 feet from the westerly line of said northwest quarter to the southerly line of the northerly 1650 feet of said northwest quarter; thence westerly 60 feet along said southerly to the westerly line of said Section 8; thence northerly along said westerly line to the northwest corner of said Section 8; thence westerly 370 feet along the southerly line of said Section 6; thence northerly along a line that is parallel with and distant westerly 370 feet from the easterly line of said section to the easterly prolongation of the southerly line of Lot 1 of Tract No. 8369 recorded in Book 118, pages 36 to 39 of map books, records of said county; thence westerly to the southeast corner of said Lot 1; thence northerly along the easterly line of said Tract No. 8369, the easterly line of Tract No. 8805 recorded in Book 126, pages 61 and 62 of map books records of said county and the easterly line and its northerly prolongation of Tract No. 8806 recorded in Book 130, pages 38 and 39 of map books, records of said county to the southerly line of said Section 31; thence westerly along said southerly line to the southwest corner of the easterly 430 feet of said Section 31; thence northerly along the westerly line of said easterly 430 feet to the southerly line of the Pacific Electric Railroad right-of-way; thence westerly along said southerly right-of-way line to the westerly line of the southeast quarter of said section being also the centerline of Rochester Avenue; thence southerly along said westerly line and said centerline to the southerly line of said Section 31; thence westerly along said southerly line to the southwest corner of said Section 31 and the centerline of Milliken Avenue; thence northerly along westerly line and said centerline to the southerly line of the Pacific

Electric Railroad right-of-way, thence westerly along said southerly line to the westerly line of Deer Creek Channel; thence northerly along said westerly line to the westerly line of the easterly 260 acres of said Section 36; thence northerly along said westerly line to the northerly line of said Section 36; thence easterly along the northerly line of Section 36 and the northerly line of said Section 31, to the northwest corner of the east half of the west half of the northeast quarter of said Section 31; thence southerly along the westerly line of said east half to the southwest corner thereof; thence easterly along the southerly line of said east half to the southeast corner thereof; thence northerly along the easterly line of said east half to the northeast corner thereof; thence easterly along said northerly line of Section 31 to the northeast corner thereof; thence southerly 50 feet along the easterly line of said Section 31; thence easterly along a line that is parallel and southerly 50 feet from the northerly line of said Section 32 to the west line of the east half of the west half of the northwest quarter of said Section 32; thence northerly 50 feet along said west line to the northerly line of said Section 32; thence easterly along said northerly line of Section 32 to the westerly line of Etiwanda Colony Lands as per map recorded in Book 2 of maps page 24, records of said county; thence southerly along said westerly line to the northwest corner of Lot 4 in block I of said Etiwanda Colony Lands; thence easterly along said northerly line to the northwest corner of the land described in the deed recorded in Book 5397, page 475 of official records; thence southerly along the westerly line of the land described in said deed to the southwest corner thereof; thence easterly along the southerly line of the land described in said deed to the southeast corner thereof; thence northerly along the easterly line of the land described in said deed to the northerly line of said block I; thence easterly along said northerly line to the northeast corner of said block I; thence southerly along the easterly line of said block to the northeast corner of the south 150.00 feet of the east 200.00 feet of Lot 8 in said block I; thence westerly along the northerly line of said south 150.00 feet to the west line of the east 200.00 feet of said block I; thence southerly along said westerly line to the southerly line of the northerly 100.00 feet of Lot 9 in said block I; thence easterly along said southerly line to said easterly line of block I; thence southerly along last mentioned easterly line to the northeast corner of the southerly 360.00 feet to the westerly line of the easterly 300.00 feet of said block I; thence southerly along said last mentioned westerly line to the northerly line of the southerly 200.00 feet of said Lot 9; thence easterly along last mentioned northerly line to the easterly line of said block I; thence southerly along last mentioned easterly line to the southeast corner of said Lot 9; thence westerly along the southerly lines of Lots 9 and 10 in said block I to the southwest corner of said Lot

10; thence southerly along the westerly line of Lot 15 in said block I, its southerly prolongation, the westerly line of Lot 2 in block J and the westerly line of Lot 7 in block J of said Etiwanda Colony Lands to the northeast corner of the southwest quarter of said Section 32; thence westerly along the northerly line of the southwest quarter of said southeast quarter to the westerly line of the easterly 40 feet of said southwest quarter; thence southerly along last mentioned westerly line to the northerly line of the Pacific Electric Railroad right-of-way; thence easterly along last mentioned northerly line to the westerly line of the easterly 200 feet of Lot 8 in said block J; thence southerly along said westerly line and its southerly prolongation to the southerly line of the northerly 50 feet of Lot 9 in said block J; thence westerly along last mentioned northerly line to the westerly line of the easterly 220 feet of said Lot 9; thence southerly along last mentioned westerly line to the southerly line of the northerly 250 feet of said Lot 9; thence easterly along last mentioned southerly line to the easterly line of said Lot 9; thence southerly along said easterly line to the southeast corner of said lot; thence westerly along the southerly line of said lot to the northeast corner of Lot 15 in said block J; thence southerly along the easterly line of said Lot 15 to the northerly line of the southerly 87.83 feet of said lot; thence westerly along last mentioned northerly line to the westerly line of the easterly 72 feet of said lot; thence southerly along said westerly line and its southerly prolongation to the northerly line of Lot 2 in block S of said Etiwanda Colony Lands; thence easterly along last mentioned northerly line to the northeast corner of said Lot 2; thence southerly along the easterly line of said lot to the southeast corner thereof; thence westerly along the southerly line of said lot to the northeast corner of Lot 6 in said block S; thence southerly along the easterly lines of Lots 6, 11 and 14 in said block S to the northerly line Etiwanda Cactus Acres recorded in Book 19 of maps, pages 63 records of said county; thence easterly along said northerly line to northeast corner of said tract; thence southerly along the easterly line of said tract to the northwesterly line of State Highway 31; thence southwesterly along said northwesterly line to the southerly line of block 6 of said Etiwanda Cactus Acres; thence westerly along last mentioned southerly line to the easterly line of Lot F of said Etiwanda Cactus Acres; thence southerly along said easterly line to the northwesterly line the right-of-way of Highway 31; thence southwesterly along said right-of-way to the southerly line of the northerly 50 feet of said Section 8; thence westerly along said southerly line to the northeast corner of the land described in the deed to the Southern California Edison Company recorded in Book 8279, page 62 of Official Records; thence southerly along the easterly line of the land described in said deed to the northwesterly line of the right-of-way 31; thence southwesterly along said

1

northwesterly right-of-way line to the southerly line of the northwest quarter of said Section 8; thence westerly along said southerly line to the point of beginning.

Also except the north 200 feet of the west 230 feet of the east 380 feet of Lot 4 in block S of said Etiwanda Colony Lands.

Also except the north 200 feet of the west 218 feet of the east 368 feet of Lot 3 in block S of said Etiwanda Colony Lands.

Also except that portion of lot 14 in block J of said Etiwanda Colony Lands, lying easterly of the easterly line of the westerly 330 feet of said lot.

Also except Lots 23 and 24 of Orange Empire Acres as per map recorded in Book 20 of maps page 1 records of said county.

Also except Lots 10, 11, 12, 13, 14, and that portion of Lot 9 of Orange Empire Acres as per map recorded in Book 20 of maps, page 1, records of said county lying westerly of the west line of the east half of the southwest quarter of said Section 32.

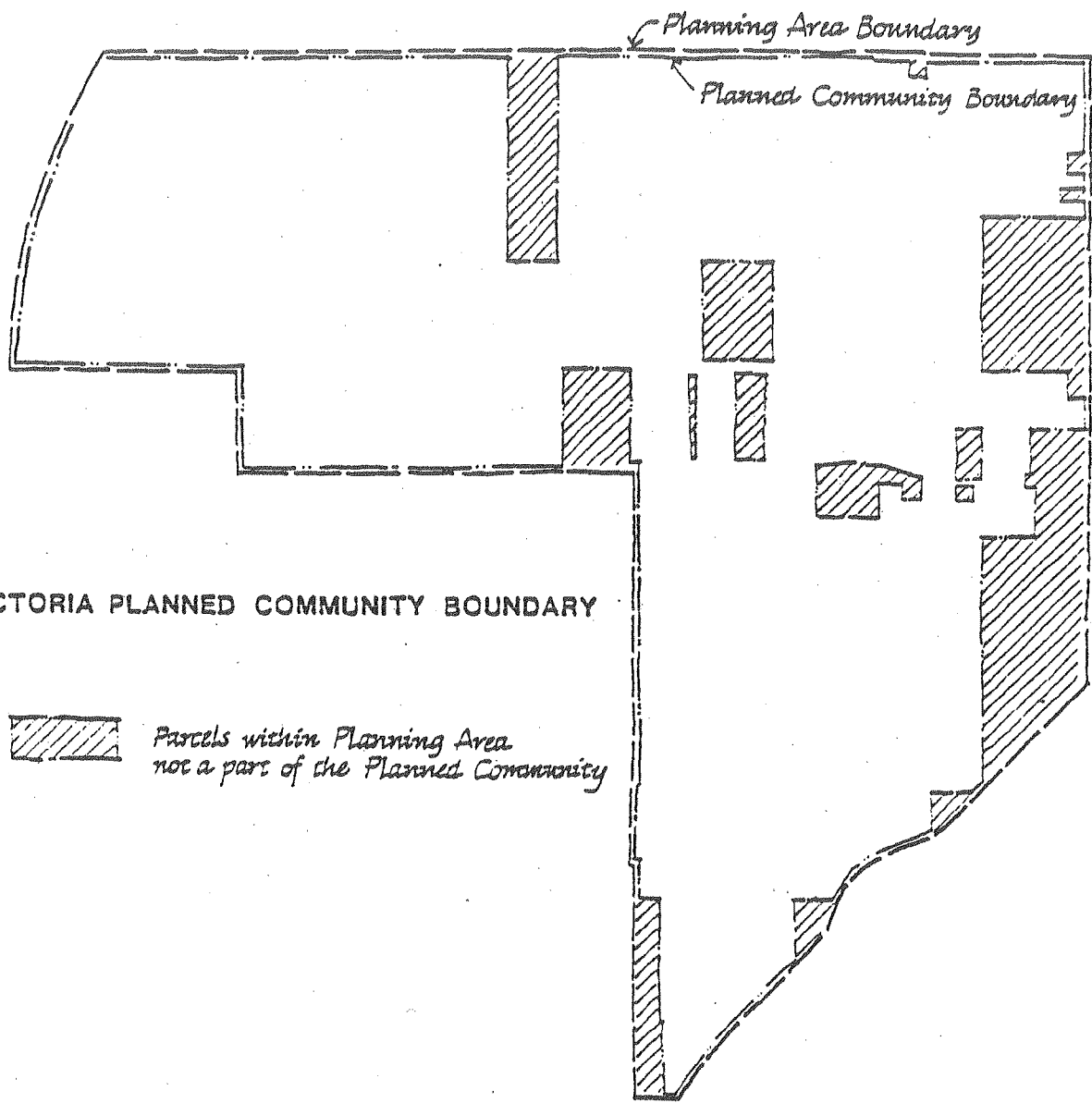
Also except that portion of Lot 19 of Orange Empire Acres as per map recorded in Book 20 of maps, page 1, records of said county lying easterly of the following described line;

Beginning at the intersection of the north line of said Lot 19 with a line that is parallel with and 270.00 feet easterly from the west line of Lot 18 of said tract; thence southerly to the intersection of the south line of said Lot 19 with a line that is parallel with and 300.00 feet easterly from the westerly line of said Lot 18.

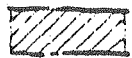
Also except that portion of Lot 13 in block J and the unnamed road 66.00 feet wide adjoining said lot on the south of said Etiwanda Colony Lands lying southerly of the following described line;

Beginning at the intersection of southerly line of Baseline Avenue 66 feet wide as described in the deed recorded in Book 1174 page 148 of Official Records with the westerly line of Lot 13 in block J of said Etiwanda Colony Lands; thence easterly along said southerly line to the northerly prolongation of the westerly line of the easterly 150 feet of Lot 4 in block S of said Etiwanda Colony Lands; thence southerly along said prolongation to the southerly line of said unnamed road.

Also except the northerly 652.25 feet of the easterly 800.01 feet of the northwest quarter of said Section 5.



VICTORIA PLANNED COMMUNITY BOUNDARY



*Parcels within Planning Area
not a part of the Planned Community*

RESIDENTIAL DEVELOPMENT STANDARDS

1. General Provisions For All Residential Areas

a. Building Setbacks from Common Areas:

Structures which abut a plaza, park, mall, greenbelt or other permanent open space may abut the common property line but have no openings onto such uses.

b. Fences, Hedges and Walls:

Fences, hedges and walls constructed as acoustical barriers shall have no height limit. All other fences shall be limited to 8 feet unless they are attached to a main building and are an architectural design element, in which case they can exceed 8 feet subject to design approval.

c. Trellis:

Open trellis and beam construction shall be permitted to attach the garage or carport to the dwelling and may also extend from the dwelling to the property line in the side, rear yards or front yards.

d. Garage and Carport Placement:

(1) Where garages or carports are entered directly from an alley, the setback may be zero (0) feet, provided at least 26 feet of clear area is provided directly behind the garage or carport to allow for adequate back-up clearance.

(2) See typical site plan diagrams, and Garage Setback Standards.

e. Minimum lot width:

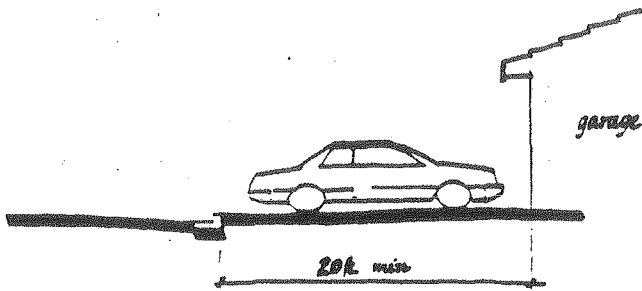
The minimum lot width on cul-de-sacs or knuckles shall not be less than 20 feet.

f. Recreational Vehicle Storage

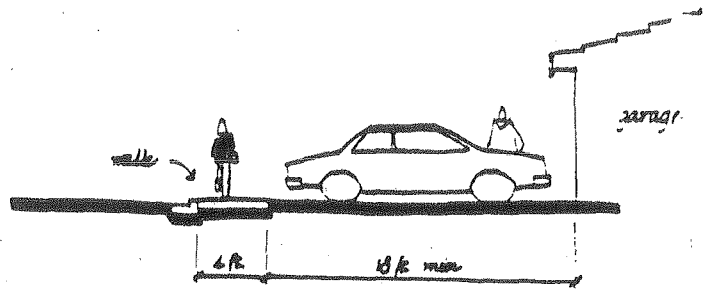
(1) Recreational Vehicle Storage shall be provided within the Planned Community for 25% of the lots or units contained with the "L", "LM", and "M" residential land use category. The percent requirement shall be reviewed on an annual basis and may be modified by the Planning Commission.

TYPICAL GARAGE SETBACKS

FROM CURB ONLY
(NO SIDEWALK)

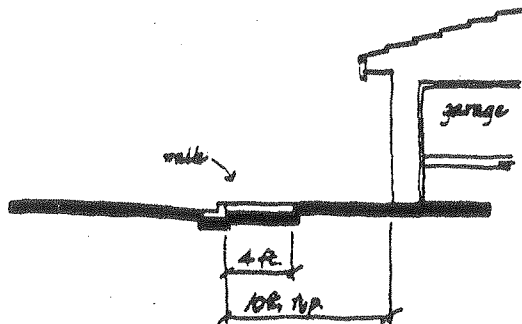


FROM CURB AND WALK

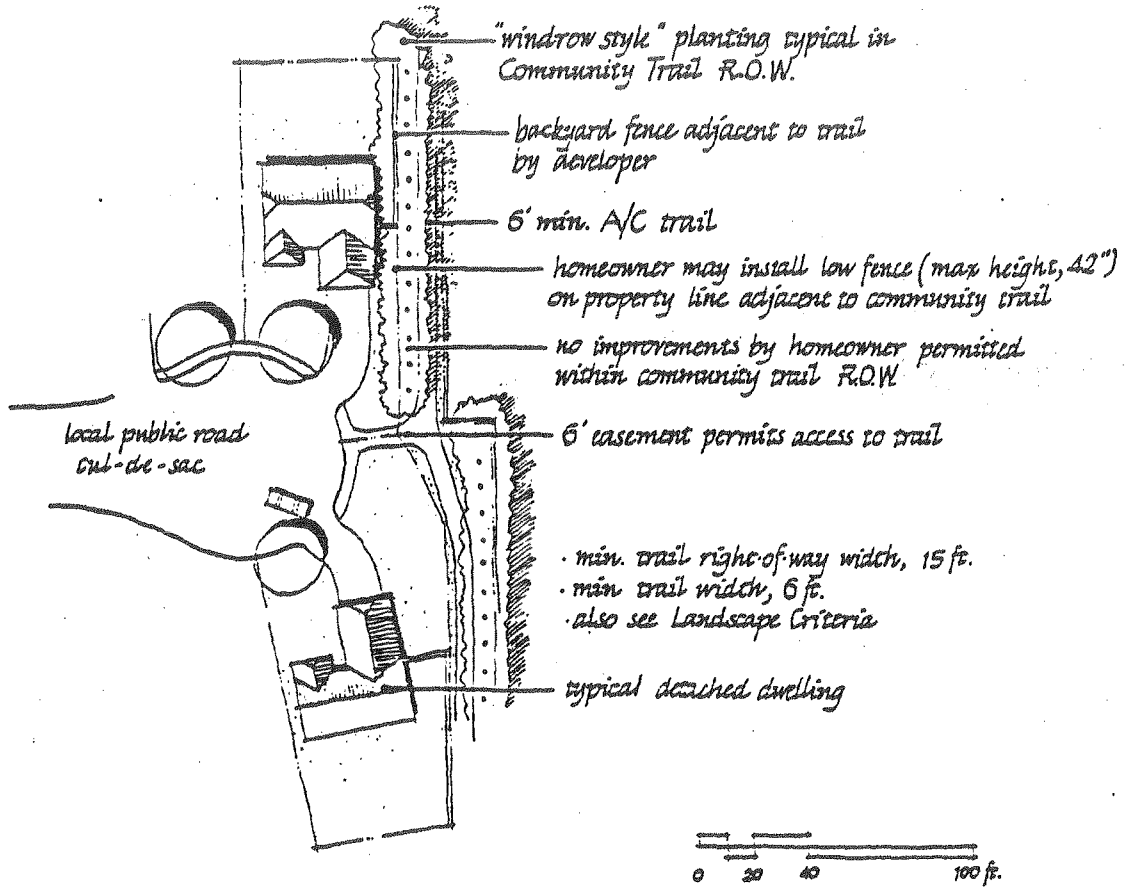


Site garages to discourage the storage of automobiles in a manner where they block public walks or roadways, and so that automatic garage door openers do not interfere with pedestrians.

SIDE ENTRY GARAGE



COMMUNITY TRAIL / CUL-DE-SAC CONNECTION



- (2) Recreational vehicle storage is permitted within the "L", "LM", and "M" residential land uses areas and shall be restricted by CC&R's subject to the approval of the City Attorney.

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

2. Low Density Residential ("L" Land Use Plan designation):

Land designated as Low Density Residential is intended for residential development that has a range of from 2 to 4 dwellings per adjusted gross acre. The following regulations apply:

- a. Uses Permitted: Detached or attached residential dwellings not exceeding four dwellings per adjusted gross acre, including, but not limited to:
 - (1) Single family detached dwellings
 - (2) Single family attached dwellings, including, but not limited to, duplexes and triplexes
 - (3) Cluster housing
 - (4) Community facilities as specified.
- b. Accessory Uses Permitted:
 - (1) Garages and carports, in compliance with site development standards provided herein
 - (2) Fences, walls, trellises
 - (3) Swimming pools with a minimum 5' high fence enclosing pool
 - (4) Accessory uses and structures necessary or customarily incidental to a principal use as permitted by the Rancho Cucamonga Zoning Ordinance
- c. Site Development Standards:
 - (1) Subdivisions:
 - (a) Building site area: see typical lot plans, pp. 208 - 211.
 - (b) Building site width: see typical lot plans, pp. 208 - 211.

- (c) Building coverage: see typical lot plans, pp. 208 - 211.
 - (d) Building setbacks: see typical lot plans, pp. 208 - 211.
 - (e) Building height: 35 feet maximum
- (2) Cluster Housing
- (a) Building site area: .3 acres minimum
 - (b) Building site coverage: 50%, maximum
 - (c) Building setbacks and building separation
 - . no minimum subject to Development Approval Process (Part III, Section II-A)
 - . buildings should be situated in a manner that provides adequate visual clearance at intersections
 - (d) Building height: 35 feet, maximum

"L" LAND USE

detached cluster dwellings
single lot or multiple lot subdivision
minimum lot size: 8000 s.f.

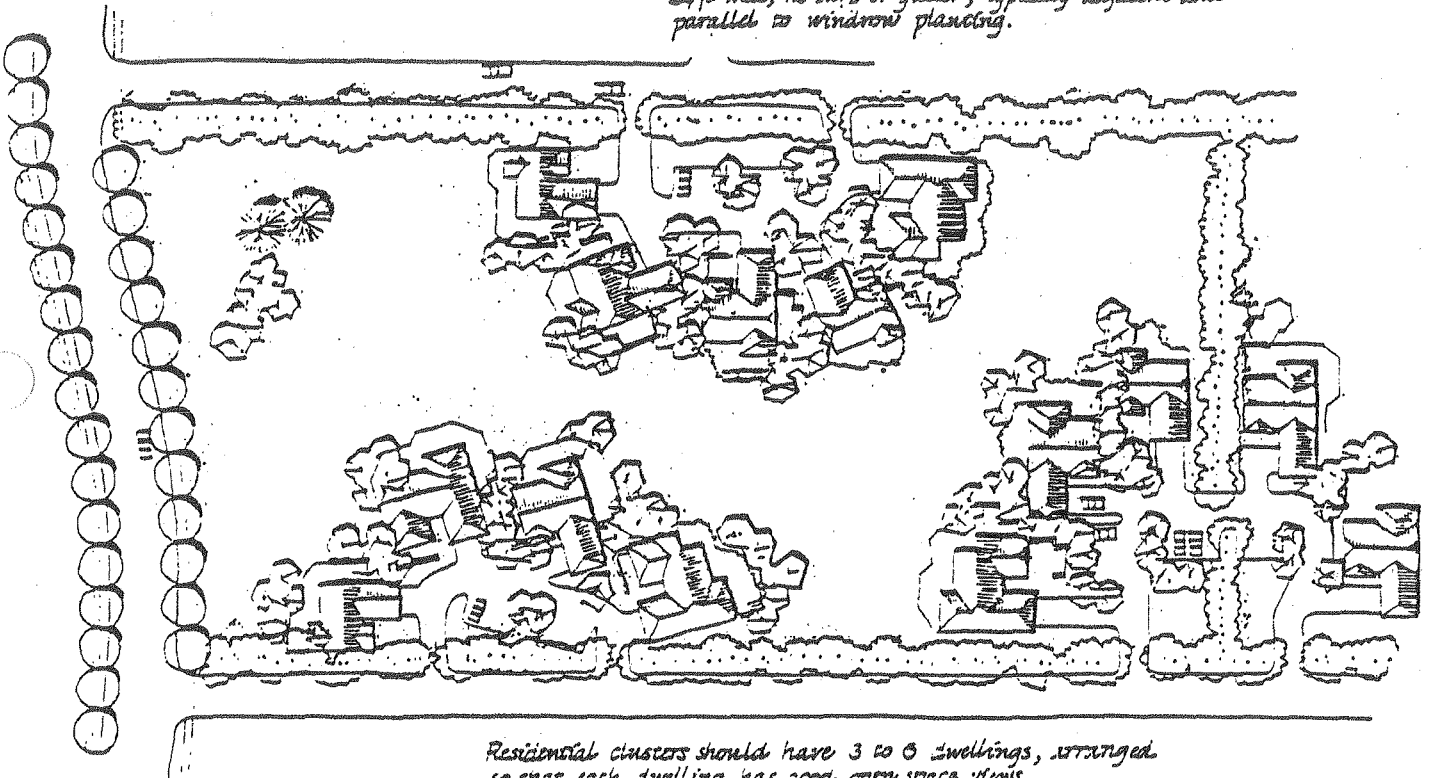


Development in this category can be either a single lot or multiple lot subdivision. The clustering assures preservation of much open space; each area should be designed as a whole to insure continuity of architecture and landscape, and to provide each dwelling with views and privacy.

Window spacing 330 x 660 ft; Use existing tree framework, place new windows on traditional spacing.

Offset dwellings to minimize views into adjacent dwellings

Local private street provides access to parking courts: 24 ft wide, no curb or gutter; typically adjacent and parallel to window planting.



Residential clusters should have 3 to 6 dwellings, arranged so that each dwelling has good open space views.

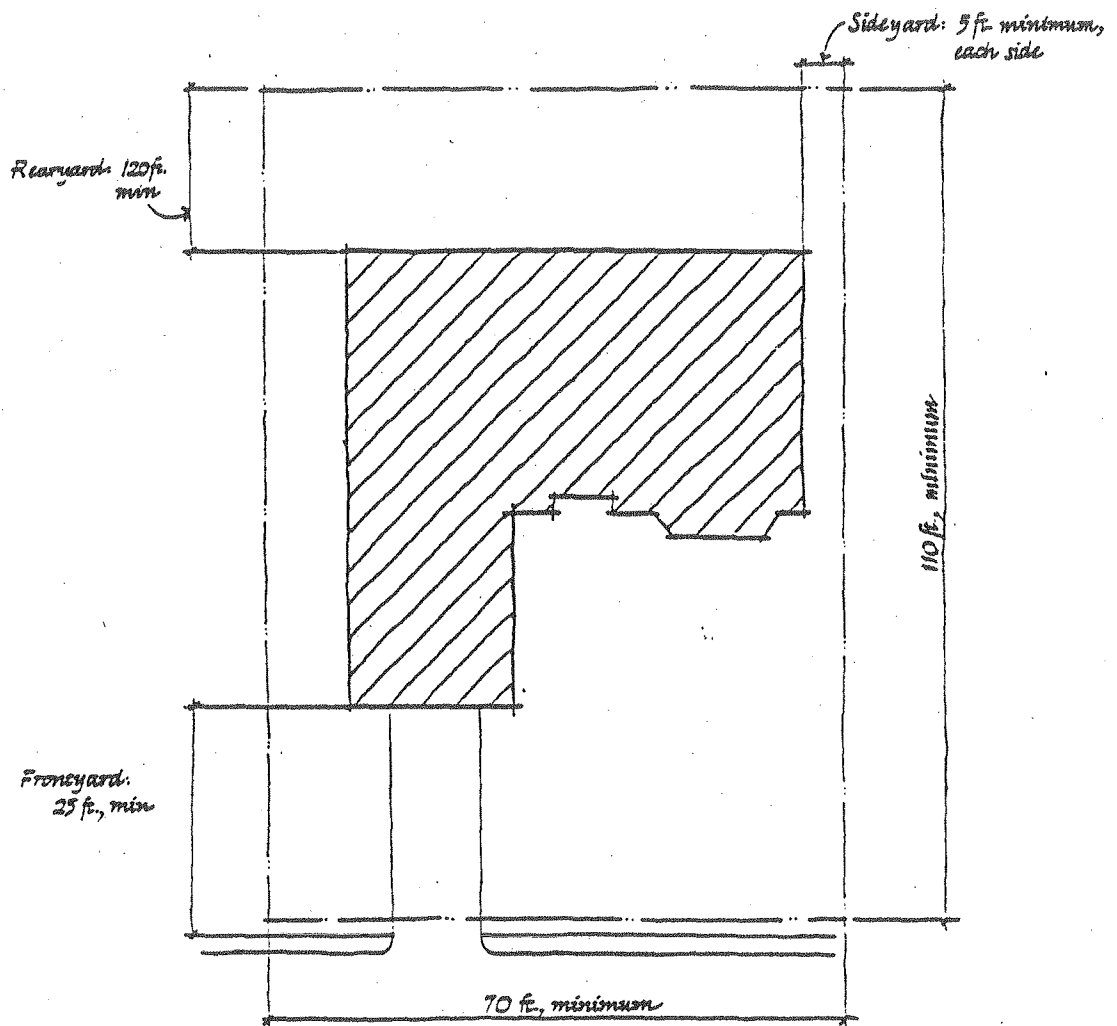
Driveways serve six dwellings or less, max 16 ft. wide.

Each dwelling should have one or more usable outdoor spaces carefully screened or fenced to assure privacy.

Landscaping will emphasize native and drought tolerant plants. Trees and shrubs sensibly clustered around dwellings to create privacy; buildings to appear tucked into landscape.

Open spaces planted with naturalized grasses, scattered clumps of trees.

**"L" LAND USE
GREATER THAN 10,000 S.F. LOTS**



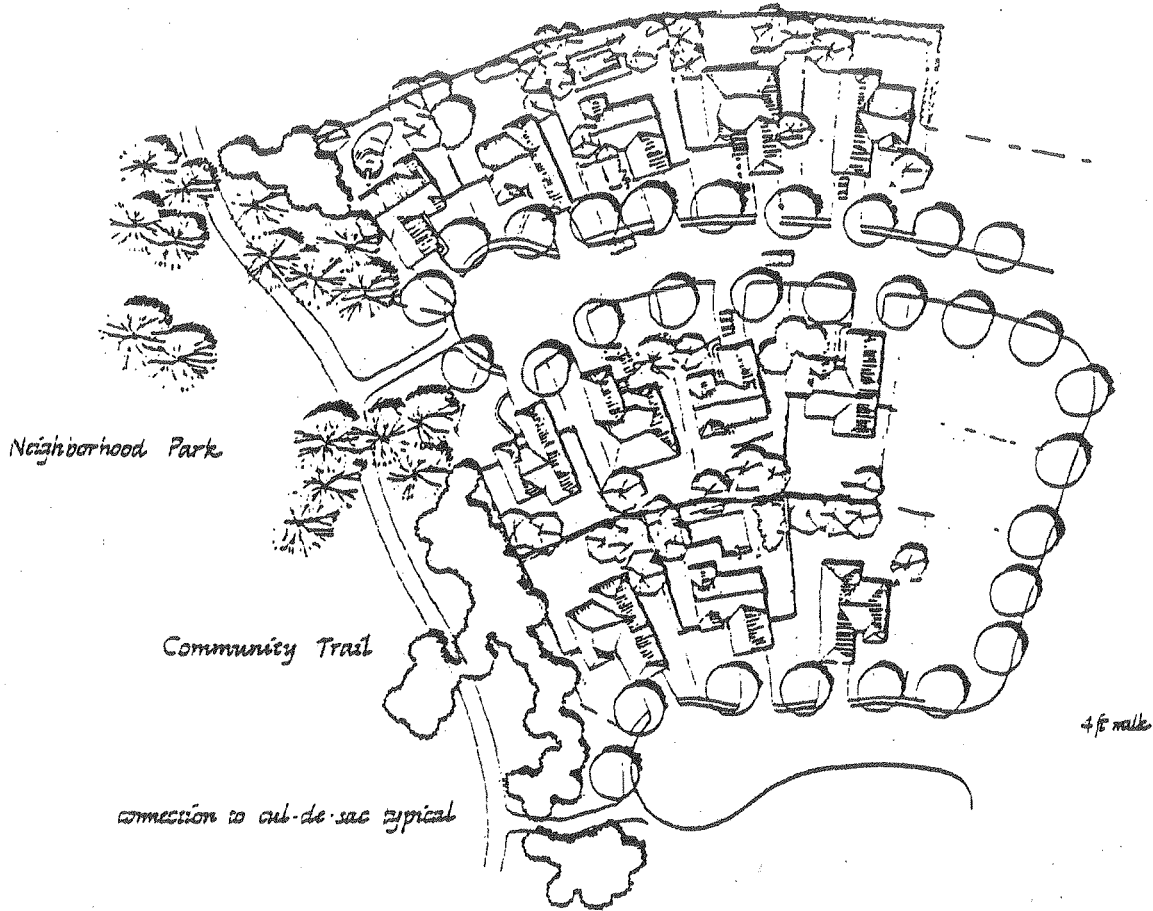
Maximum Coverage: 50% of area within property lines

"L" OR "LM" LAND USE

minimum lot size: 7200 s.f.
typical dimensions: 60 x 120 ft.



Street trees by developer, min. two trees per lot (15 gal size)



Attractive architectural treatment required on building elevations visible from roads, community trails and open space

Fence on property line adjacent to park by developer

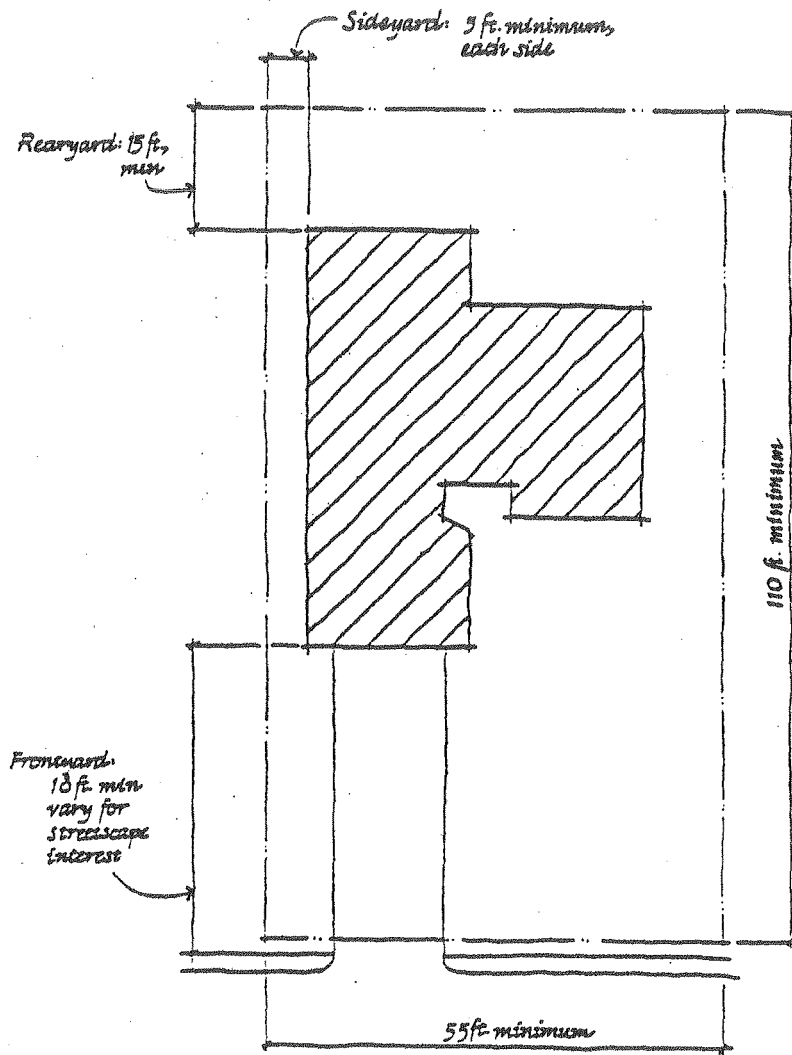
Setbacks:

Frontyard: varies for streetscape increase from back of curb: 18 ft, min.

Sideyard: variable
min. 20 ft. between buildings; min. 5 ft from building to property line

Rearyard: 15 ft minimum.

7200-10,000 S.F. LOT
center plot



Maximum Coverage: 55% of area within property lines

GENERAL STANDARDS AND REGULATIONS

1. Whenever the regulations contained herein conflict with the regulations of the Zoning Ordinance of the City of Rancho Cucamonga, the regulations contained herein shall take precedence. Where an issue is not covered (i.e., variance, temporary use, signs, home occupations, etc.), refer to the Rancho Cucamonga Zoning Ordinance.
2. Within the Planned Community ²⁴Boundary, the continued use of the land for agricultural purposes with uses, structures, and appurtenances accessory thereto shall be permitted, subject to the provisions of the Agricultural Zone Regulations set forth in the City of Rancho Cucamonga Zoning Ordinance.
3. Grading will be permitted within the Planned Community area outside of an area of immediate development upon the securing of a grading permit.

During site development, preparation, and construction, the hours of operation shall be limited to the period between 7:00 a.m. and dusk Monday through Saturday. No activities will be permitted outside of these hours including maintenance work that might be required on any equipment used in grading and/or construction unless a temporary waiver is granted by the Building Official. No such waiver will be granted where such work is to be conducted adjacent to existing and occupied dwelling units except in cases of emergency as determined by the Building Official.

4. Regardless of the provisions of this text, no construction shall be allowed within the boundaries of the Planned Community area except that which complies with all provisions of applicable building codes and the various mechanical codes related thereto.
5. Any land use proposal not specifically covered by this plan and its text shall be subject to the regulations of the City of Rancho Cucamonga General Plan.
6. Model homes and their garages and private recreation facilities may be used as offices for the sale of homes within a recorded tract and subsequent tracts utilizing these same architectural designs. They may be occupied for a maximum of three years, with one year extensions subject to the approval of the City Planner.
7. With respect to all residential developments within this Planned Community, developer will display a copy of the adopted Community Land Use Plan in all sales offices and will provide a copy of the Land Use Plan to all buyers at their request.

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

3. Low-Medium Density Residential ("LM" Land Use Plan designation):

Land designated as Low-Medium Density Residential is intended for residential development that has a range of four to eight dwellings per adjusted gross acre. The following regulations are applicable to these areas:

a. Uses Permitted: detached or attached residential dwellings not exceeding eight dwellings per adjusted gross acre, including, but not limited to:

- (1) Single family attached dwellings.
- (2) Single family attached dwellings, including, but not limited to, duplexes and triplexes.
- (3) Cluster Housing.
- (4) "Zero lot line" homes.
- (5) Community Facilities as specified on page 241.

b. Accessory Uses Permitted: any of the following uses and structures.

- (1) Garages and carports, in compliance with the site development standards provided herein.
- (2) Fences, walls, and trellises.
- (3) Swimming pools with a minimum 5 foot high fence enclosing pool.
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as permitted by the Rancho Cucamonga Zoning Ordinance.

c. Site Development Standards:

- (1) Single Family Center Plot dwellings.
 - (a) Building site area: 5,000 sq. ft. minimum, 5,500 sq. ft. average.
 - (b) Building site width: 50 feet minimum. It is intended that site widths will vary according to lot size.
 - (c) Building site coverage: varies according to lot size, see typical lot plans, pp. 214 - 223.
 - (d) Building setbacks: varies according to lot size, see typical lot plans and table.
 - (e) Building height: 35 feet maximum.

- (2) Cluster housing.
- (a) Building site area: 2 acres minimum.
 - (b) Building site coverage: 50% maximum.
 - (c) Building setbacks: See diagrams for typical building setbacks, pp. 214 - 223.
 - (d) Building separation: See diagram for typical building separation distances, pp. 214 - 223.
 - (e) Building height: 35 feet, maximum.
 - (f) Building site width and depth: As permitted by required setbacks.
 - (g) Private open space: 300 sq. ft. minimum.
- (3) Innovative housing.
- (a) Building site area: 3,500 sq. ft. minimum, 4,000 sq. ft. average.
 - (b) Building site coverage: As permitted by required setbacks and private open space.
 - (c) Building setbacks: See diagrams for typical building setbacks, pp. 214 - 223.
 - (d) Building height: 35 feet maximum.
 - (e) Building site width and depth: As permitted by required setbacks.
 - (f) Private open space: 700 sq. ft. minimum.

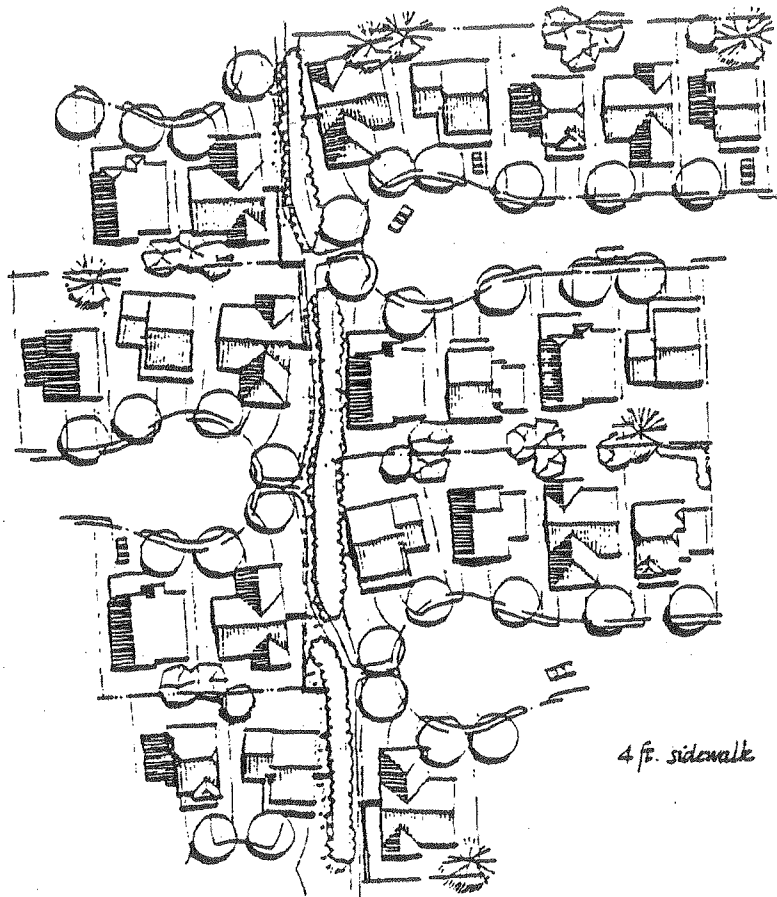
The above site development standards apply to projects which are deemed innovative. Innovation in single family development means providing creative design solutions which address the critical concerns of neighborhood compatibility, density transition, and design quality. Innovative projects are characterized by an attractive streetscape which is not monotonous, nor is the street scene dominated by asphalt/concrete, garages, and cars. Innovative design means finding creative ways to create well-designed space, particularly usable yard space.

"LM" LAND USE

minimum lot size: 5000 s.f. - 6000 s.f.
typical dimensions: 50 x 100 ft.



*street trees by developer, minimum 1.5 trees per lot (15 gal size)
regular spacing typical on local residential roads, may be grouped
on neighborhood streets*



Setbacks:

Garage: see garage setback standards

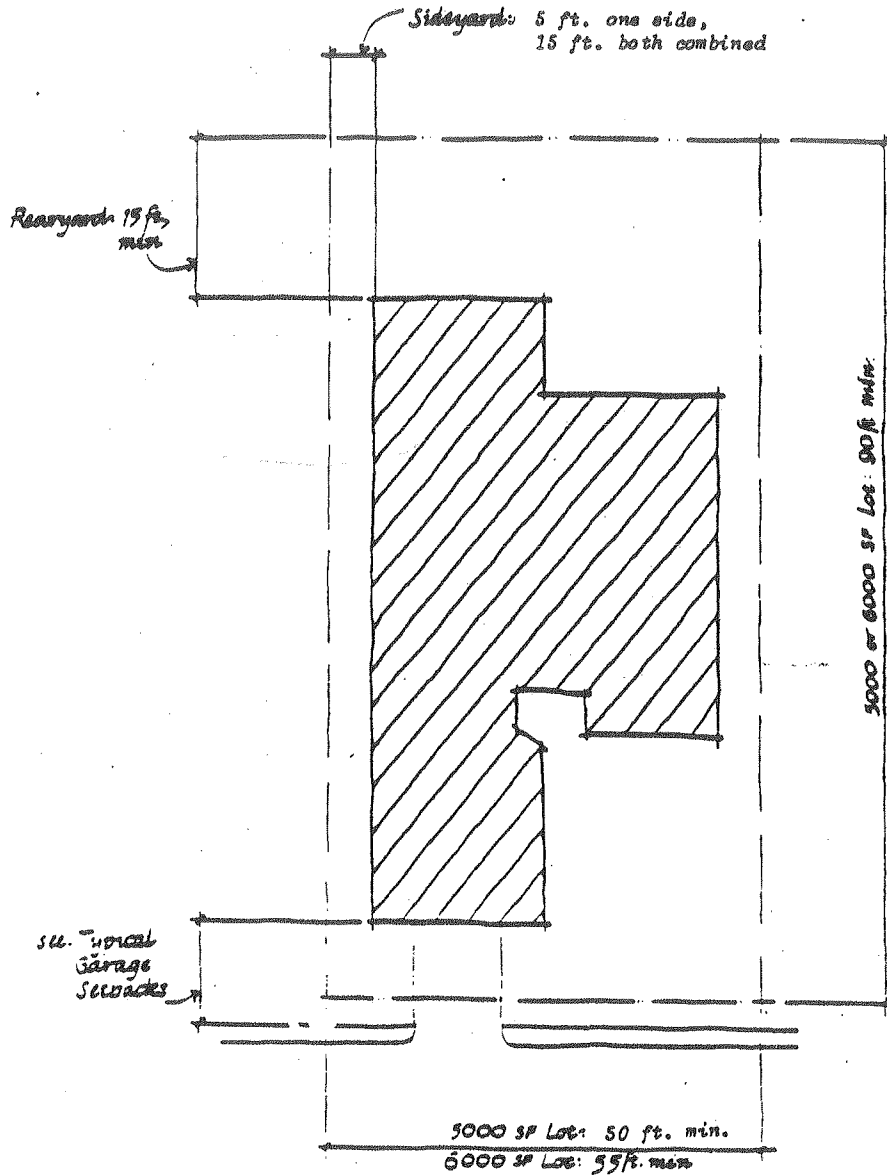
Sidyard: 5/10 ft. min.

Rearyard: 5 ft. minimum

*staggering cul-de-sacs provides maximum exposure of
neighborhood to community trail system.*

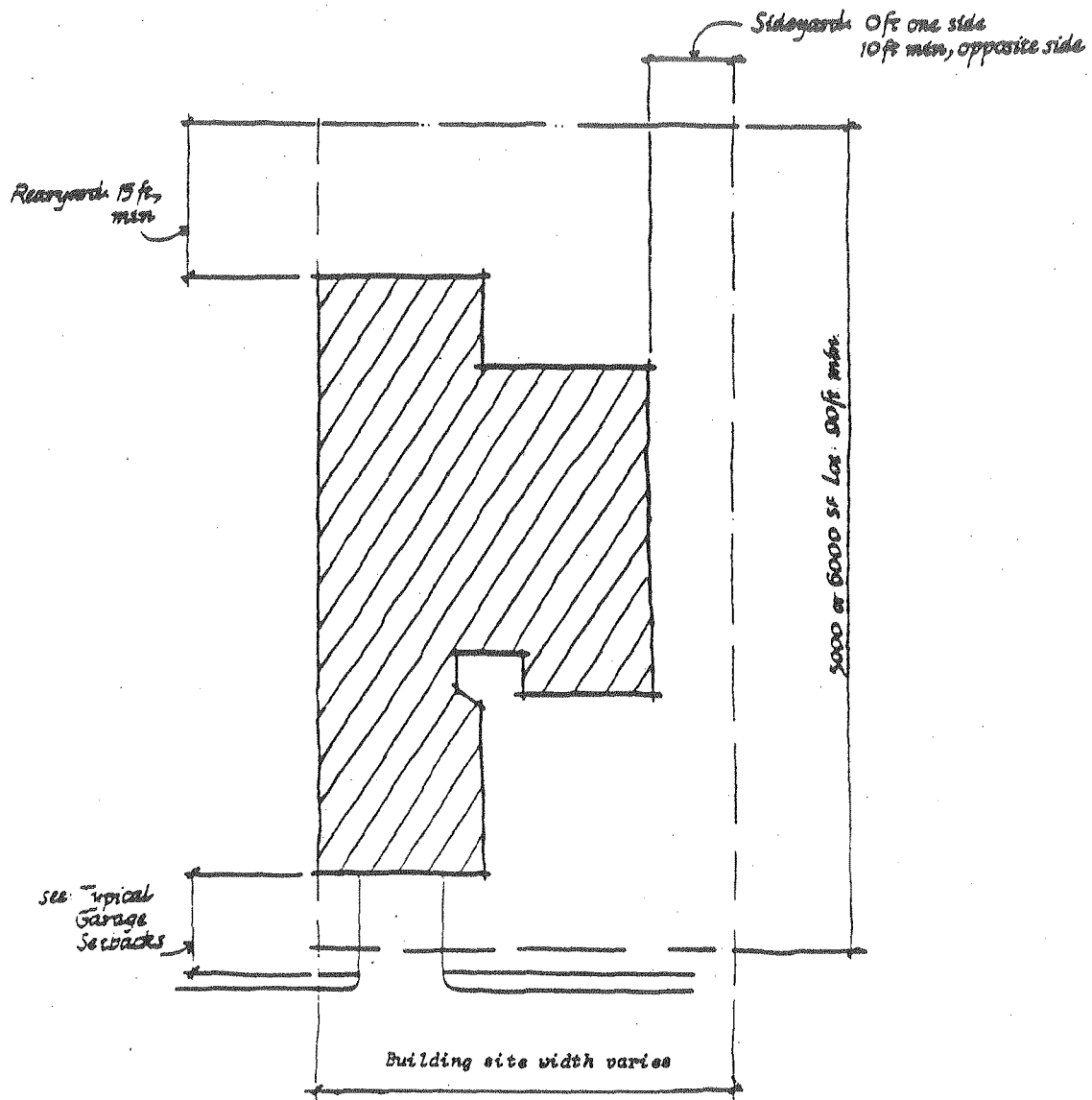
community trail system - see additional site planning criteria

5000-6000 S.F. LOT
center plot



Maximum Coverage: 60% of area within property lines

5000-6000 S.F. LOT
zero-lot-line



Maximum Coverage: 60% of area within property lines

"LM" LAND USE

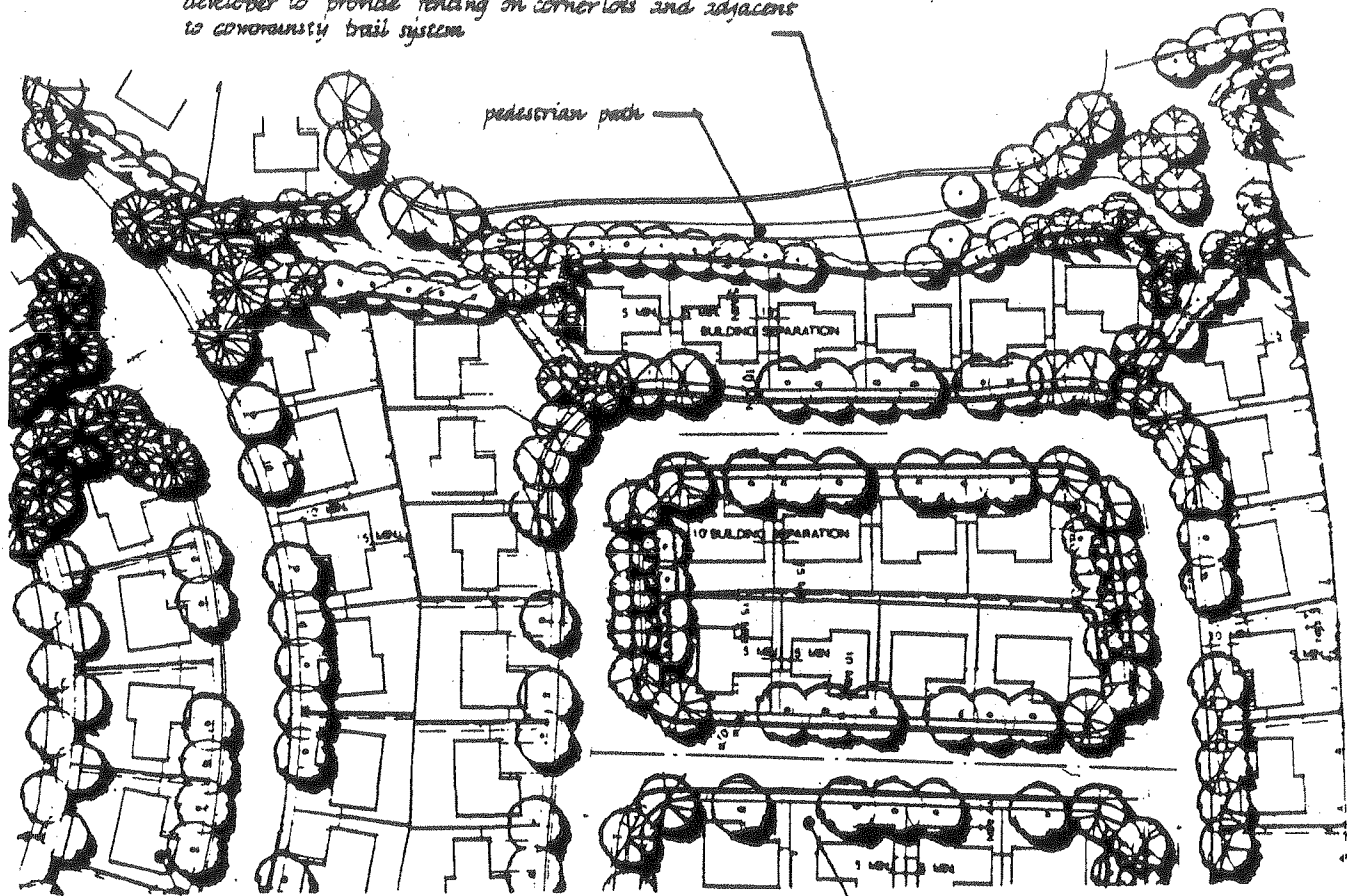
minimum lot sizes 3600 s.f. innovative
typical dimensions: 60 x 70 ft.

Varying setbacks, streetscape landscaping on the developer,
and attractive architectural treatments on elevations visible
from public roads, trails and open spaces will assure an
attractive environment.

Wide shallow concept allows more
living area to street and allows
standard garage setback.



developer to provide fencing on corner lots and adjacent
to community trail system



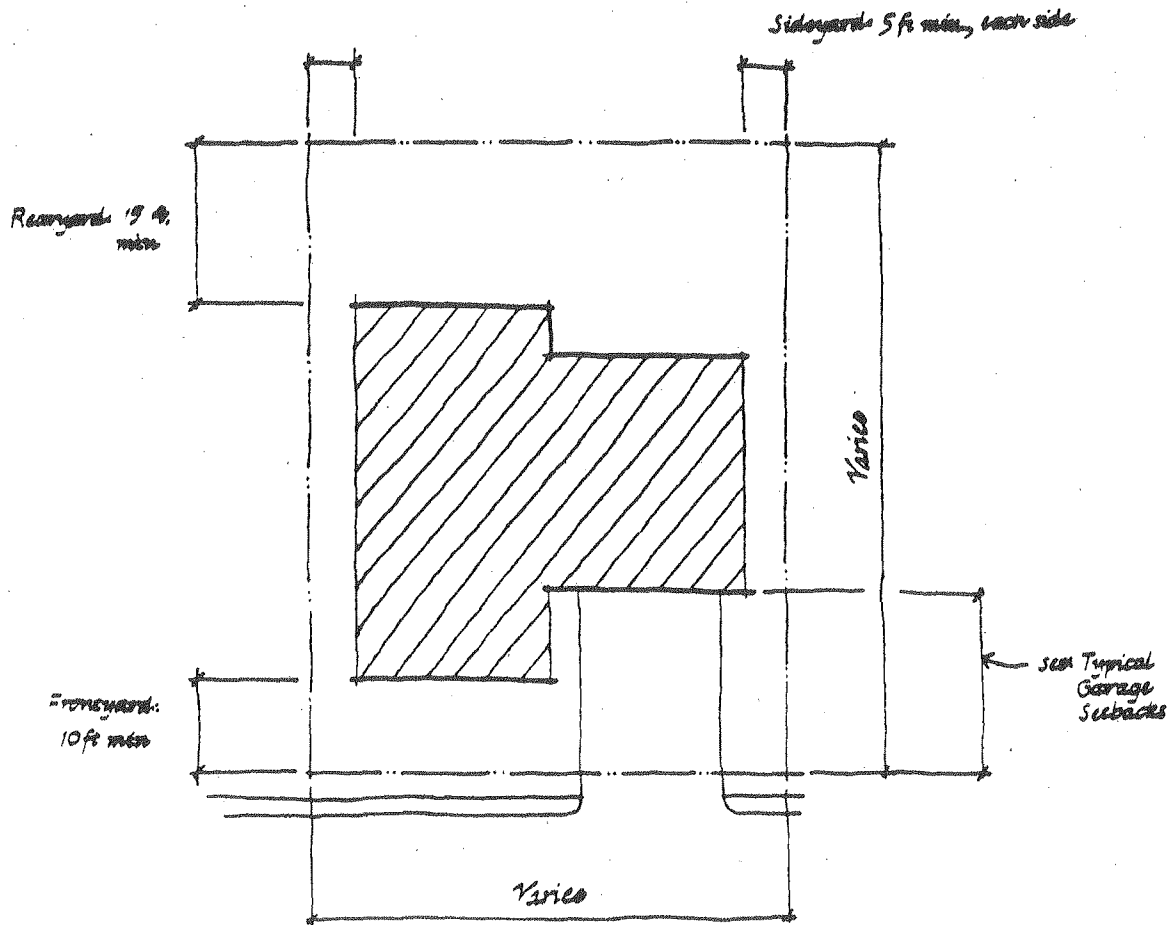
frontyard landscape by developer

Attractive architectural treatments required on elevations
visible from public roads, community trails, and public
open space.

Setbacks:

- Garage: see Typical Garage Setback Standards
- Sideyard: 5 ft., min. each side
- Rearyard: 15 ft., minimum

3500 S.F. LOT
wide-shallow innovative product

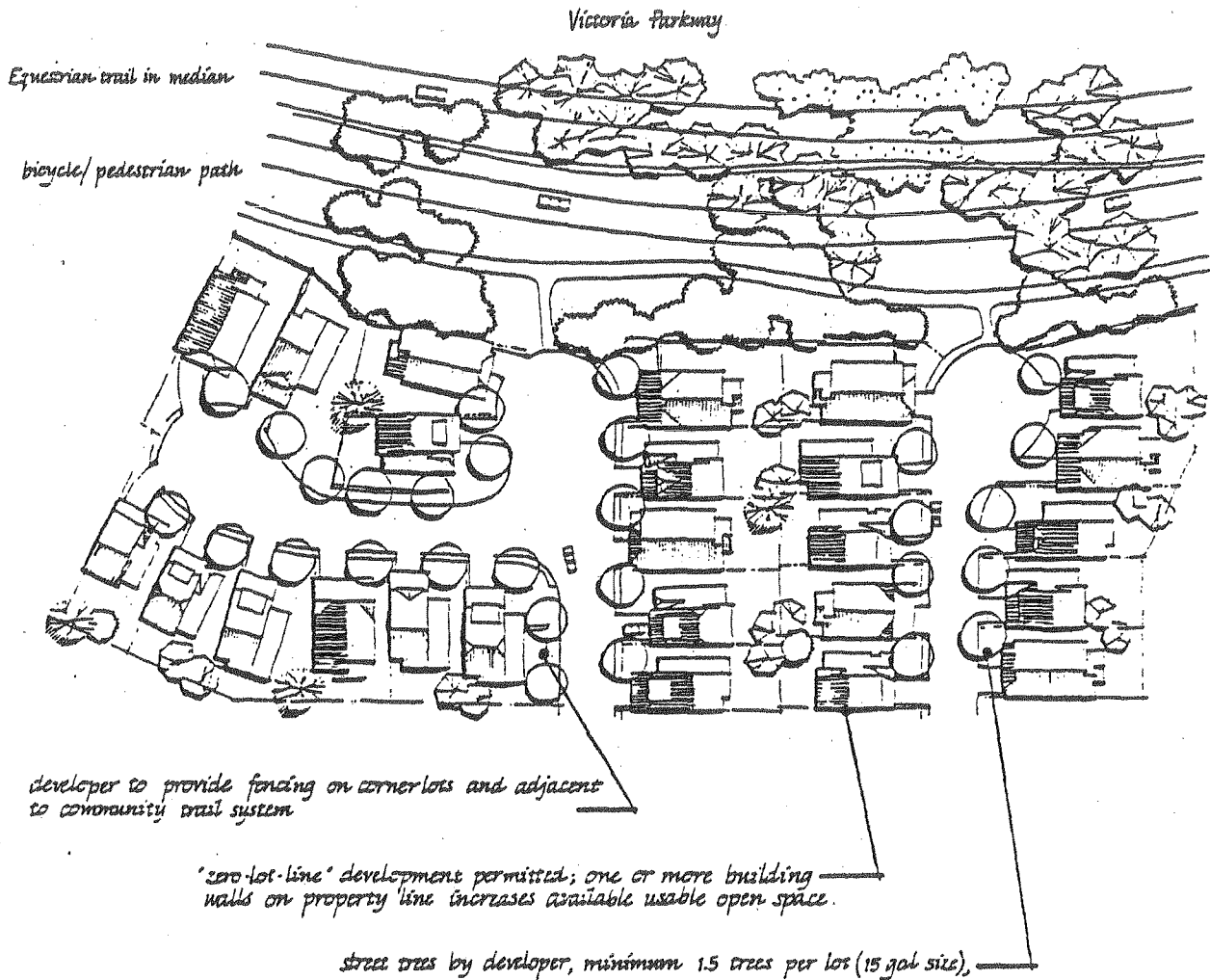


"LM" LAND USE

minimum lot size: 3,500 s.f. zero - lot - line
typical dimensions: 45 x 90 ft.



Attractive architectural treatment required on elevations visible from public roads, community trails, and public open space.

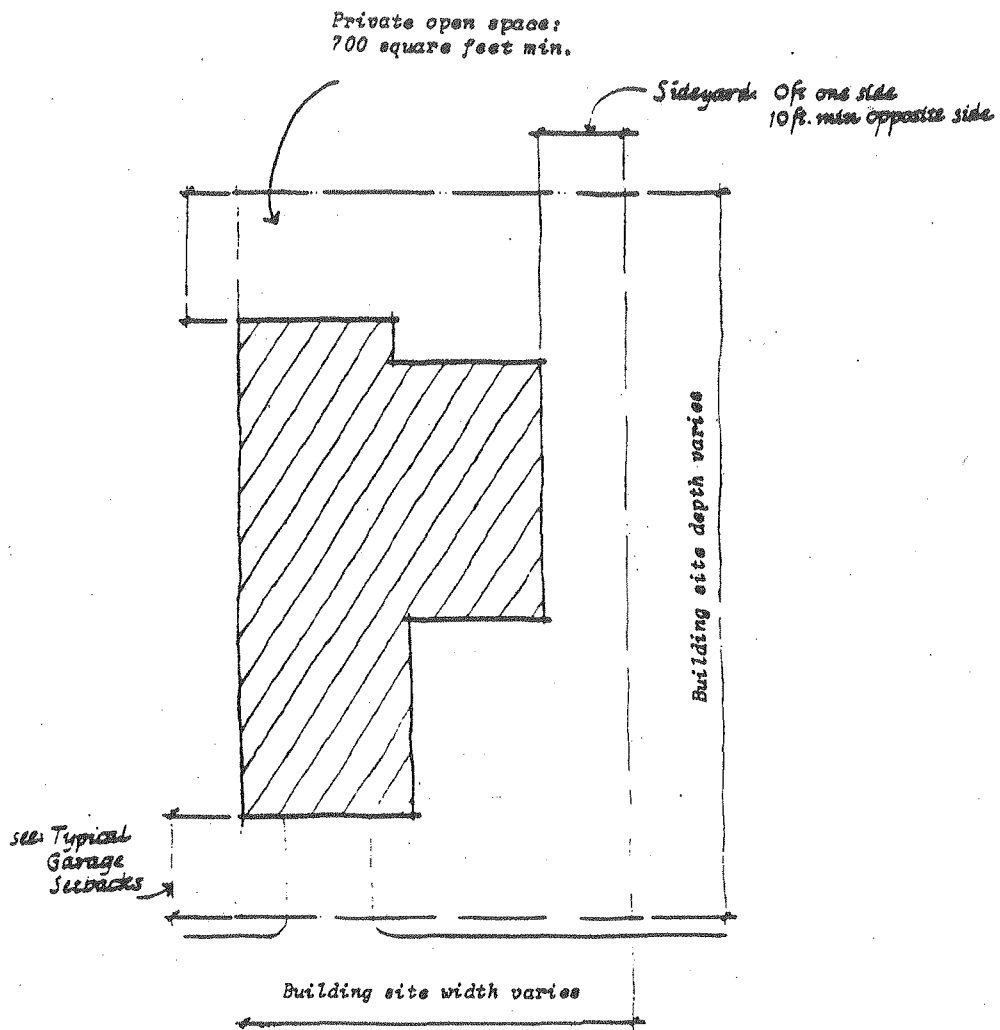


Setbacks:

Garage: see Typical Garage Setback Standards

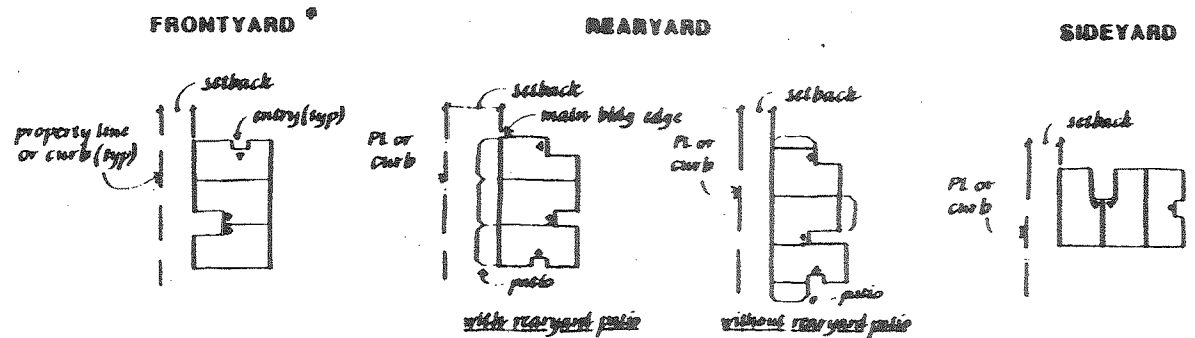
sideyard: 0 ft; min. 10 ft between buildings

3500 S.F. LOT
zero-lot-line



Maximum Coverage: 60% of area within property lines

CENTER PLOT HOUSING SETBACKS:
standards for 'LM' and 'M'
residential land uses



Adjoining Condition:

RESIDENTIAL OR COMMERCIAL

with fence or wall on property line	20 ft. min, 25 ft. avg	15 ft. min	15 ft. min	5 ft. min. one side; 15 ft. both combined
without fence or wall on property line	20 ft. min, 25 ft. avg	15 ft. min	15 ft. min	5 ft. min. one side; 15 ft. both combined

OPEN SPACE

except Victoria Parkway	10 ft. min	patio fence may abut property line	5 ft. min	5 ft. min
-------------------------	------------	---------------------------------------	-----------	-----------

NEIGHBORHOOD STREET

without facing garage access (see Typical Garage Setbacks)	20 ft. min, 25 ft. avg	15 ft. min	15 ft. min	5 ft. min to back of curb or walk
---	------------------------	------------	------------	--------------------------------------

LOCAL RESIDENTIAL STREETS

without facing garage access (see Typical Garage Setbacks)	20 ft. min, 25 ft. avg	20 ft. min	15 ft. min	10 ft. min
---	------------------------	------------	------------	------------

ARTERIAL ROADS

with fence or wall on property line	10 ft. min, 15 ft. typ	25 ft. min	25 ft. min	25 ft. min
without fence or wall on property line	20 ft. min	25 ft. min	25 ft. min	25 ft. min

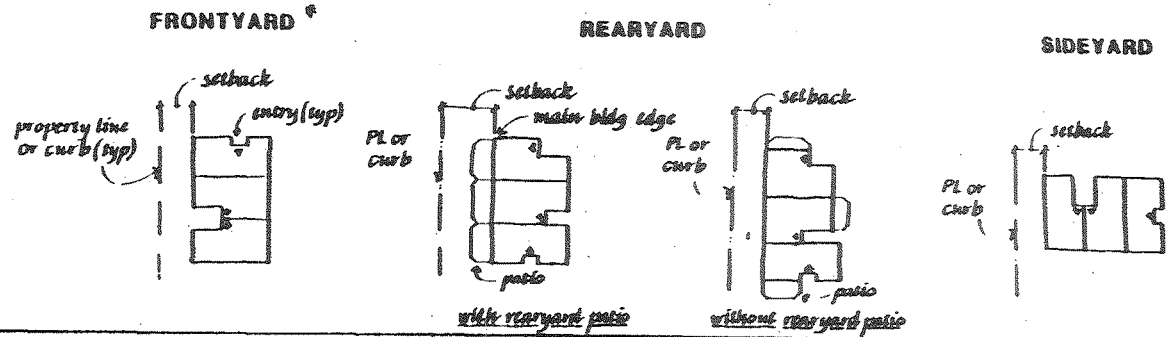
VICTORIA PARKWAY

see Typical Edge Condition Cross-Sections

note: drawings not to scale

* also see Typical Garage Setbacks

CLUSTER AND INNOVATIVE HOUSING SETBACKS:
standards for 'LM', 'M'
residential land uses



Adjoining Condition:

RESIDENTIAL OR COMMERCIAL

with fence or wall on property line	10 ft. min, 15 ft. typ	15 ft. min	10 ft. min	5 ft. min
without fence or wall on property line	10 ft. min, 15 ft. typ	15 ft. min	10 ft. min	5 ft. min

OPEN SPACE

except Victoria Parkway	10 ft. min	patio fence may abut property line	5 ft. min	5 ft. min
-------------------------	------------	------------------------------------	-----------	-----------

NEIGHBORHOOD STREET

without facing garage access (see Typical Garage Setbacks)	10 ft. min ^{**}	15 ft. min	10 ft. min	5 ft. min to back of curb or walk
--	--------------------------	------------	------------	-----------------------------------

LOCAL RESIDENTIAL STREETS

without facing garage access (see Typical Garage Setbacks)	10 ft. min, 15 ft. typ ^{**}	20 ft. min	15 ft. min	10 ft. min
--	--------------------------------------	------------	------------	------------

ARTERIAL ROADS

with fence or wall on property line	10 ft. min, 15 ft. typ	25 ft. min	25 ft. min	25 ft. min
without fence or wall on property line	20 ft. min	25 ft. min	25 ft. min	25 ft. min

VICTORIA PARKWAY

see Typical Edge Condition Cross-Sections

note: drawings not to scale

* also see Typical Garage Setbacks

** second story shall be set back further than 10 ft. subject to Design Review approval. The setback area shall be landscaped and is exclusive of walkways or structures.

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

4. Medium Density Residential ("M" Land Use Plan designation):

Land designated as Medium Density Residential is intended for residential development that ranges from 8 to 14 dwelling units per adjusted gross acre. The following regulations are applicable for these areas:

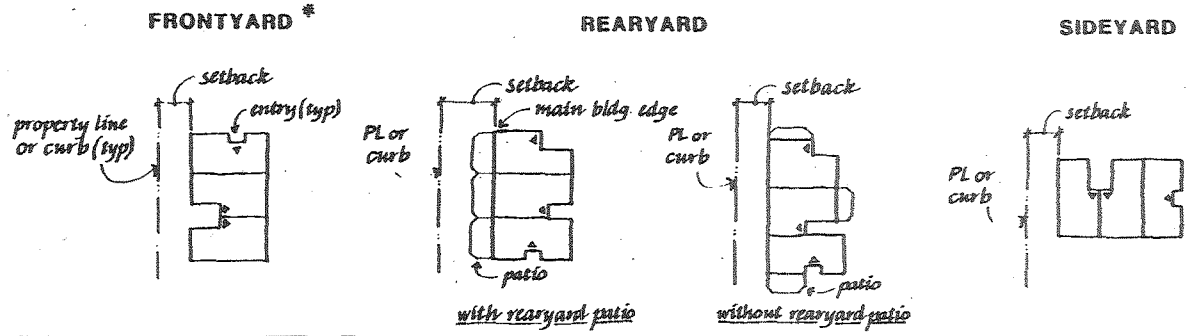
- a. Uses Permitted: detached or attached residential dwellings not exceeding fourteen dwellings per adjusted gross acre, including, but not limited to:
 - (1) Single family dwellings - attached or detached, including, but not limited to townhouses, triplexes, fourplexes, and condominiums.
 - (2) Cluster housing.
 - (3) Community facilities, page 241.
- b. Site Development Standards:
 - (1) Cluster housing.
 - (a) Building site area: 3 acres minimum.
 - (b) Building site coverage: As permitted by required setbacks and private open space.
 - (c) Building setbacks: See building setback diagrams on the following page for typical setbacks.
 - (d) Building separation:
 - (i) building 35 feet or less in height, 10 feet minimum.
 - (ii) buildings over 35 feet in height, 15 feet minimum.
 - (e) Building height: 40 feet maximum.
 - (f) Building site width and depth: As permitted by required setbacks.
 - (g) Private open space: 300 sq. ft. minimum.
 - (2) Innovative single family housing.
 - (a) Building site area: 3,500 sq. ft. minimum, 4,000 sq. ft. average.

- (b) Building site coverage: As permitted by required setbacks and private open space.
- (c) Building setbacks: See building setback diagrams on the following page for typical setbacks.
- (d) Building separation: See building setback diagrams on the following page for typical setbacks.
- (e) Building height: 35 feet maximum.
- (f) Building site width and depth: As permitted by required setbacks.
- (g) Private open space: 300 sq. ft. minimum.

The above site development standards apply to projects which are deemed innovative. Innovation in single family development means providing creative design solutions which address the critical concerns of neighborhood compatibility, density transition, and design quality. Innovative projects are characterized by an attractive streetscape which is not monotonous, nor is the street scene dominated by asphalt/concrete, garages, and cars. Innovative design means finding creative ways to create well-designed space, particularly usable yard space.

CLUSTER AND INNOVATIVE HOUSING SETBACKS:

standards for M residential land use



	FRONTYARD *	REARYARD		SIDEYARD
RESIDENTIAL OR COMMERCIAL				
<i>with fence or wall on property line</i>	10 feet min, 15 feet typ	15 feet min	10 feet min	5 feet min
<i>without fence or wall on property line</i>	10 feet min, 15 feet typ	15 feet min	10 feet min	5 feet min
OPEN SPACE				
<i>except Victoria Parkway</i>	10 feet min	patio fence may abut property line	5 feet min	5 feet min
NEIGHBORHOOD STREET				
<i>without facing garage access (see Typical Garage Setbacks)</i>	10 feet min	15 feet minimum	10 ft min	5 feet min to back of curb or walk
LOCAL RESIDENTIAL STREETS				
<i>without facing garage access (see Typical Garage Setbacks)</i>	10 feet min, 15 feet typ	20 feet min	15 feet min	10 feet min
ARTERIAL ROADS				
<i>with fence or wall on property line</i>	10 feet min, 15 feet typ	25 feet min	25 feet min	25 feet min
<i>without fence or wall on property line</i>	20 feet min	25 feet min	25 feet min	25 feet min

VICTORIA PARKWAY

see Typical Edge Condition Cross Sections

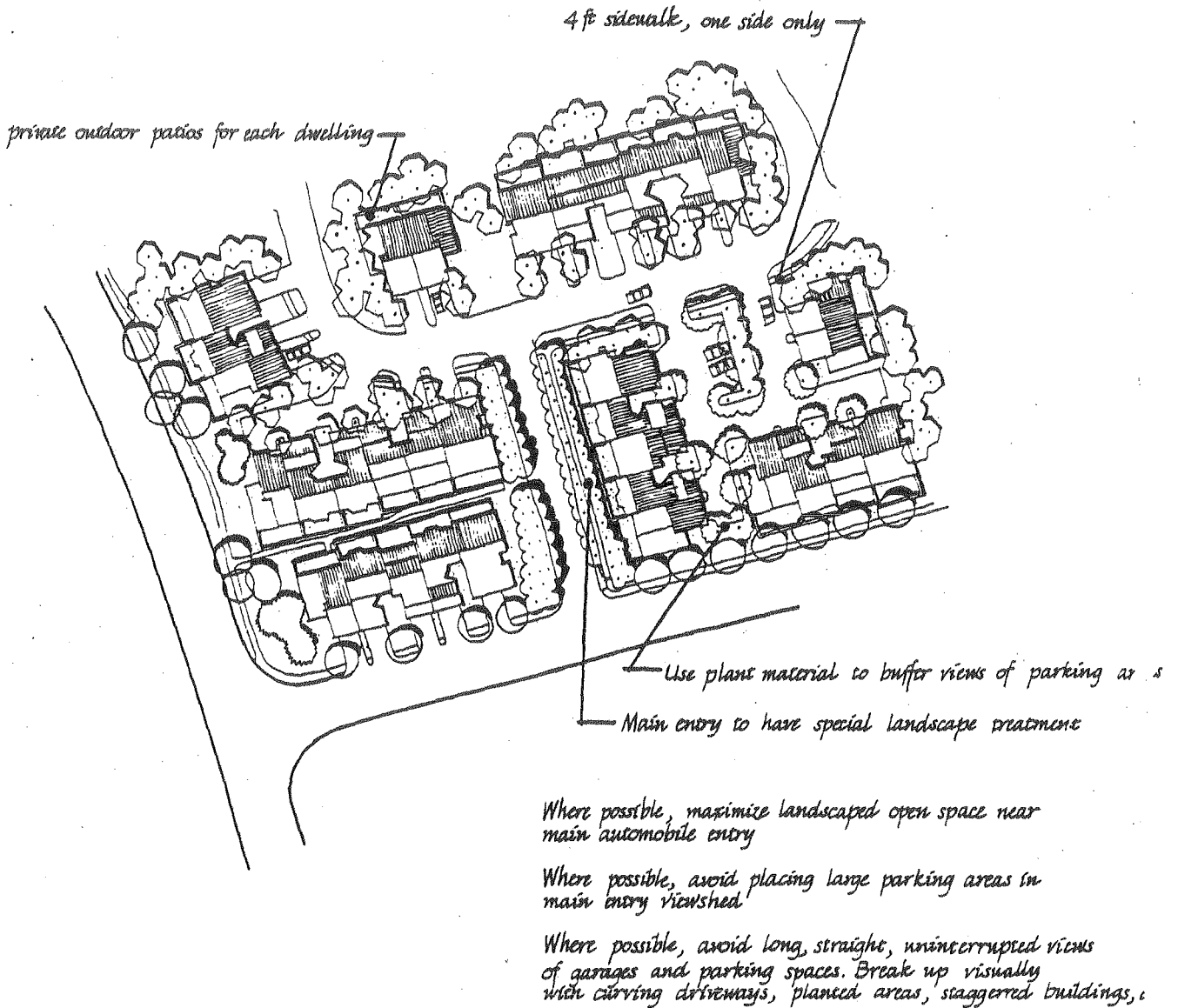
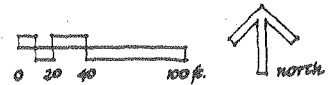
note: drawings not to scale

** also see Typical Garage Setbacks*

"M" LAND USE

front loaded townhouse
10 - 12 dwelling units / acre

A front loaded townhouse has the front entry on the same side as the garage or carport. The private patio is usually at the rear of the dwelling. A small walk connects the dwellings to the street, open space, or community trail network. Landscaping should help minimize the impact of parking areas, driveways, etc.



Garage Setback: see Typical Garage Setback Standards

Street trees on local roads or feeder roads can provide continuity with adjacent land uses. Landscape treatment subject to design review.

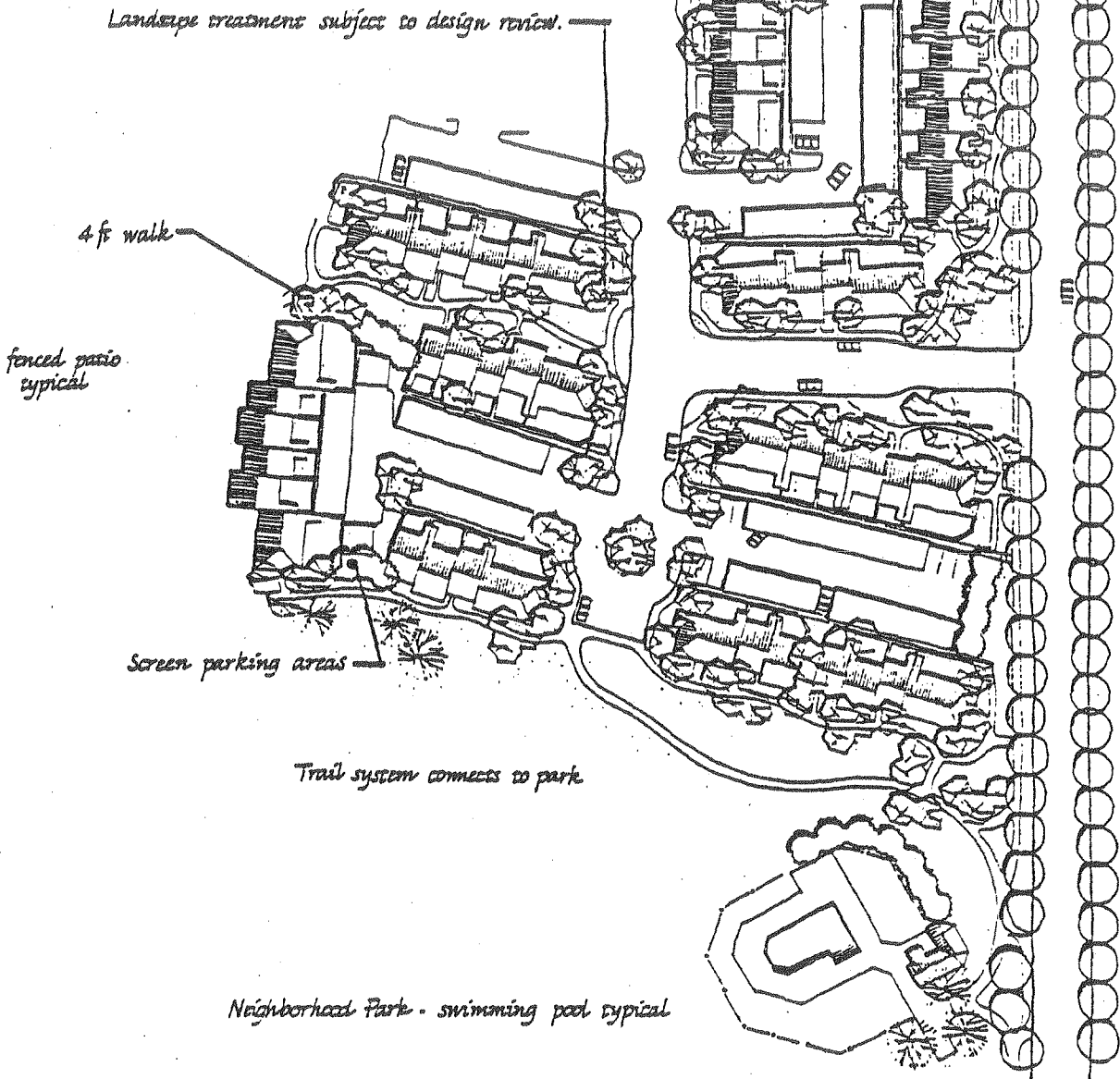
"M" LAND USE

rear loaded townhouse
10 - 12 dwelling units / acre

Rear loaded townhouses have a front door which opens to a common greenbelt, and a backyard adjacent to the dwellings' covered parking. The site plan should make individual units readily accessible. Placing one story dwellings next to the main roads visually softens the streetscape. Buildings typically have 3 to 7 dwellings each.



Special landscape treatment at entry typical

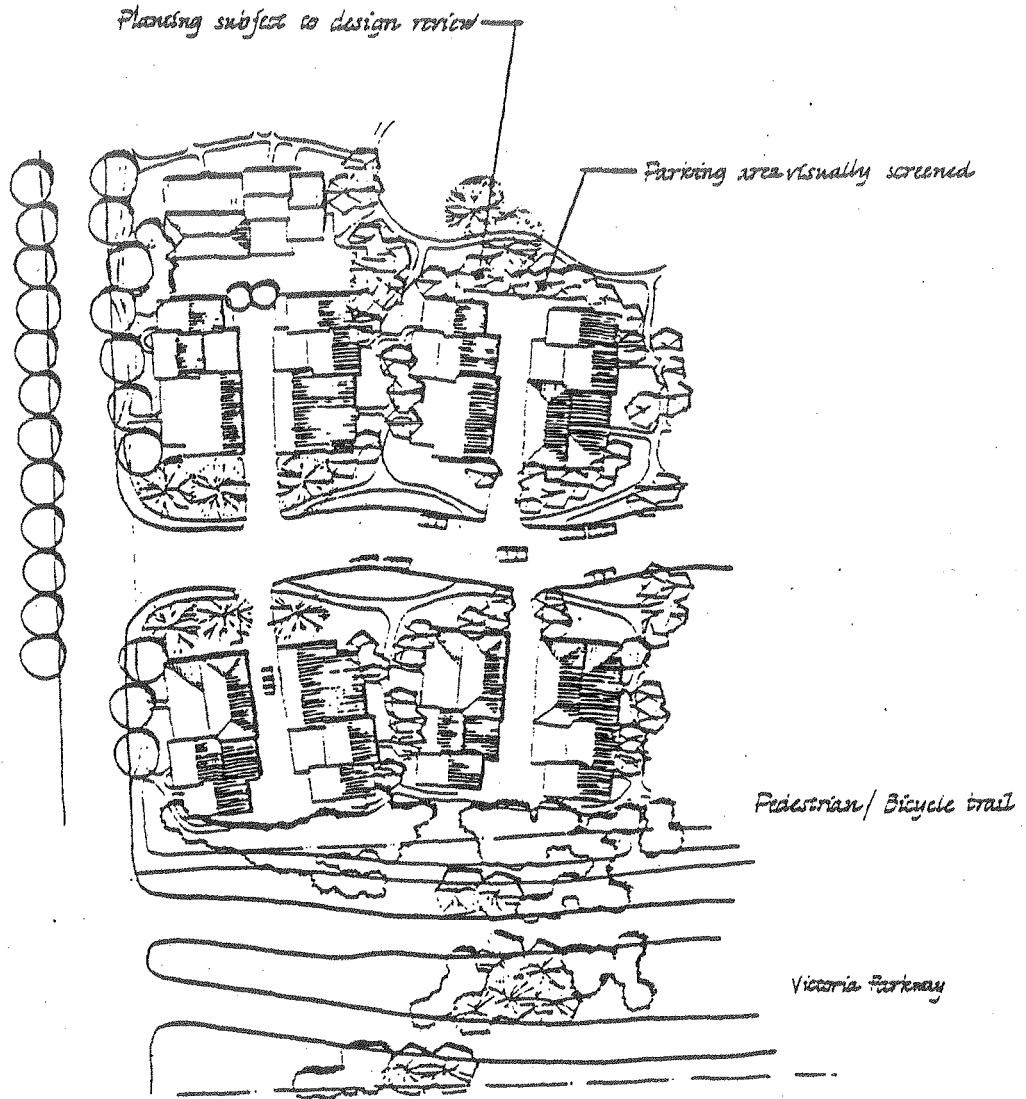
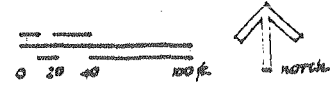


"M" LAND USE

apartmentplex

- 14 dwelling units / acre

The fourplex is a rear-loaded unit with covered parking designed as an integral part of the 1/2 dwelling unit building. Greenbelts provide the formal access to the front door.



Victoria Parkway planting extends to buildings

Walks may connect dwellings to road, trail, open space network

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

5. Medium High Density Residential ("MH" Land Use Plan designation):

Land designated as Medium High Density Residential housing is intended for residential development that ranges from 14 to 24 dwellings per adjusted gross acre. The following regulations are applicable for these areas:

a. Uses Permitted:

- (1) Multiple-family dwellings, including, but not limited to, apartment projects, condominium projects, and cooperative apartment projects.
- (2) Accessory buildings, structures and uses where related and incidental to a permitted use.
- (3) Community Facilities, pg. 241.

b. Site Development Standards:

- (1) Building site area: 3 acres, minimum
- (2) Building Setbacks: see building setback diagrams on the following page.
- (3) Building separation:
 - (a) buildings 35 feet or less in height, 10 feet minimum
 - (b) buildings greater than 35 feet, 15 feet minimum
- (4) Building height: 40 feet maximum

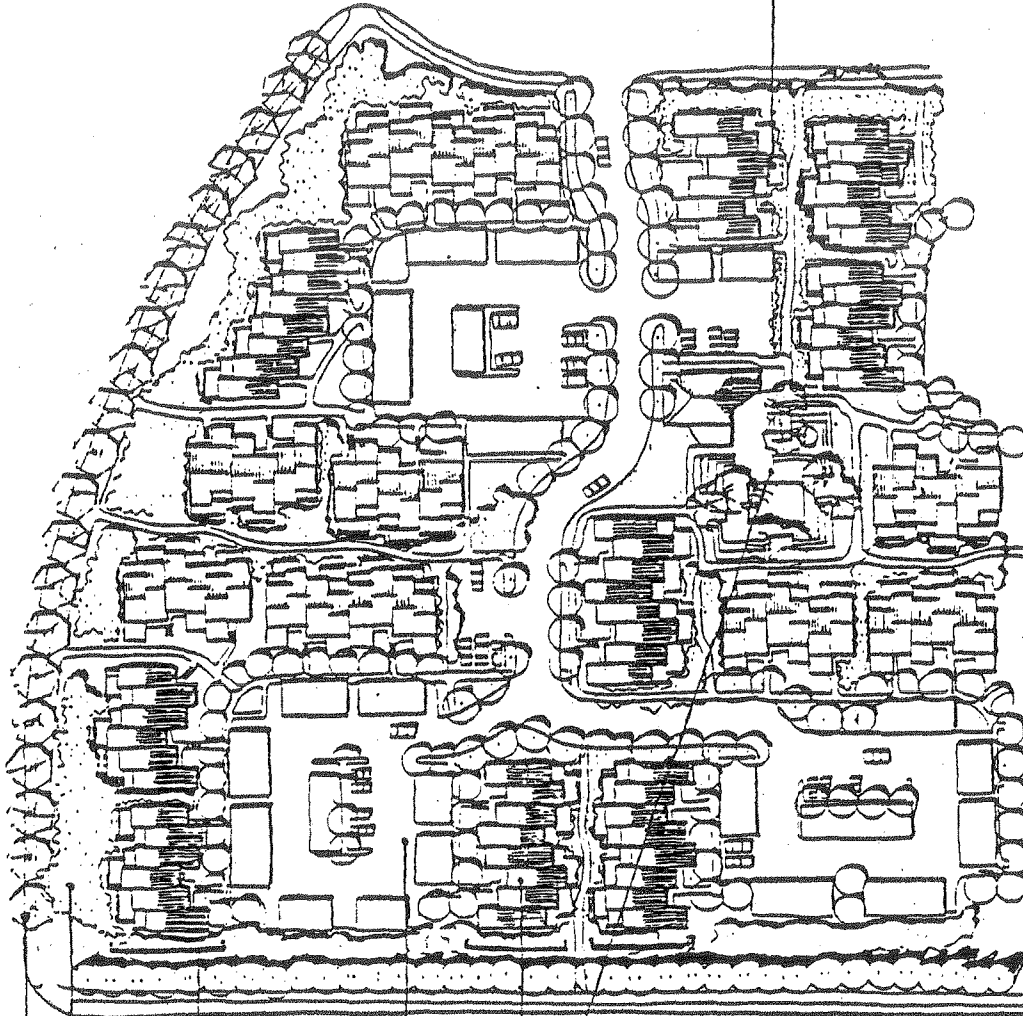
"MH" LAND USE

garden apartment / condominium
18 - 20 dwelling units / acre



Garden apartments and condominiums can be organized so that buildings are clustered around parking courts. Buildings are typically two stories with upstairs and downstairs units. Each unit has a private outdoor patio or deck, and the project may have recreation facilities for residents only. Landscaping should soften building masses and screen parking areas.

Outdoor decks and patios typical



private recreation facilities, including swimming pools and recreation buildings, or rental offices are permitted in this land use category.

where possible, arrange building masses and parking areas to minimize adverse noise impact.

parking courts should be landscaped to minimize impact of paved areas, and to screen parking areas from roads, trails & open space.

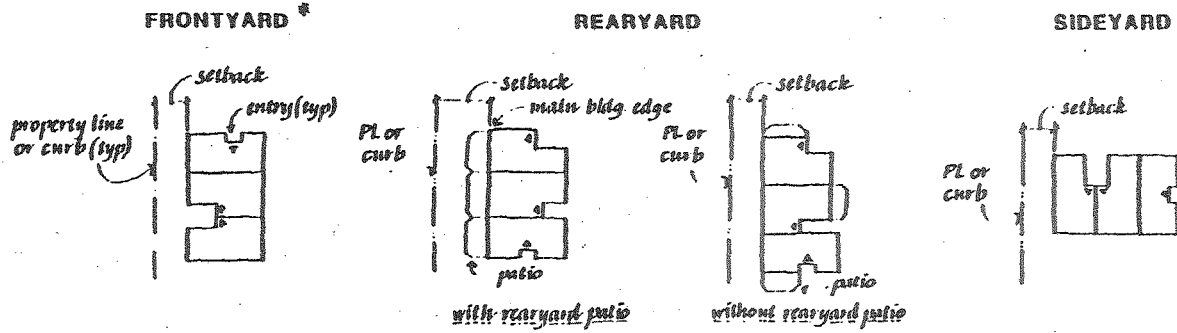
where sound attenuation walls are required, they may not need to be continuous, but may work satisfactorily when broken or staggered; however, must conform to the City noise ordinance

common areas to be maintained by owner or homeowners association

landscape treatment subject to design review.

CLUSTER HOUSING SETBACKS:

standards for 'LA', 'M', and 'MH'
residential land uses



RESIDENTIAL OR COMMERCIAL

with fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

without fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

OPEN SPACE

except Victoria Parkway

10 feet min

patio fence may
abut property line

5 feet min

5 feet min

NEIGHBORHOOD STREET

without facing garage access
(see Typical Garage Setbacks)

10 feet min

15 feet minimum

10 ft min

5 feet min to
back of curb or walk

LOCAL RESIDENTIAL STREETS

without facing garage access
(see Typical Garage Setbacks)

10 feet min

20 feet min

15 feet min

10 feet min

ARTERIAL ROADS

with fence or wall on property line

10 feet min, 15 feet typ

25 feet min

25 feet min

25 feet min

without fence or wall on property line

20 feet min

25 feet min

25 feet min

25 feet min

VICTORIA PARKWAY

see Typical Edge Condition Cross-Sections

note: drawings not to scale

* also see Typical Garage Setbacks

25 to 15

RESIDENTIAL DEVELOPMENT STANDARDS (continued)

6. High Density Residential ("H" Land Use Plan designation):

Land designated as High Density Residential is intended for residential development that ranges from 24 to 30 dwellings per adjusted gross acre. The following regulations are applicable for these areas:

a. Uses Permitted:

- (1) Multiple family dwellings, including, but not limited to, apartment projects, condominium projects and cooperative apartment projects.
- (2) Accessory building structures and uses where related and incidental to a permitted use.
- (3) Community facilities, page 240.

b. Site Development Standards:

- (1) Building site area: 3 acres, minimum.
- (2) Building site coverage: 60%
- (3) Building setbacks: see setback diagrams, on the following page.
- (4) Building separation:
 - (a) Buildings 35 feet in height or less:
10 feet minimum.
 - (b) Buildings greater than 35 feet:
15 feet minimum.
- (5) Building height: 50 feet, maximum.

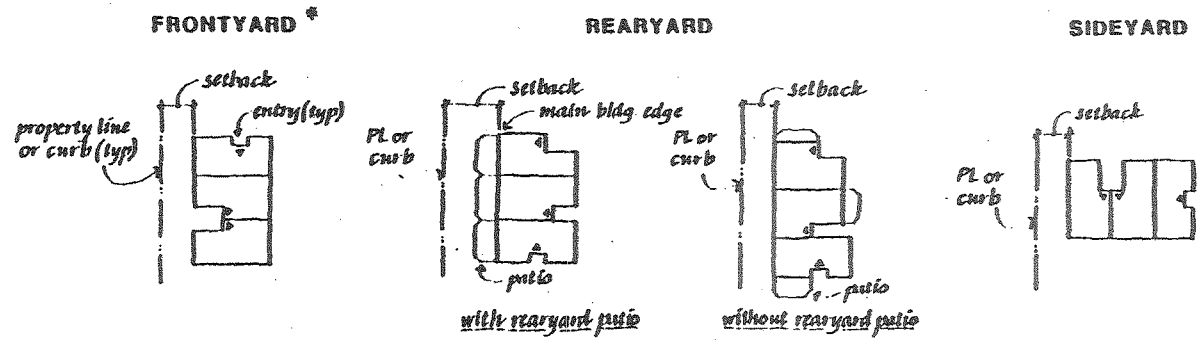
7. Mixed Use (MU Land Use Plan Designation):

Development within any Mixed Use area must be in conformance with an adopted Master Plan that established development standards specifically for the defined Mixed Use area.

- a. The village of Victoria Arbors shall be in conformance with the adopted provisions of the Victoria Arbors Master Plan.

CLUSTER HOUSING SETBACKS:

standards for LM, M, and MH residential land uses



RESIDENTIAL OR COMMERCIAL

with fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

without fence or wall on property line

10 feet min, 15 feet typ

15 feet min

10 feet min

5 feet min

OPEN SPACE

except Victoria Parkway

10 feet min

patio fence may abut property line

5 feet min

5 feet min

NEIGHBORHOOD STREET

without facing garage access (see Typical Garage Setbacks)

10 feet min

15 feet minimum

10 feet min

5 feet min to back of curb or walk

LOCAL RESIDENTIAL STREETS

without facing garage access (see Typical Garage Setbacks)

10 feet min, 15 feet typ

20 feet min

15 feet min

10 feet min

ARTERIAL ROADS

with fence or wall on property line

10 feet min, 15 feet typ

25 feet min

25 feet min

25 feet min

without fence or wall on property line

20 feet min

25 feet min

25 feet min

25 feet min

VICTORIA PARKWAY

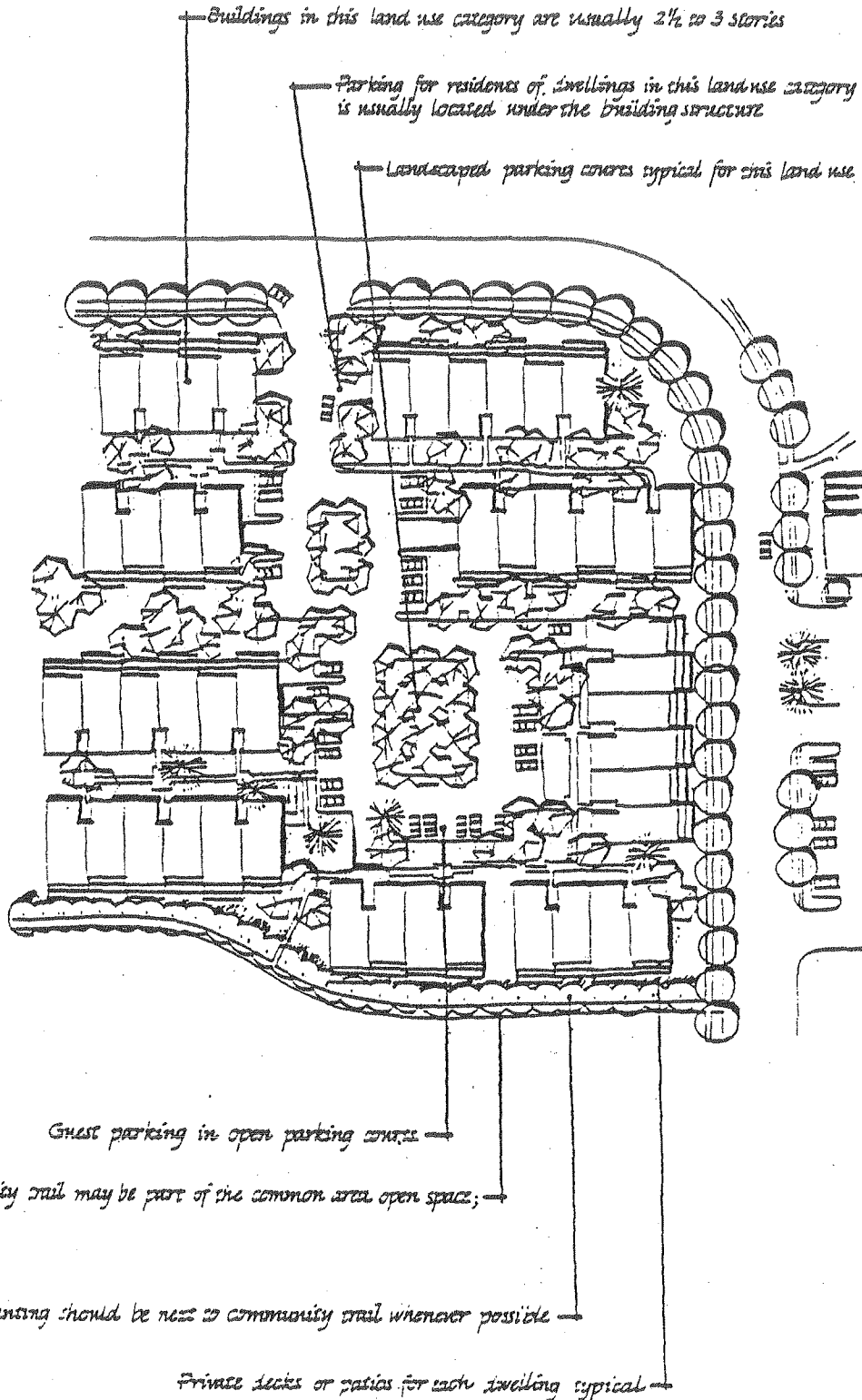
see Typical Edge Condition Cross-Sections

note: drawings not to scale

* also see Typical Garage Setbacks

H¹ LAND USE

High-rise apartments / condominiums
8 - 30 dwelling units / acre



COMMERCIAL STANDARDS

General provisions for land uses designated as Regional Center, Regional-related Commercial and Village Commercial

a Site Development Standards

- (1) Building site coverage: No maximum subject to Development Approval Process
- (2) Building site width: 150-foot minimum
- (3) Building height: 65 feet typical maximum, buildings over 65 feet subject to a Conditional Use Permit

b Uses not permitted within the Planned Community

- Massage Parlors, except as conditionally permitted within the Victoria Arbors Master Plan
- "Adult" Entertainment Establishments

Shopping Centers. To ensure that the goals and policies of the General Plan and Community Plan are implemented, a Conditional Use Permit shall be required for shopping centers. In such a review, the following criteria shall be considered:

- (1) The transition from more sensitive land uses and buffering methods to mitigate commercial activities such as loading, lighting, and trash collection;
- (2) The Center has been planned as a group of organized uses and structures,
- (3) The Center is designed with one theme, with buildings and landscaping consistent in design (similar architectural style, similar exterior building materials, and a coordinated landscaping theme);
- (4) The Center makes provisions for consistent maintenance, reciprocal access, and reciprocal parking;
- (5) Vehicle and pedestrian access is coordinated and logically linked to provide a comprehensive circulation system, and
- (6) The development or approval of any portion of a center shall require the development of a conceptual development plan, which shall consider such things as, but not limited to, circulation, uniform architectural design, drainage/grading, buffers, phased improvements, and landscaping

Regional Center

a. The following general categories of uses shall be permitted

- (1) Retail businesses, including, but not limited to
 - ~~Discount stores~~
 - Department stores
 - Drug stores

COMMERCIAL STANDARDS (continued)

2. Regional Center

a. The following general categories of uses shall be permitted:

- (1) Retail businesses, including, but not limited to:
 - Department stores
 - Drug stores
 - Jewelry stores
 - Book stores
 - Record stores
 - Hi-fidelity equipment sales
 - Clothing stores
 - Musical instrument sales
 - Shoe stores
 - Candy stores
 - Photography equipment sales
- (2) Service businesses, including, but not limited to:
 - Watch and jewelry repair
 - Beauty parlors
 - Travel agencies
 - Locksmiths
 - Banks and financial institutions
 - Movie theaters
- (3) Administrative and professional offices.
- (4) Commercial recreation.
- (5) Restaurants.
- (6) Nurseries and garden supply stores, provided that fertilizer of any type shall be sold and stored in packaged form only.
- (7) Public Utility offices.
- (8) Wholesale business offices with samples on the premises, but not to include general storage.
- (9) Accessory structures and uses necessary or customarily incidental to the above uses as permitted by the Rancho Cucamonga Zoning Ordinance.
- (10) Permitted Community Facilities listed on page 240.
- (11) Permitted Uses listed under General Commercial in Development Code Section 17.10.030.

COMMERCIAL STANDARDS (continued)

b. Uses permitted subject to specific approval of Conditional Use Permit:

- (1) Animal care facilities, not including kennels.
- (2) Parking lots and parking buildings.
- (3) Automobile service stations and car washes.
- (4) Automobile sales and service.
- (5) Electric distribution switch stations.
- (6) Communication equipment buildings.
- (7) Public utility booster stations.
- (8) Commercial recreation.
- (9) Accessory structures and uses necessary or customarily incidental to the above uses as specifically provided for by the Use Permit and the Zoning Ordinance of the City of Rancho Cucamonga.
- (10) Conditionally permitted Community Facilities listed on page 240.
- (11) Conditional Permitted Uses listed under General Commercial in Development Code 17.10.030. *SEE PAGE 235 FOR PROHIBITED USES*
- (12) Shopping Centers subject to provisions in Section 1 on page 235.

3. Regional Related Land Uses

a. The following general categories of uses shall be permitted:

- (1) Retail business, including, but not limited to:
 - Department stores
 - Furniture stores
 - Automobile sales
 - Pet stores
 - Motorcycle sales and service
 - Home improvement center
 - Hardware stores
 - Grocery stores
 - Clothing stores
 - Meat markets or delicatessens

COMMERCIAL STANDARDS (continued)

(2) Service business, including, but not limited to:

- Banks, financial institutions
- Automobile repair
- Blueprinting and photostating
- Cleaning and pressing business
- Laundries
- Locksmiths
- Mortuaries
- Mechanical auto wash
- Plumbing supply
- Movie theatres
- Photographic supplies or studios
- Equipment rental

(3) Administrative and professional offices.

(4) Restaurants.

(5) Commercial.

(6) Nurseries and garden supply stores, provided that fertilizer of any type be stored and sold in packaged form only.

(7) Public Utility offices.

(8) Hotels and motels.

(9) Self-service laundry and self-service dry cleaning facilities.

(10) Accessory structures and uses necessary or customarily incidental to the above uses as permitted by the Rancho Cucamonga Zoning Ordinance.

(11) Permitted Community Facilities listed on page 240.

(12) Permitted Uses listed under General Commercial in Development Code Section 17.10.030. *SEE PAGE 236 FOR PROHIBITED USES*

b. Uses permitted subject to a Conditional Use Permit:

(1) Automobile service facilities.

(2) Animal care facilities.

(3) Public Utility booster stations.

COMMERCIAL STANDARDS (continued)

- (4) Parking lots and parking buildings.
- (5) Public Utility exchange and substations.
- (6) Public buildings.
- (7) Accessory structures and uses necessary or customarily incidental to the above uses as specifically provided for by the use permit.
- (8) Conditionally permitted Community Facilities listed on page 240.
- (9) Conditional Permitted Uses listed under General Commercial in Development Code 17.10.030. *SEE PAGE 235 FOR PROHIBITED USES*
- (10) Shopping Centers subject to provisions in Section 1 on page 235.

4. VILLAGE COMMERCIAL

a. The following general categories of uses shall be permitted:

- (1) Retail businesses, including, but not limited to:
 - Grocery stores
 - Meat markets, delicatessens
 - Produce markets
 - Drug stores
 - Dry good stores
 - Hardware sales
 - Pet stores
 - Clothing stores
 - Florist shops
- (2) Service businesses, including, but not limited to:
 - Banks, financial institutions
 - barber shops, beauty parlors
 - Locksmiths
 - Laundry and dry cleaning establishments
 - Self-service laundry and dry cleaning
- (3) Administrative and professional offices.
- (4) Governmental offices.

COMMERCIAL STANDARDS (continued)

- (5) Restaurants (other than fast food), including incidental serving of beer and wine but without a cocktail lounge, bar, entertainment, or dancing.
 - (6) Accessory structures and uses necessary or customarily incidental to the above as provided for in the Rancho Cucamonga Zoning Ordinance.
 - (7) Permitted Uses listed under Neighborhood Commercial in Development Code Section 17.10.030.
- b. Uses permitted subject to specific approval of a Conditional Use Permit:
- (1) Automotive service stations.
 - (2) Convenience markets.
 - (3) Fast foot restaurants.
 - (4) Wine and liquor stores.
 - (5) Restaurants with entertainment and/or serving of alcoholic beverages.
 - (6) Shopping centers subject to provisions in Section 1 on page 235.
 - (7) Conditionally permitted Community Facilities listed on page 240.
 - (8) Conditional Permitted Uses listed under Neighborhood Commercial in Development Code Section 17.10.030.

L. COMMUNITY FACILITIES

The Community Facilities section of the Victoria Community Plan is established to provide for community support uses and those additional uses which are found to be compatible with the basic permitted uses by the Site Plan Review Process procedure.

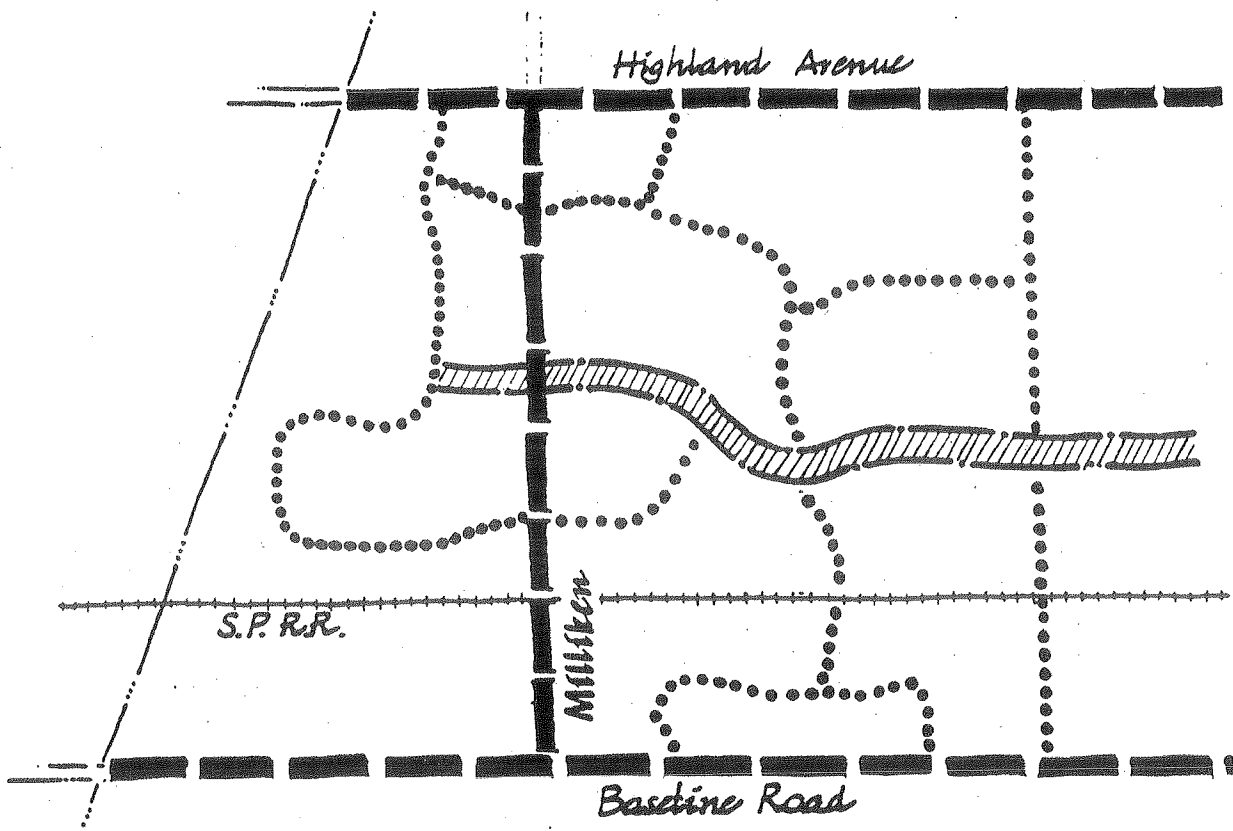
1. Uses Permitted – The following uses shall be permitted within Residential and Commercial Land Use area in Victoria:
 - a. Small Family Day Care in the home, providing care for six (6) or less persons.
 - b. Public Park and Playground.
 - c. General Open Space uses.
 - d. Accessory structures and uses necessary or customarily incidental to the above as provided for in the Rancho Cucamonga Development Code.

2. Conditionally permitted Uses – The following uses shall be permitted within Residential or Commercial Land Use areas in Victoria subject to the Conditional Use Permit process:

- a. Church.
- b. Club, lodge, fraternity and sorority.
- c. Convalescent center.
- d. Public facility.
- e. Large Family Day Care in the home, providing care for 7-12 persons.
- f. Child Care Centers.
- g. Fire and Police station.
- h. Outdoor recreation.
- i. Schools, private and parochial.
- j. Utility or service facility.
- k. Historic winery consisting of manufacturing, pressing, blending and bottling of wine, including accessory uses such as, but not limited to vineyards, retail sales, wine tasting, banquet facility, restaurants, and other uses as found by the Planning Commission to be consistent with the spirit or intent of a Historic Wine Site.
- l. R.V. and Mini-Storage

M. ROAD STANDARDS

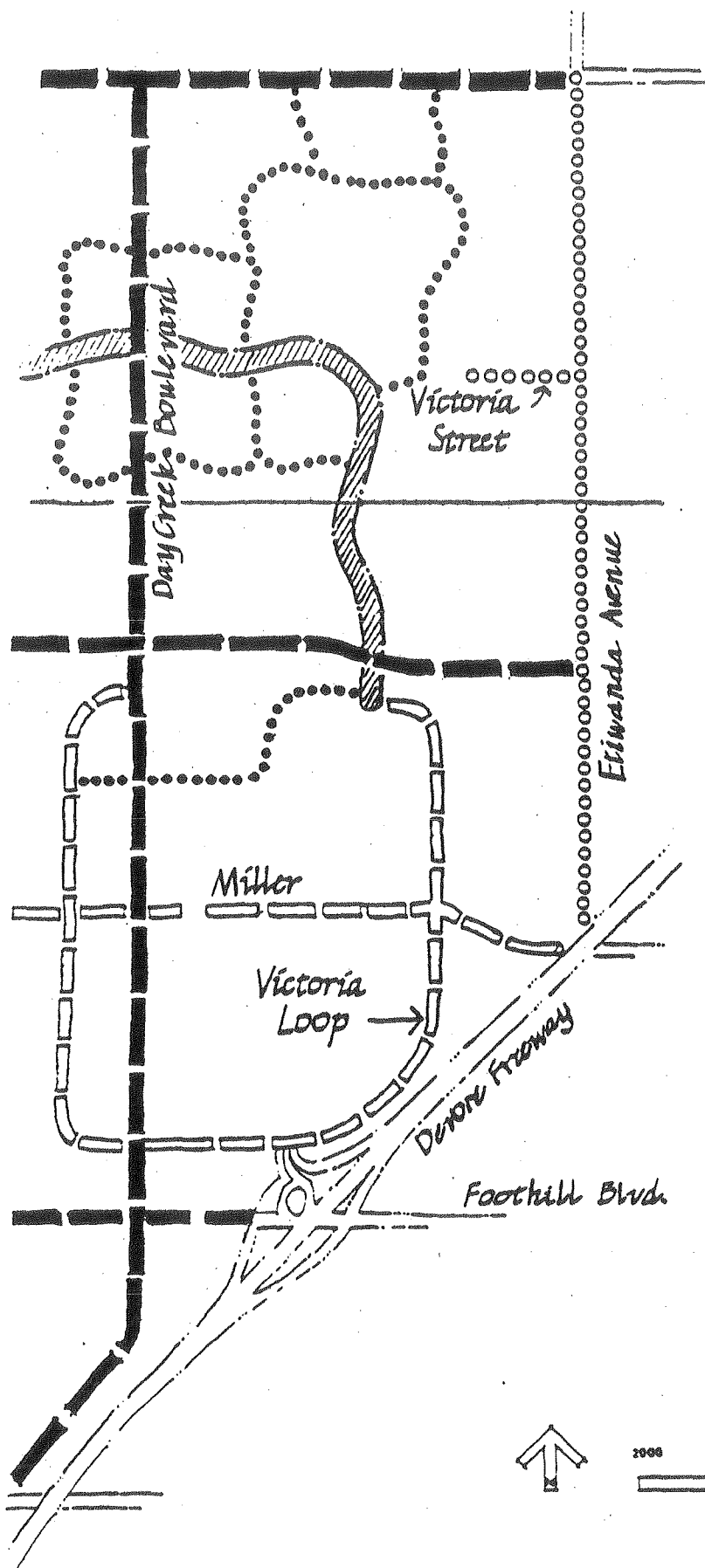
This section contains specific criteria for the roads within Victoria. The Standard Road Section Reference Plan shows the entire planning area and the location of the various kinds of roads. The Standard Road Cross-Sections follow and they contain information such as the width of the right-of-way, pavement width, number of travel lanes, and the location of walks, bicycle lanes and medians. Each road has been examined individually and the design of the cross-section reflects not only the need for efficient automobile circulation, but also the goals of the plan with respect to land use and urban design. All road sections are schematic and do not show any grade conditions that may exist where the roads are built.



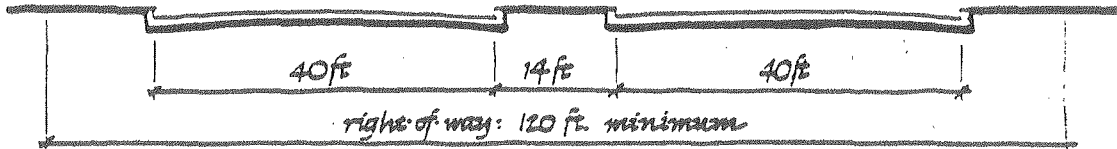
STANDARD ROAD CROSS-SECTIONS REFERENCE PLAN

- REGIONAL CIRCULATION**
*Highland, Milliken, Baseline, Foothill
 Day Creek Boulevard*
- MILLER & VICTORIA LOOP**
- ETIWANDA & VICTORIA STREET**
- LOCAL RESIDENTIAL ROADS**
- VICTORIA PARK LANE**

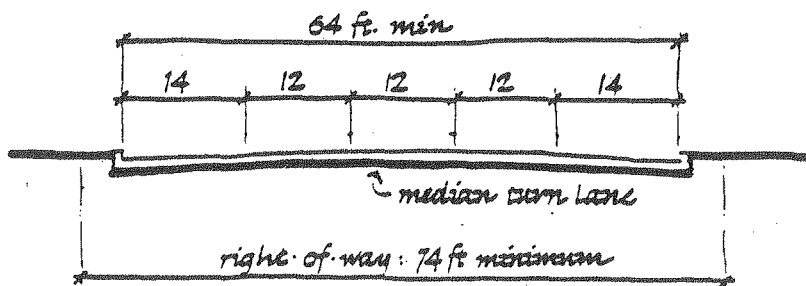
note: public and private neighborhood streets not shown



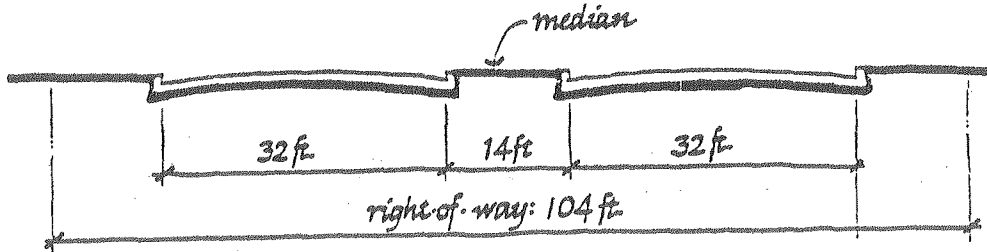
MILLIKEN; BASELINE, DAY CREEK BOULEVARD



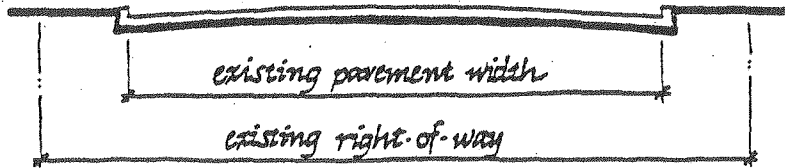
HIGHLAND AVENUE



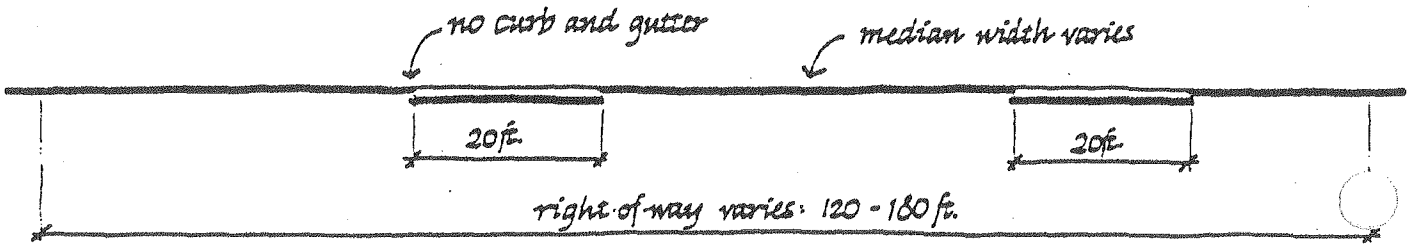
MILLER AVE, VICTORIA LOOP



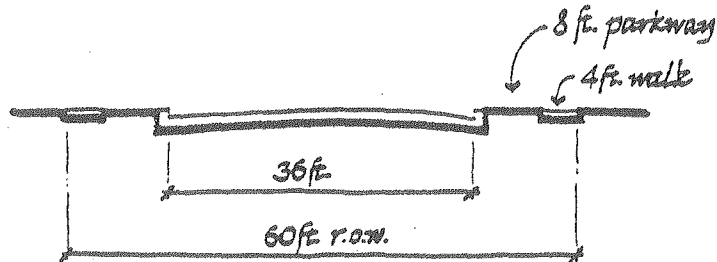
ETIWANDA AVENUE



VICTORIA PARKWAY

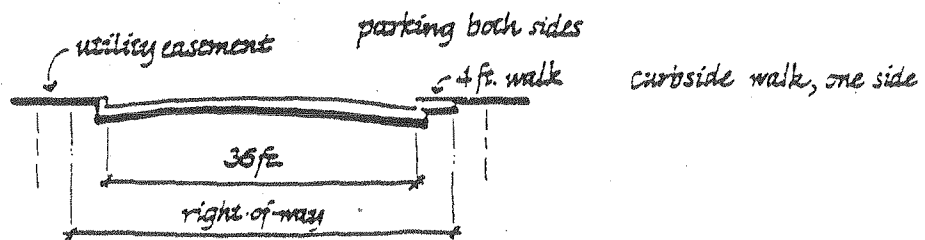
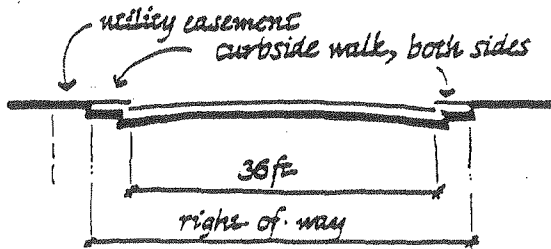


LOCAL RESIDENTIAL ROADS

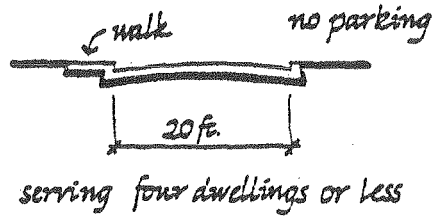
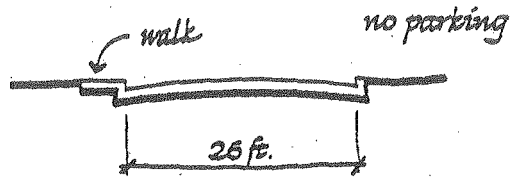
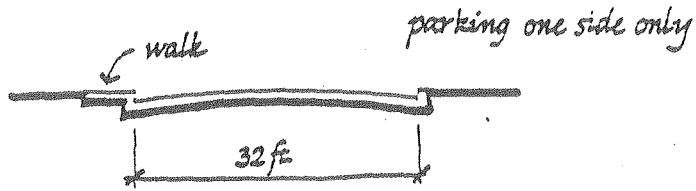


NEIGHBORHOOD STREETS

Neighborhood streets are public or private streets that may have a special cross-section design to respond to a particular condition in the Planned Community. The following examples are typical of the cross-sections that may be used in the Planned Community subject to the Development Approval Process.



PRIVATE NEIGHBORHOOD STREETS



II
IMPLEMENTATION

APPENDIX

APPENDIX A: DEFINITIONS

Abutting: Having lot lines or zone boundaries in common.

Accessory Building: A building which is incidental to and customarily associated with a specific principal use or facility.

Accessory Use: A use customarily incidental and accessory to the principal use of the land or building site, or to a building or other structure located thereon.

Adjusted Gross Acreage: The acreage contained within an area that includes the legal lot and portion of road right-of-way adjacent to the lot up to the road centerline except for arterial roads and Victoria Parkway, which are not included in the Adjusted Gross Acreage.

Adjusted Gross Density: The density of residential dwellings calculated by dividing the total number of dwellings by the Adjusted Gross Average.

Alley: A narrow paved passage between or behind residential, commercial or office buildings designed primarily to provide access to parking facilities or access to service or maintenance vehicles.

Animal Care Facility: A facility designed primarily to provide grooming or veterinary services to animals.

Apartment House: Any building or portion thereof containing three or more dwelling units, each of which is rented, let, or hired out to one family living independently of all other occupants.

Apartment Complex: An apartment house or two or more apartment houses constructed and operated as one multiple-residential entity.

Apartment, Garden: An apartment house or complex with landscaped areas and recreational facilities designed for exclusive use of the project's residents.

Apartment High-Rise: An apartment house exceeding 4 stories in height.

Apartment Project: See Apartment Complex.

Arterial Road: A through road or street serving major traffic needs, including but not limited to those roads designated as Special Boulevards, Major Thoroughfares, and Secondary Thoroughfares by the Rancho Cucamonga General Plan.

Attached Dwelling Unit: Any dwelling unit that shares one or more walls with another, adjacent dwelling unit.

Automobile Service Station: A use providing motor fuel, oil, tires, batteries, small parts and accessories, and services incidental thereto for automobiles, light trucks, and similar motor vehicles.

Automotive Services: A use engaged in sale, rental, service, or minor repair of new or used automobile, trucks, trailers, boats, motorcycles, mopeds, recreational vehicles, or other similar vehicles, including tire recapping, painting, body and fender repair, and engine, transmission, air-conditioning, and glass repair and replacement and similar services.

Building Height: The vertical distance from the finished floor elevation to the highest point of the coping of a flat roof or to the deck line of a mansard roof or to the average height of the highest gable on a pitched or hipped roof, or at the highest point of a structure, measured from a point on the finished floor elevation to a point directly overhead.

Building Site Coverage: The area of a lot covered by buildings, including eaves, projecting balconies, and similar features but excluding ground level paving, landscaping, and open recreational facilities.

Business or Commerce: The purchase, sale, exchange, or other transaction involving the handling or disposition of any article, substance, or commodity for profit or livelihood, or the ownership or management of office buildings, offices, recreation or amusement enterprises, or the maintenance and use of offices by professions and trades rendering services.

Carport: A roofed structure, or a portion of a building, open on two or more sides, primarily for the parking of automobiles belonging to the occupants of the property.

Cluster Housing: More definition of "Single Lot Subdivision": An area developed by combining or arranging attached or detached dwelling units and their accessory structures on contiguous or related residential lots of record where the yards and open spaces are combined into more desirable arrangements of common areas which are not part of the individual lot of record. Single lot subdivisions shall also include statutory condominiums. Designation of single lot subdivision shall be shown on the tentative tract map.

Collector Road: A road or street intended to carry neighborhood traffic only to the nearest arterial road.

Commercial: Shall mean any activity on or use of land which involves the buying, selling, processing, or improving of things not produced on the land and having financial gain as the primary aim of the activity or use; whether or not such activity or use be for hire, or on account of the buyer, seller, processor or improver.

Commercial Recreation: A use providing recreation, amusement, or entertainment services, including theaters, bowling lanes, billiard parlors, skating areas and similar services, operated on a private or for-profit basis.

Communication Equipment Building: A building housing operating mechanical or electronic switching and microwave receiving equipment of a telephone or similar communication system and personnel necessary for operation of such equipment.

Community Facility: A use established primarily for the benefit and enjoyment of the population of the community in which it is located.

Community Plan: A plan which, when approved by the Planning Commission and City Council, establishes guidelines and regulations for development. Where these guidelines or regulations conflict with city ordinances, the regulations of the Community Plan shall have precedence.

Condominium: As defined in the State Subdivision Map Act, see Section 1350 of the Civil Code.

Condominium Project: A parcel of real property divided, or to be divided, into condominiums, including all structures thereon.

Conventional Development: Use Multiple Lot Subdivision definition here.

Cul-De-Sac: A dead-end road or street.

Day Care Facility: A facility, or use of a dwelling unit or portion thereof, for daytime care of individuals. This term includes nursery schools, pre-schools, and similar facilities.

Detached Dwelling Unit: A dwelling unit not attached to any other dwelling units, including, but not limited to, patio homes and zero-lot-line homes.

Driveway: A vehicular passageway for the exclusive use of the occupants of a property and their guests; not considered a street or alley.

Drought Tolerant Plant Material: Any tree, shrub, grass or ground cover that can survive once established with deep infrequent irrigation. It is assumed that two full growing seasons with regular irrigation would be the minimum time necessary to establish any plant material, and that, after established, plants could survive with irrigation spaced no more often than once per month from April through November.

Duplex: A permanent building containing two dwelling units on one building site.

Dwelling Unit: A single residential unit providing complete, independent living facilities for one family, including permanent provisions for living, sleeping, eating and cooking.

Easement: A recorded right or interest in the land of another, which entitles the holder thereof to some use, privilege, or benefit out of or over said land.

Financial Facilities: Uses relating to the circulation of money, the granting of credit, the making of investments, provision of banking facilities and ancillary uses.

Gross Acre: The total area of land within specified lot lines, including all roads, streets and land uses.

Gross Density: The total number of dwelling units per gross acre of land.

Gross Floor Area: The total horizontal area, in square feet, including the exterior walls, of all floors of a structure.

Gross Leasable Area: The total horizontal area, in square feet, which is leased, except for Major Department Stores, whose gross leasable area shall be defined as sales floor area only.

Hotel: Any building or portion thereof with access provided through a common entrance, lobby, or hallway to six or more guest rooms having no cooking facilities and which rooms are used to provide temporary or overnight lodging of guests.

Home Improvement Center: A retail service engaged in providing retail sale, rental, service, or related repair and installation of home improvement products, including building materials, paint and wallpaper, carpeting and floor covering, decorating, heating, air conditioning, electrical, plumbing, and mechanical equipment, roofing supplies, yard and garden supplies, home appliances, and similar home improvement products.

Kennel: A totally enclosed facility designed primarily to provide boarding services for domestic animals such as cats and dogs.

Landscaping: An area devoted to, or developed and maintained predominantly with native or exotic planting, lawn, groundcover, gardens, trees, shrubs and other plant materials; and also including accessory decorative outdoor landscape elements such as pools, fountains, water features, paved or decorated surfaces or rock, stone, brick, block or similar material (excluding driveways, parking, loading or storage areas), and sculptural elements.

Local Residential Street: A public or private street or road that dispenses automobile traffic from feeder roads to individual dwelling units.

Lot or Site: A parcel of real property used or intended for use under the provisions of the regulations as a site for use or a group of uses, including:

- a. A parcel with a separate and distinct number or other designation shown on a plat recorded in the office of the County Recorder.
- b. A parcel delineated on an approved record of survey, parcel map or sub-division map as filed in the office of the County Recorder or County Planning Department, or in the Community Development Department of the City, and abutting at least one (1) public street or right-of-way, or easement determined by the Director to be adequate for the purpose of access, or
- c. A parcel legally created or established pursuant to applicable zoning or sub-division regulations in effect prior to the effective date of application of this ordinance to such parcel.
- d. Corner Lot: A lot located at the intersection of two streets, or of two segments of a curved street, forming an angle of not more than one hundred thirty five degrees.
- e. Flag (Panhandle) Lot: A lot having access to a street by means of a private driveway, access easement, or parcel of land not meeting the requirements of this ordinance for lot width, but having a dimension of at least 20 feet at its narrowest point.
- f. Interior Lot: A lot other than a corner lot.
- g. Reverse Corner Lot: A corner lot, having a side lot line which is substantially a continuation of the front lot line of a lot to its rear.

- h. **Through Lot:** A lot other than a corner lot abutting more than one street.

Lot Area: The net horizontal area within bounding lot lines, but excluding any portion of a flag (panhandle) lot providing access to a street and excluding any public or private easement or right-of-way providing access to another lot.

Lot Depth: The horizontal distance between the midpoint of the front lot line and the midpoint of the rear lot line.

Lot Line: A line bounding a lot as herein defined.

- a. **Front Lot Line:** On an interior lot, the lot line abutting the street. On a corner lot, the shorter lot line abutting a street or the line designated as the front lot line by a subdivision or parcel map. On a through lot, the lot line abutting the street providing the primary access to the lot. On a flag (panhandle) lot, the interior lot line designated as a front lot line by a subdivision or parcel map.
- b. **Interior Lot Line:** A lot line not abutting a street.
- c. **Rear Lot Line:** A lot line, not intersecting a front lot line, which is most distant from and most closely parallel to the front lot line. In the case of an irregularly shaped lot or lot bounded by only three lot lines, a line within the lot having a length of 10 feet, parallel to and most distant from the front lot line shall be interpreted as the rear lot line for the purpose of determining required yards, setbacks and other provisions of this ordinance.
- d. **Side Lot Line:** A lot line that is not a front or rear lot line
- e. **Street Lot Line:** A lot line abutting a street

Lot Width: The horizontal distance between side lot lines, measured at the front setback line, or the average of the front and rear lot lines, whichever is greater.

Medical Office: A use providing consultation, diagnosis, therapeutic, preventative, or corrective personal treatment services by doctors, dentists, medical and mental laboratories, and similar practitioners of medical and healing arts for humans licensed for such practice by the State of California.

Motel: A building or group of buildings containing guest rooms or dwelling units used primarily to provide temporary lodging for transient automobile travelers, including but not limited to buildings or building groups designated as auto cabins, motor courts, or motor hotels.

Multiple Family Use: The use of a site for two or more dwelling units, which may be in the same building or in separate buildings on the same site.

Multiple Lot Subdivisions: areas developed in such a manner that each dwelling unit is situated as a residential lot of record and no lot contains more than one (1) dwelling unit. Designation of multiple lot subdivision shall be shown on the tentative tract map. Also called Conventional Development.

Net Acre: The area of land devoted to a specific land use, including the building area, parking area, driveways, private roads and accessory buildings or uses, but not including public right-of-way or other land uses.

Net Density: The number of dwelling units per net acre of land.

Office Uses: A building or group of buildings designed for use as a place of business for persons engaged in recognized professions, administrative functions, and/or business activities, and for those support activities related to such uses.

Park: A tract of land set aside for public use and enjoyment.

Parking Aisle: Driveways which have parking space taking access from either or both sides and which are intended primarily to provide access to parking spaces.

Parking Space: An area on a lot or within a building, used or intended for use for parking of a motor vehicle, having permanent means of access to and from a public street or alley independently of any other parking space.

Parking Lot: An area containing one or more parking spaces together with driveways, aisles, turning and maneuvering areas, and similar areas.

Parkway: The area of land between the road curb and the ultimate right-of-way.

Patio Home: See: "Zero-Lot-Line"

Private: Belonging to, or restricted for the use or enjoyment of, particular persons.

Plot Plan: See: "Site Plan"

Private Educational Facility: A privately owned school, including schools owned and operated by religious organizations, offering instruction in the several branches of learning and study required to be taught in the public schools by the Education code of the State of California.

Professional Offices: A use providing professional or consulting services in the fields of law, architecture, medicine design, engineering, accounting, and similar professions.

Public Education Facility: A public school, operated by a public school district and offering instruction in the several branches of learning and study required to be taught in the public schools by the Education Code of the State of California.

Restaurant: A use providing preparation and retail sale of food and beverages, including restaurants, fountains, cafes, coffee shops, sandwich shops, ice cream parlours, and similar uses, and including licensed "on-sale" provision of wine and beer for consumption on the premises when accessory to such food service.

Right-of-Way: An area or strip of land, either public or private, on which an irrevocable right of passage has been recorded for the use of vehicles or pedestrians, or both.

Screened: Shielded, concealed, and effectively hidden from view by a person standing at ground level on an abutting site, or outside the area or subject so screened, by a fence, wall, hedge, berm or similar architectural or landscape feature.

Service Business: Direct sale to the public of personal and professional service.

Setback Area: The area between the building line and the property line, or when abutting a street, the area between the building line and the ultimate right-of-way line.

Setback Distance: The distance between the building line and the property line, or when abutting a street, between the building line and the ultimate right-of-way line.

Setback Line: A line within a lot parallel to and measured from a corresponding lot line, forming the boundary of a required yard and governing the placement of structures uses on the lot.

Shopping Center: A commercial center, or group of commercial establishments, planned, developed, managed, and maintained as a unit, with common off-street parking provided to serve all uses on the property.

Single Lot Subdivisions: An area developed by combining or arranging attached or detached dwelling units and their accessory structures on contiguous or related residential lots of record where the yards and open spaces are combined into more desirable arrangements of common areas which are not part of the individual lot of record. Single lot subdivisions shall also include statutory condominiums. Designation of single lot subdivision shall be shown on the tentative tract map.

Site Plan: A plan, prepared to scale, showing accurately and with complete dimensioning, all of the buildings, structures and uses and the exact manner of development proposed for a specific parcel of land.

Site Plan Review: The Development Approval Process as specified in Part III, Section II-A.

Story: That portion of a building included between the upper surface of any floor and the upper surface of the floor next above, except that the topmost story shall be that portion of a building included between the upper surface of the topmost floor and the ceiling or roof above.

Street: A public or private thoroughfare which affords a primary means of access to abutting property, including all land within the right-of-way thereof.

Structure: That which is built or constructed an edifice or building of any kind, or any piece of work artificially built up or composed of parts joined together in some definite manner.

Swimming Pool: An outside body of water artificially created and used for swimming or bodily immersion any portion of which exceeds 18 inches in depth.

Townhouse: Two or more dwelling units arranged in a row, connected by common side walls, and usually having two or more stories.

Trail: A trail or way designed for and used by equestrians, pedestrians, or cyclists using non-motorized bicycles.

Ultimate Right-of-Way: The right-of-way shown as ultimate on an adopted precise plan of highway alignment, or the street rights-of-way shown within the boundary of a recorded tract map, a recorded parcel map, or a recorded Planning Commission development plan. The latest adopted or recorded document in the above cases shall take precedence. If none of these exists, the ultimate right-of-way shall be considered the right-of-way required by the highway classification as shown on the Master Plan of Arterial Highways. In all other instances, the ultimate right-of-way shall be considered to be the existing right-of-way.

Use: The purpose for which land or a building is occupied, arranged, designed, or intended, or for which either land or building is or may be occupied or maintained.

Zero Lot Line: A type of residential development in which residential dwellings on individual lots of record are situated so that one or two of the exterior building walls relocated directly on the property line.

